State of Florida
Department of Transportation
Public Transit Office

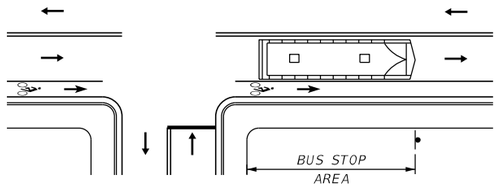
TRANSIT FACILITIES GUIDELINES

7/28/2017 5:19:20 PM

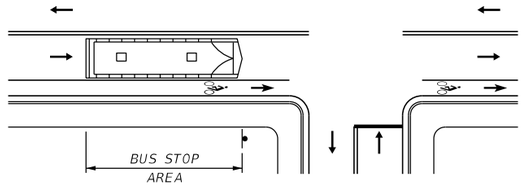
	TRANSIT FACILITIES GUIDELINES STREETSIDE BUS STOP LOCATIONS & DESIGN TYPES	SHEET NO. CS

TABLE OF CONTENTS	
STREETSIDE BUS STOP LOCATIONS & DESIGN TYPES	1 of 41
STREETSIDE COMBINATION BUS STOP LOCATIONS	2 of 41
BUS STOP LOCATION RELATIVE TO ACCESS POINTS	3 of 41
BUS STOP LOCATION RELATIVE TO RAILROAD CROSSING	4 of 41
BUS STOP LANDING PADS AND SIGNAGE	5 of 41
BUS SHELTER DETAILS	6 of 41
CLOSED BUS BAY LAYOUT URBAN/CURB & GUTTER	7 of 41
CLOSED BUS BAY LAYOUT RURAL/SHOULDER	8 of 41
NEAR SIDE BUS FACILITY DECISION TREE	9 of 41
NEAR SIDE BUS STOPS	10 of 41
NEAR SIDE BUS BAYS WITH ON STREET PARKING	11 of 41
NEAR SIDE BUS BAYS WITH RIGHT TURN LANE	12 of 41
NEAR SIDE BUS BAYS/STOPS	13 of 41
NEAR SIDE BUS BAYS/STOPS	14 of 41
FAR SIDE BUS FACILITY DECISION TREE	15 of 41
FAR SIDE BUS BAYS WITH RIGHT TURN LANE	16 of 41
FAR SIDE CLOSED BUS BAYS WITH RIGHT TURN LANE	17 of 41
FAR SIDE NUB/BULB WITH ON STREET PARKING	18 of 41
FAR SIDE BUS BAYS	19 of 41
FAR SIDE BUS BAYS/STOPS	20 of 41
MID-BLOCK BUS FACILITY DECISION TREE	21 of 41
MID-BLOCK DETAILS	22 of 41
MID-BLOCK BUS STOPS WITH ON STREET PARKING	23 of 41
MID-BLOCK BUS BAYS/STOPS	24 of 41
MID-BLOCK BUS BAYS/STOPS	25 of 41
MID-BLOCK BUS STOPS	26 of 41
MID-BLOCK BUS BAYS/STOPS	27 of 41
NEAR SIDE BUS STOP LOCATION ADJACENT TO CANAL	28 of 41
FAR SIDE BUS STOP LOCATION ADJACENT TO CANAL	29 of 41
MID-BLOCK BUS STOP LOCATION ADJACENT TO CANAL	30 of 41
NEAR SIDE ISOLATED BUS STOP LOCATION ADJACENT TO CANAL	31 of 41
FAR SIDE ISOLATED BUS STOP LOCATION ADJACENT TO CANAL	32 of 41
MID-BLOCK ISOLATED BUS STOP LOCATION ADJACENT TO CANAL	33 of 41
BOARDING AND ALIGHTING AREA DETAIL	34 of 41
BOARDING AND ALIGHTING AREA DETAIL	35 of 41
OFF STREET HALF SAW TOOTH BUS BAY	36 of 41
CONCURRENT FLOW BUS LANES MID-BLOCK	37 of 41
CONTRA-FLOW BUS LANES MID-BLOCK	38 of 41
BUSWAY MID-BLOCK	39 of 41
EXCLUSIVE BUSWAY	40 of 41
BUS-ON-SHOULDER OPERATIONS UNIT ERUPTED FLOW HIGHWAY	41 of 41

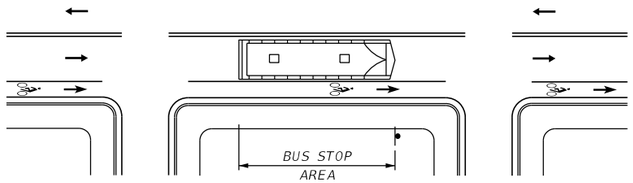
STREETSIDE BUS STOP LOCATIONS



FAR SIDE BUS STOP

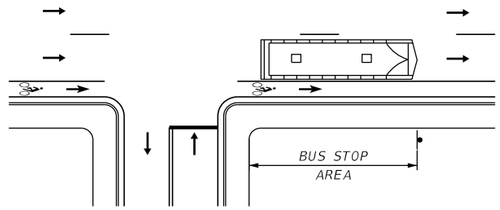


NEAR SIDE BUS STOP

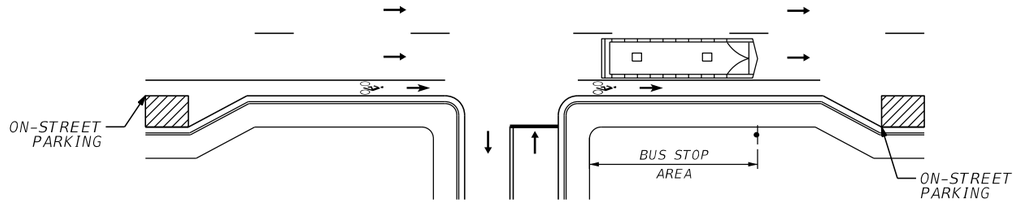


MID-BLOCK BUS STOP

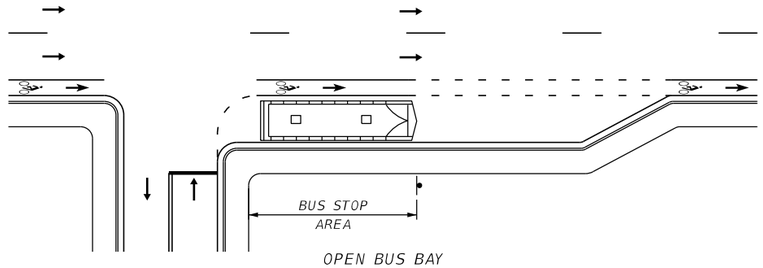
STREETSIDE BUS DESIGN TYPES



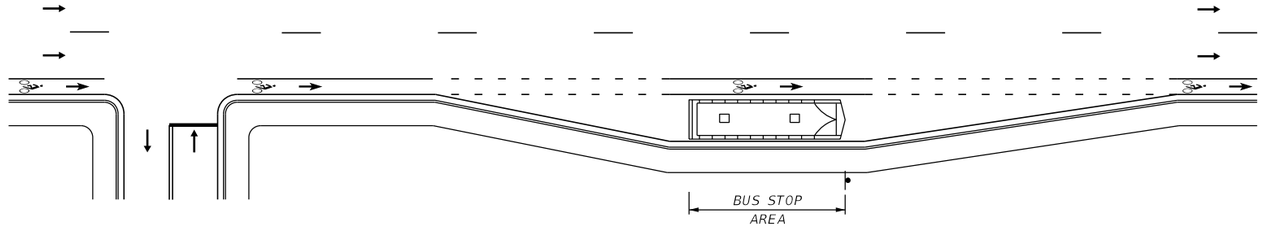
CURBSIDE BUS STOP
FAR SIDE



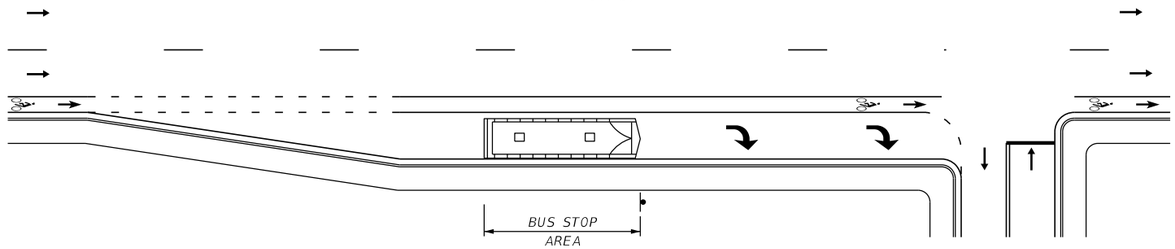
BUS NUB/BULB OR
CURB EXTENSION
(FAR SIDE)



OPEN BUS BAY
FAR SIDE



CLOSED BUS BAY
(FAR SIDE)



RIGHT TURN LANE BUS BAY
(NEAR SIDE)

STREETSIDE BUS FACILITY GENERAL GUIDELINES

1. FAR SIDE BUS STOPS AND BAYS ARE GENERALLY PREFERRED OVER NEAR SIDE STOPS AND BAYS
   - EXCEPTION IS AT TWO-LANE ROADWAYS WHERE VEHICLES ARE RESTRICTED FROM GOING AROUND THE BUS STOPPED AT A CURBSIDE STOP

2. BUS BAYS ARE GENERALLY PREFERRED OVER CURBSIDE BUS STOPS IN TRAVEL LANES
   - PARTICULARLY FOR ARTERIAL DESIGN SPEEDS GREATER THAN OR EQUAL TO 40 MPH AND
   - WHERE THERE IS SIGNIFICANT BUS AND PASSENGER VOLUMES AND
   - WHERE PLACED DOWNSTREAM FROM A TRAFFIC SIGNAL

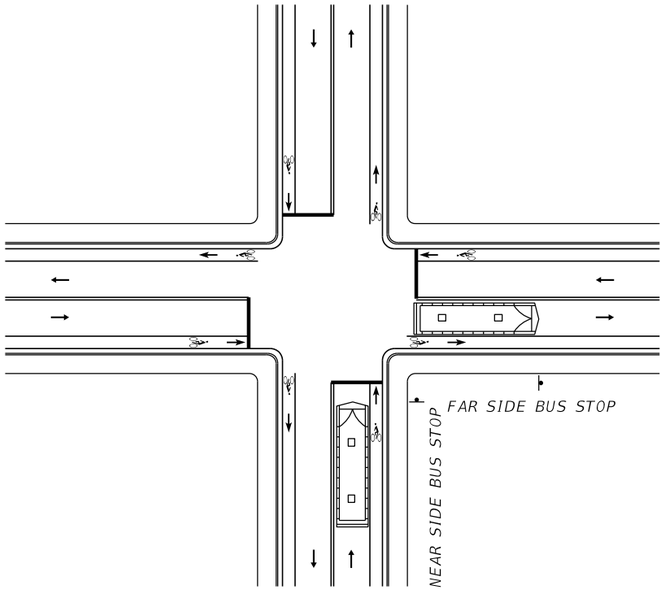
3. CLOSED BUS BAYS ARE GENERALLY PREFERRED OVER OPEN BUS BAYS
   - EXCEPTION WOULD BE AT A PHYSICALLY CONSTRAINED SITE.

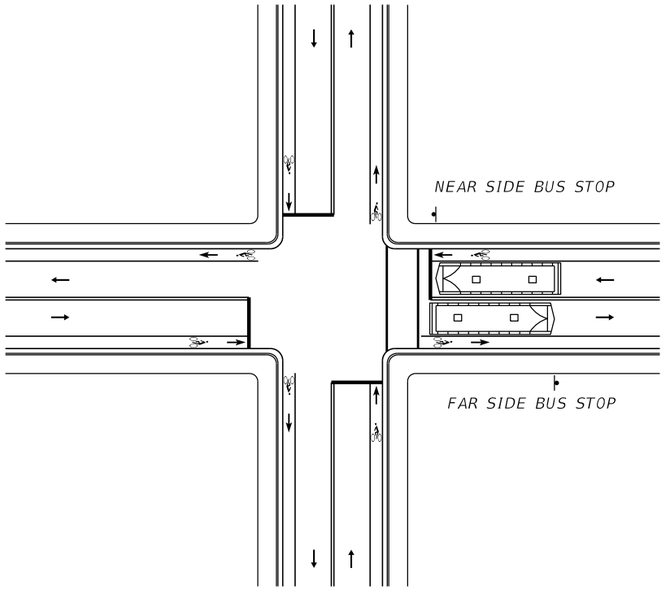
$TIMES

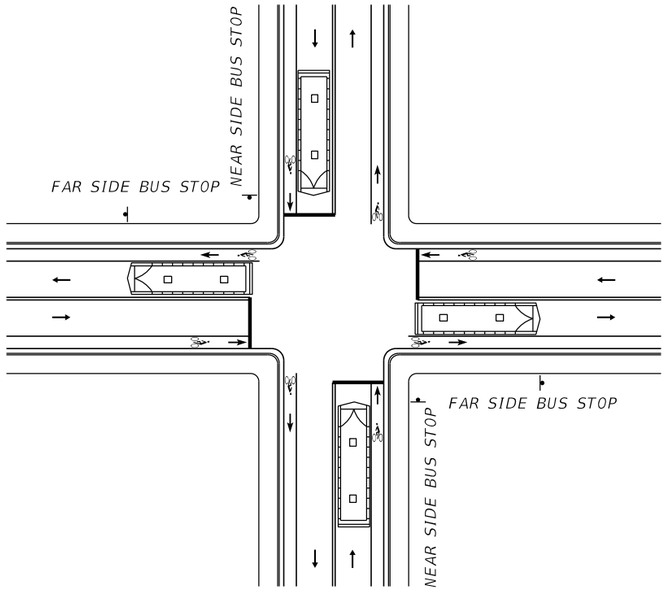
$DATES

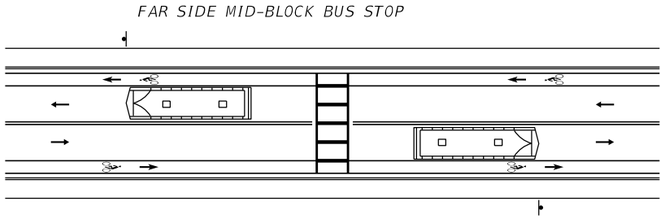
TRANSIT FACILITIES GUIDELINES

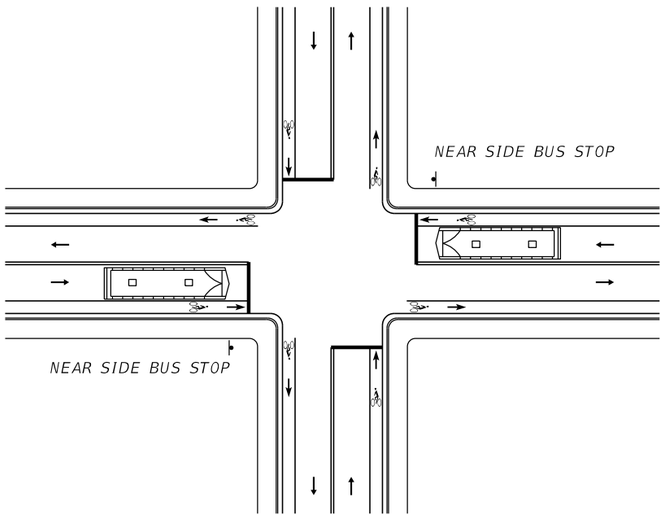
SHEET NO.
1 of 41

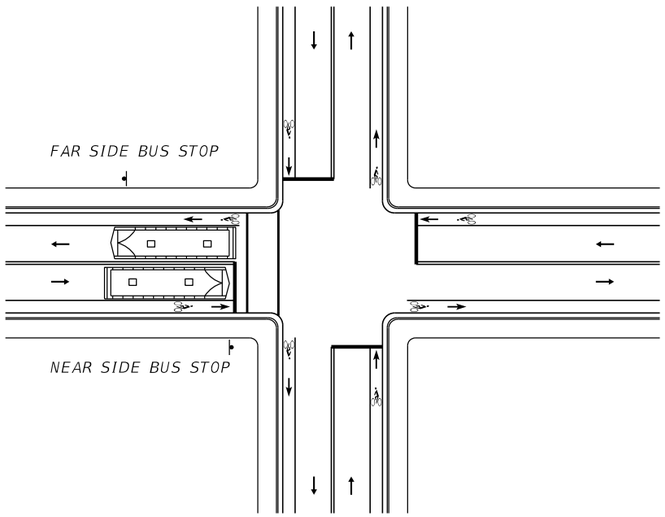


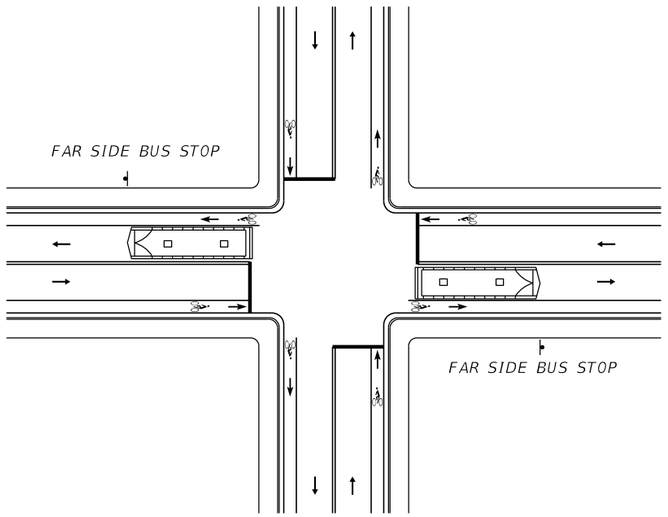












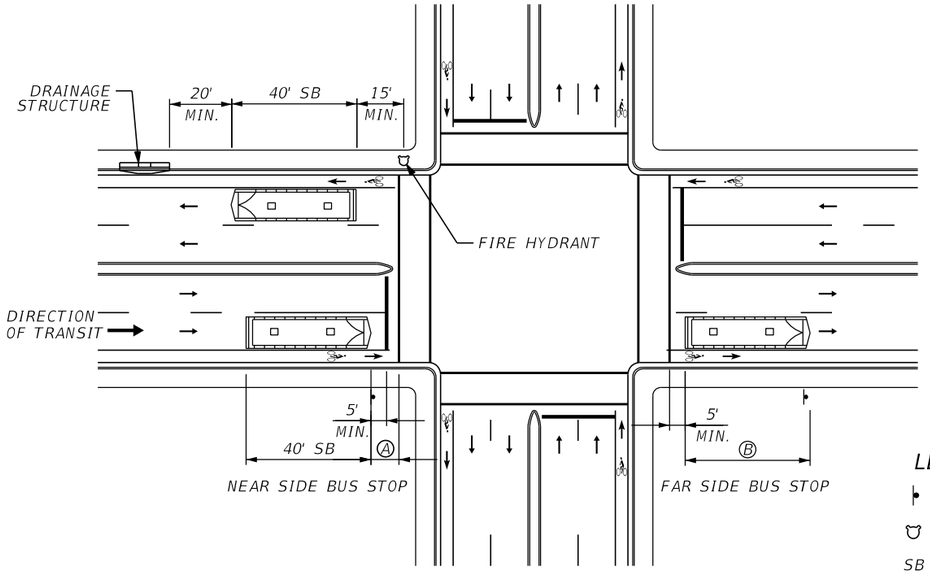
STREETSIDE COMBINATION BUS STOP LOCATIONS

$TIME$

$DATE$

TRANSIT FACILITIES GUIDELINES

SHEET NO. 2 of 41



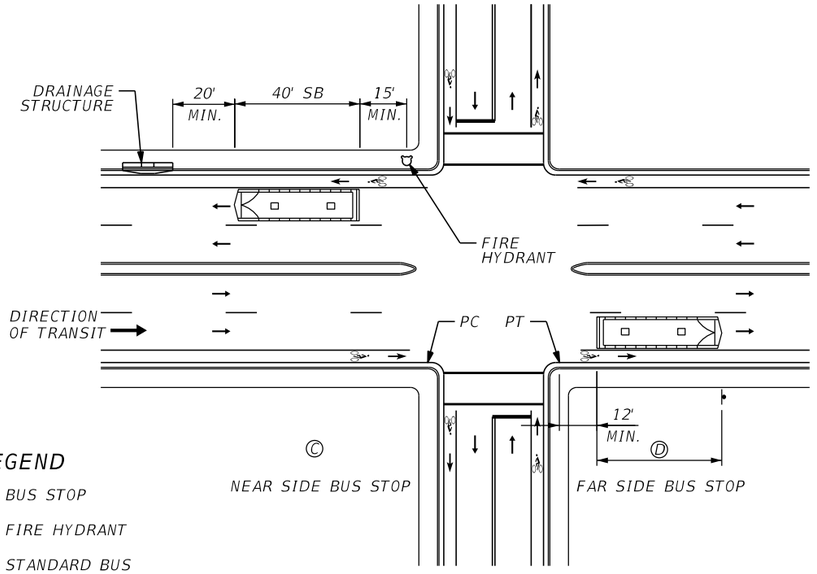
MAJOR INTERSECTION-SIGNALIZED

LEGEND

BUS STOP

FIRE HYDRANT

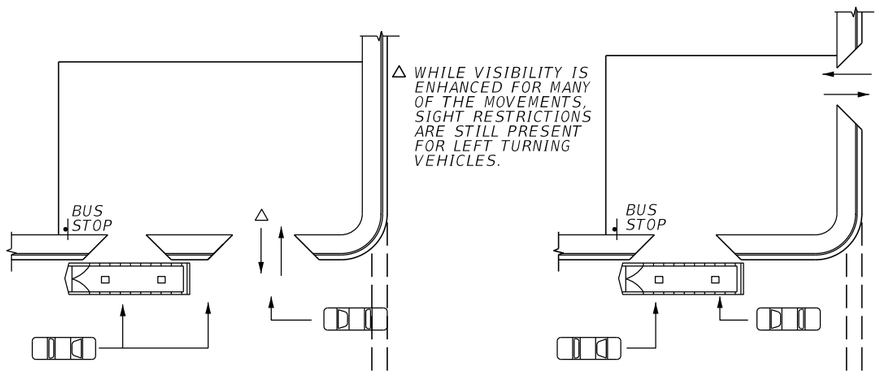
SB STANDARD BUS



MINOR INTERSECTION-UNSIGNALIZED

GENERAL NOTES

1. THESE BUS STOP CONFIGURATIONS ARE PROVIDED AS GUIDELINES. ACTUAL BUS STOP PLACEMENT SHOULD TAKE ALL LOCATION FACTORS INTO ACCOUNT AND BE BASED ON ENGINEERING JUDGEMENT FOR SAFETY.
2. ALL BUS STOPS IN URBAN AREAS (CURB & GUTTER AND SIDEWALK) SHALL BE SITUATED SO THAT PASSENGERS BOARD AND ALIGHT AT A LOCATION WHERE FULL HEIGHT CURB & GUTTER IS PRESENT AND NOT IN A DRIVEWAY.
3. ALL BUS STOPS IN URBAN AREAS (CURB & GUTTER AND SIDEWALK) SHALL BE SITUATED TO BE 20' OR MORE AWAY FROM THE EDGE OF A DRAINAGE STRUCTURE, AND 15' OR MORE AWAY FROM A FIRE HYDRANT OR DISABLED PARKING SPACE.
4. WHEN POSSIBLE, PROVIDE BUS STOP AT AN EXISTING ROADWAY LIGHT POLE. A 4' CLEAR TRAVEL PATH MUST BE PROVIDED ON SIDEWALKS AT ALL TIMES IN THE VICINITY OF THE BUS STOP. THE RECOMMENDED MINIMUM ILLUMINATION LEVEL IS 1.0 HORIZONTAL FOOT CANDLE FOR THE ENTIRE BUS STOP AREA. (REFERENCE PLANS PREPARATION MANUAL).
5. FOR BOARDING AND ALIGHTING AREA REQUIREMENTS SEE BUS STOP PAD AND SIGNAGE DETAILS.
6. ALL BUS BAY DRAWINGS REPRESENT A BUS STOP WITH SPACE FOR ONE BUS AT A TIME. IF MORE THAN ONE BUS IS EXPECTED, ADD 50' FOR EACH ADDITIONAL STANDARD BUS AND 70' FOR EACH ADDITIONAL ARTICULATED BUS.
7. DECELERATION/ACCELERATION LANE REQUIREMENTS SHOULD BE CONSIDERED FOR BUS BAY TYPE DESIGNS IF RIGHT OF WAY ALLOWS.



ACCEPTABLE BUS STOP/DRIVEWAY ARRANGEMENTS

DRIVEWAY BUS STOP LOCATION GUIDELINES

1. AVOID BUS STOPS THAT BLOCK THE DRIVEWAY OF A LOT WITH A SINGLE DRIVEWAY
2. BUS STOPS SHOULD NOT BE LOCATED WITHIN THE AREA OF INFLUENCE OF A DRIVEWAY TO AVOID SIGHT DISTANCE AND OTHER CONFLICTS. HOWEVER, IF THE SITUATION CANNOT BE AVOIDED:
3. LOCATE THE STOP AS FAR DOWNSTREAM (FAR SIDE) FROM THE DRIVEWAY AS FEASIBLE.
4. AVOID UPSTREAM (NEAR SIDE) STOPS IN THE TRAVEL LANE. UPSTREAM BAYS ARE ACCEPTABLE.
5. LOCATE THE STOP TO ALLOW APPROPRIATE VISIBILITY FOR VEHICLES ENTERING OR LEAVING THE DEVELOPMENT AND TO MINIMIZE VEHICLE/BUS CONFLICTS.
6. LOCATE THE STOP SO THAT PASSENGERS ARE NOT FORCED TO WAIT FOR A BUS IN THE MIDDLE OF A DRIVEWAY.
7. LOCATE THE STOP SO THAT PATRONS BOARD OR ALIGHT DIRECTLY FROM THE CURB RATHER THAN FROM THE DRIVEWAY.
8. LOCATE THE STOP SO THAT THE FRONT DOOR BOARDING AND ALIGHTING AREA IS LOCATED OUTSIDE THE DRIVEWAY.
9. ATTEMPT TO KEEP AT LEAST ONE EXIT AND ENTRANCE DRIVEWAY OPEN FOR VEHICLES ACCESSING THE DEVELOPMENT WHILE A BUS IS LOADING OR UNLOADING PASSENGERS.
10. FOR AN UNACCEPTABLE CONDITION AT A CORNER PARCEL, MOVING THE STOP DOWNSTREAM TO THE NEXT PARCEL SHOULD BE EVALUATED.

INTERSECTION BUS STOP LOCATION CRITERIA

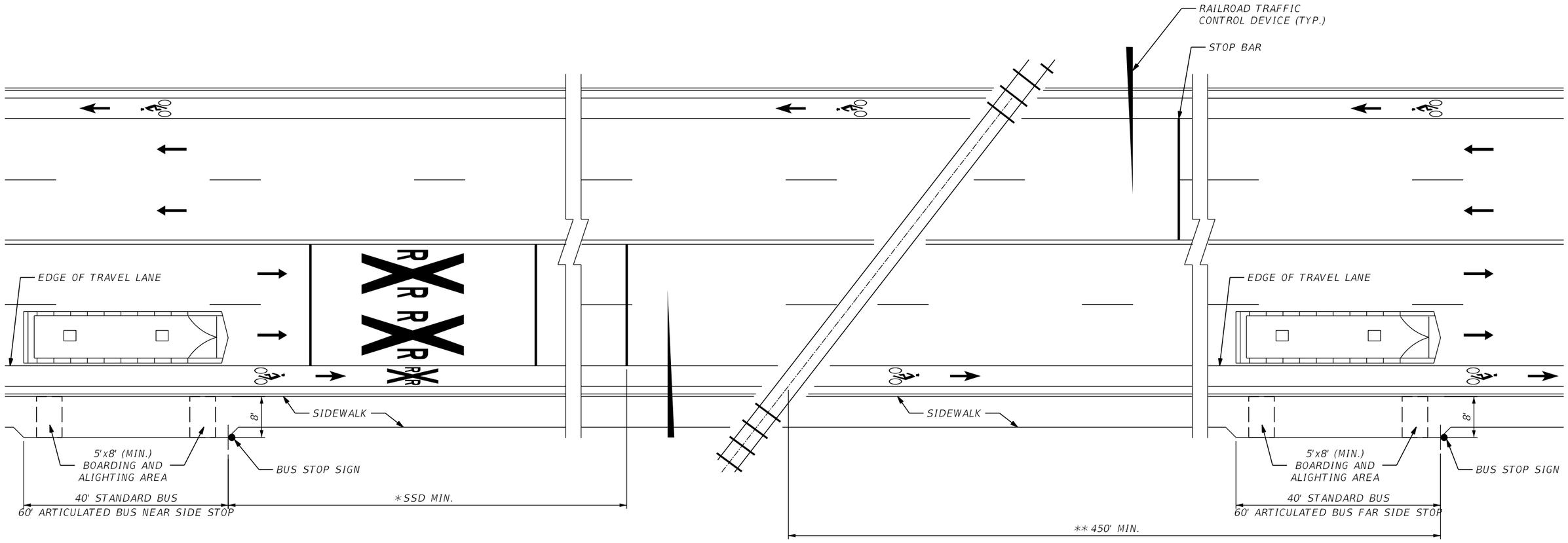
BUS STOP VARIABLES	NO TURN LANES IN DIRECTION OF TRANSIT	RIGHT TURN LANE ON NEAR SIDE IN DIRECTION OF TRANSIT	RIGHT TURN LANE ON NEAR SIDE AND AUXILIARY LANE ON FAR SIDE IN DIRECTION OF TRANSIT **	AUXILIARY LANE ON FAR SIDE IN DIRECTION OF TRANSIT ***
DIMENSION Ⓐ (NEAR SIDE MAJOR INTERSECTION)	NOT RECOMMENDED * UNLESS 2-LANE ROADWAY	NOT RECOMMENDED UNLESS 2-LANE ROADWAY 10' BEFORE ENTRY TAPER FOR TURN BAY 100' IF DROP LANE	NOT RECOMMENDED UNLESS 2-LANE ROADWAY	NOT RECOMMENDED * UNLESS 2-LANE ROADWAY
DIMENSION Ⓑ (FAR SIDE MAJOR INTERSECTION)	45' FOR A STANDARD BUS (65' FOR AN ARTICULATED BUS)	45' FOR A STANDARD BUS (65' FOR AN ARTICULATED BUS)	110' **	45' FOR A STANDARD BUS (65' FOR AN ARTICULATED BUS) ***
DIMENSION Ⓒ (NEAR SIDE MINOR INTERSECTION)	NOT RECOMMENDED * UNLESS 2-LANE ROADWAY OR LOCATED OUTSIDE OF SIGHT TRIANGLE	AS CLOSE TO ENTRY TAPER AS FEASIBLE AND LOCATED OUTSIDE OF SIGHT TRIANGLE	N/A	N/A
DIMENSION Ⓓ (FAR SIDE MINOR INTERSECTION)	45' FOR A STANDARD BUS (65' FOR AN ARTICULATED BUS)	45' FOR A STANDARD BUS (65' FOR AN ARTICULATED BUS)	N/A	N/A

* IF NECESSARY, 12' MINIMUM DIMENSION IS REQUIRED.
** THIS COMBINATION OF BUS BAYS IS REFERRED TO AS A QUEUE BYPASS BUS BAY.
*** THIS ARRANGEMENT IS REFERRED TO AS AN OPEN BUS BAY.

NOTE: THIS TABLE IS PREPARED AS A GUIDELINE FOR THE LOCATION OF BUS STOPS ALONG A STATE ROAD FACILITY WHERE ON-STREET PARKING DOES NOT EXIST.

$TIMES
$DATES

TRANSIT FACILITIES GUIDELINES



URBAN BUS STOP NEAR AT-GRADE RAILROAD CROSSING

NEAR SIDE BUS STOP PLACEMENT	
DESIGN SPEED (MPH)	* AASHTO STOPPING SIGHT DISTANCE (SSD) (FT)
25	155
30	200
35	250
40	305
45	360
50	425
55	495
60	570
65	645
70	730

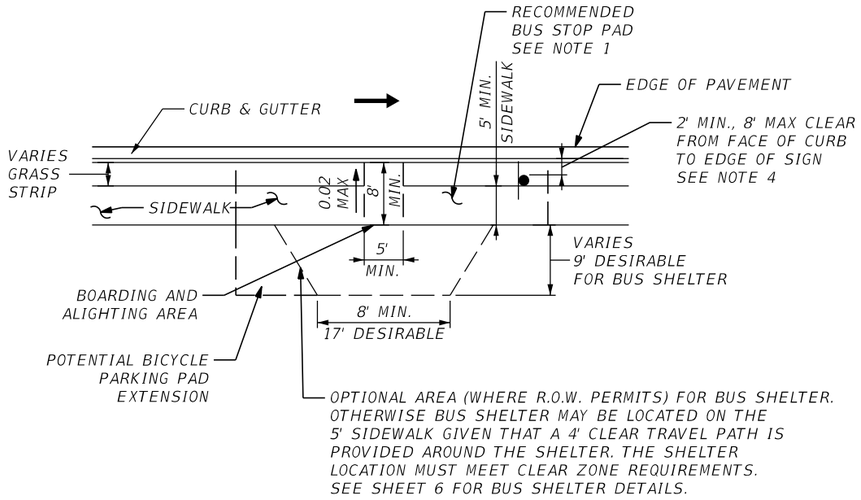
GENERAL NOTES

1. WHEN POSSIBLE IT IS RECOMMENDED TO PLACE BUS STOPS ON THE NEAR SIDE OF A RAILROAD CROSSING TO AVOID CREATING A QUEUE THAT WOULD CONFLICT WITH THE CROSSING.
2. NEAR SIDE BUS STOPS SHALL BE LOCATED SO THAT RAILROAD WARNING SIGNS ARE NOT OBSTRUCTED BY A STOPPED BUS.
3. SEE INDEX 17346 AND 17882 FOR RAILROAD MARKING AND SIGN DETAILS NOT SHOWN HERE.
4. FOR NEAR SIDE OR FAR SIDE BUS BAYS, PROVIDE A MINIMUM OF 50 FT TO THE NEAREST RAIL LINE. (PER FLORIDA STATUTE 316.1945(1))
5. FOR BOARDING AND ALIGHTING AREA REQUIREMENTS SEE SHEET 5.

* MIN VALUE CALCULATED BASED ON PROVIDING APPROPRIATE STOPPING SIGHT DISTANCE TO RAILROAD CROSSING. DIMENSION SHOULD BE VERIFIED BASED ON THE SPECIFIC DESIGN CONDITIONS OF EACH CROSSING.

** MIN VALUE CALCULATED BASED ON ACCOMMODATING THE QUEUE THAT WOULD DEVELOP DURING A 60 SECOND PERIOD BUS STOP. DIMENSION SHOULD BE VERIFIED BASED ON THE SPECIFIC DESIGN CONDITIONS, TRAFFIC PROJECTIONS, AND EXPECTED BUS STOP DELAYS AT EACH CROSSING.

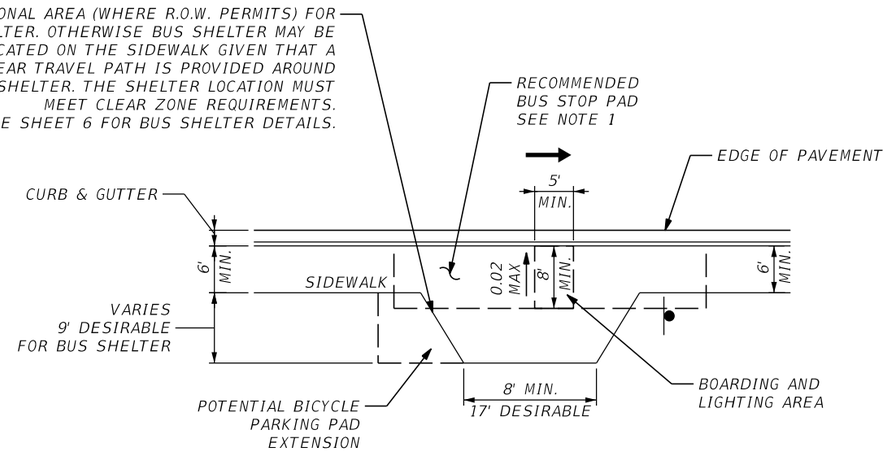
$TIMES
$DATES



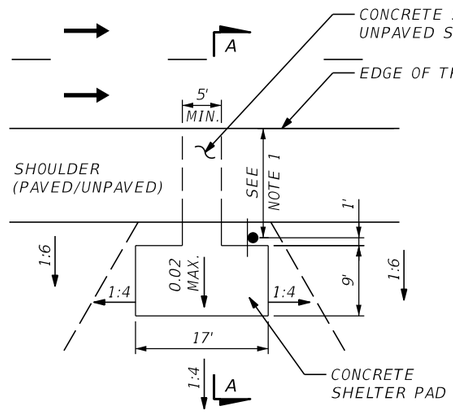
BUS STOP PAD AND SIGNAGE
URBAN/CURB & GUTTER CONDITION WITH GRASS STRIP

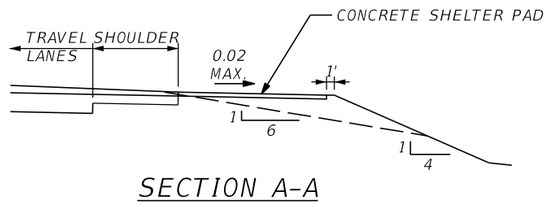
GENERAL NOTES

1. IF PERMITTED BY R.O.W., PROVIDE A CONTINUOUS 8' WIDE CONCRETE PAD ALONG THE ENTIRE LENGTH (40' STANDARD BUS – 60' ARTICULATED BUS) OF THE BUS STOP ADJACENT TO THE CURB & GUTTER. AN ADDITIONAL 50' LENGTH IS RECOMMENDED FOR EACH ADDITIONAL BUS EXPECTED TO SIMULTANEOUSLY STOP AT THE BUS STOP.
2. BOARDING AND ALIGHTING AREA MINIMUM DIMENSIONS ARE TO BE PROVIDED UNLESS R.O.W. DOES NOT ALLOW.
3. A BICYCLE PARKING AREA SHOULD BE PROVIDED ON THE UPSTREAM SIDE OF THE BUS SHELTER PAD BASED ON COORDINATION WITH THE LOCAL TRANSIT AGENCY.
4. BUS STOP PANEL MUST BE LOCATED SUCH THAT A MINIMUM CLEARANCE OF 48" IS PROVIDED ON THE SIDEWALK. FOR SIGN DETAILS SEE INDEX 11860.



BUS STOP PAD AND SIGNAGE
URBAN/CURB & GUTTER CONDITION WITHOUT GRASS STRIP





SECTION A-A

BUS STOP PAD AND SIGNAGE
RURAL/SHOULDER CONDITION

GENERAL NOTES

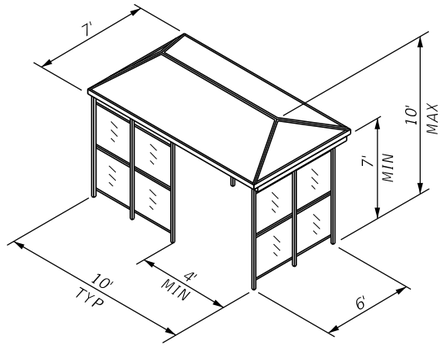
1. FOR A BUS STOP IN RURAL CONDITIONS, THE BUS STOP SIGN SHOULD BE LOCATED OUTSIDE THE CLEAR ZONE OF THE ROAD; WHERE POSSIBLE IT IS DESIRABLE TO BUILD A CONCRETE PAD FOR A SHELTER.
2. THE CONCRETE PAD SHOULD BE LOCATED OUTSIDE THE CLEAR ZONE.
3. SIZE OF THE BUS SHELTER PAD IS ADJUSTABLE DEPENDING ON LOCAL CONDITIONS. COORDINATION IS REQUIRED WITH THE LOCAL TRANSIT AGENCY.
4. SLOPES ARE REPRESENTATIVE AND VARY WITH LOCATION. ADJUST DESIGN TO LOCAL CONDITIONS.

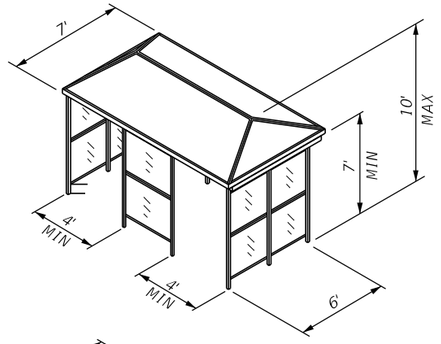
FOR MINIMUM WIDTH OF CLEAR ZONE, REFER TO THE LATEST EDITION OF THE FDOT PLANS PREPARATION MANUAL

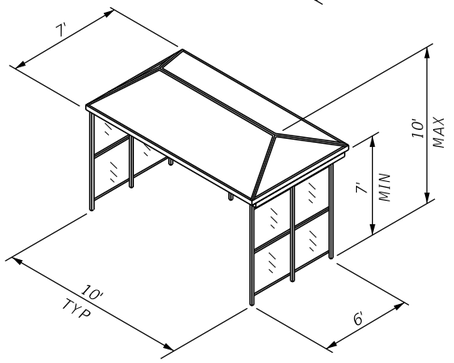
$TIMES
$DATES

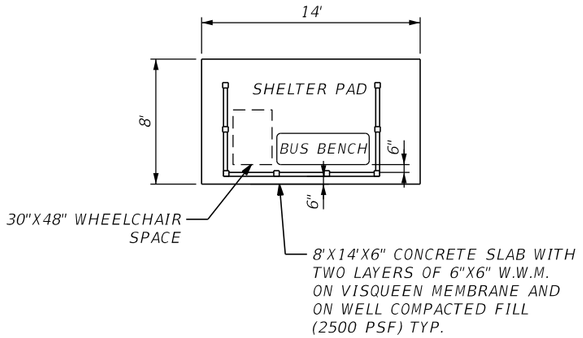
GENERAL NOTES

1. BUS SHELTERS SHALL BE LOCATED A MINIMUM OF 12 FT FROM THE INTERSECTION POINT OF CURVE/TANGENT.
2. BUS SHELTERS SHALL NOT BE LOCATED WITHIN 15 FT OF A FIRE HYDRANT OR DISABLED PARKING SPACE.
3. BICYCLE RACKS SHALL BE CONSIDERED AS PART OF THE SHELTER BASED ON COORDINATION WITH THE TRANSIT AGENCY.
4. A CLEAR AREA OF 2 FT MINIMUM SHOULD BE PROVIDED BEHIND SHELTER FOR MAINTENANCE.
5. BICYCLE RACKS SHALL BE LOCATED SO AS NOT TO BLOCK THE VIEW OF WAITING PASSENGERS.
6. BUS SHELTERS SHALL BE LOCATED AT LEAST 15 FT (DESIRABLE) AND 7 FT (MINIMUM) FROM THE NEAREST UTILITY POLE.
7. STRUCTURAL AND FOUNDATION DESIGN TO BE AS PER ALL APPLICABLE FLORIDA BUILDING CODES.

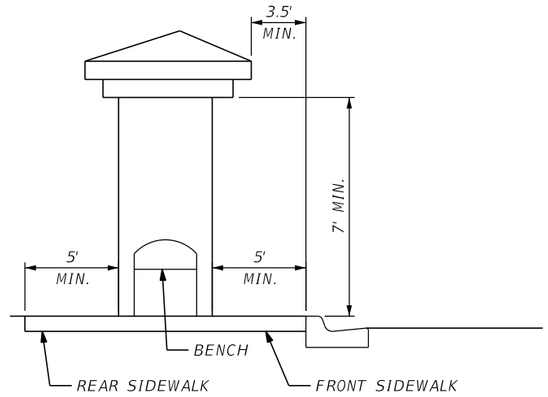




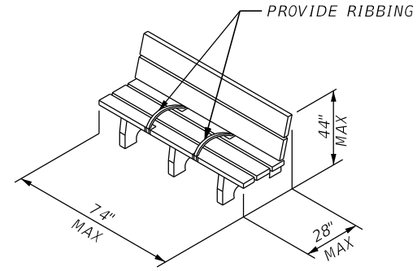




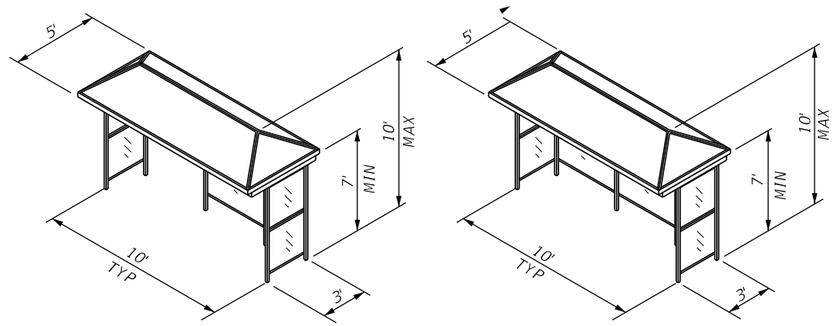
LARGE TYPICAL SHELTERS

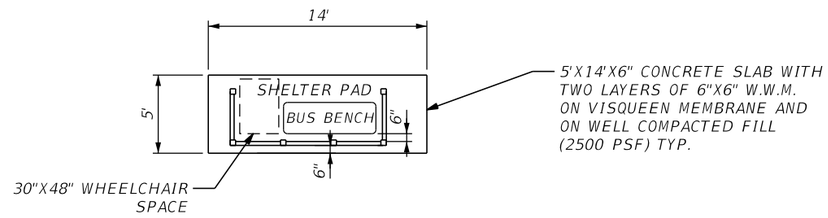


MINIMUM CLEARANCES FOR SHELTERS URBAN CONDITION

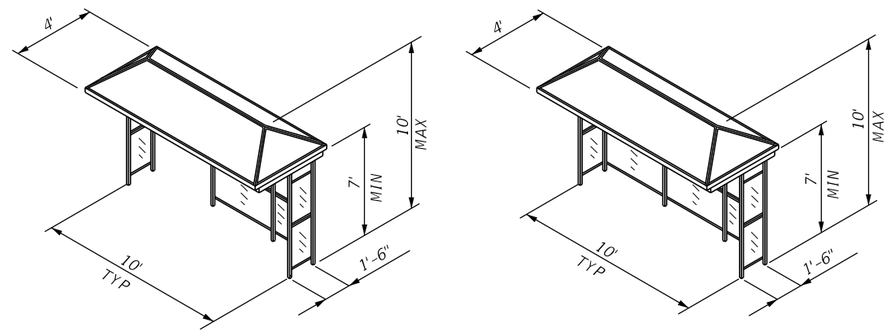


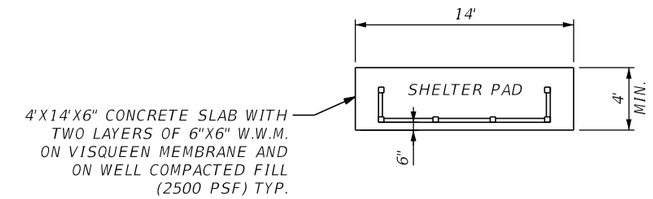
BENCH DIMENSIONS





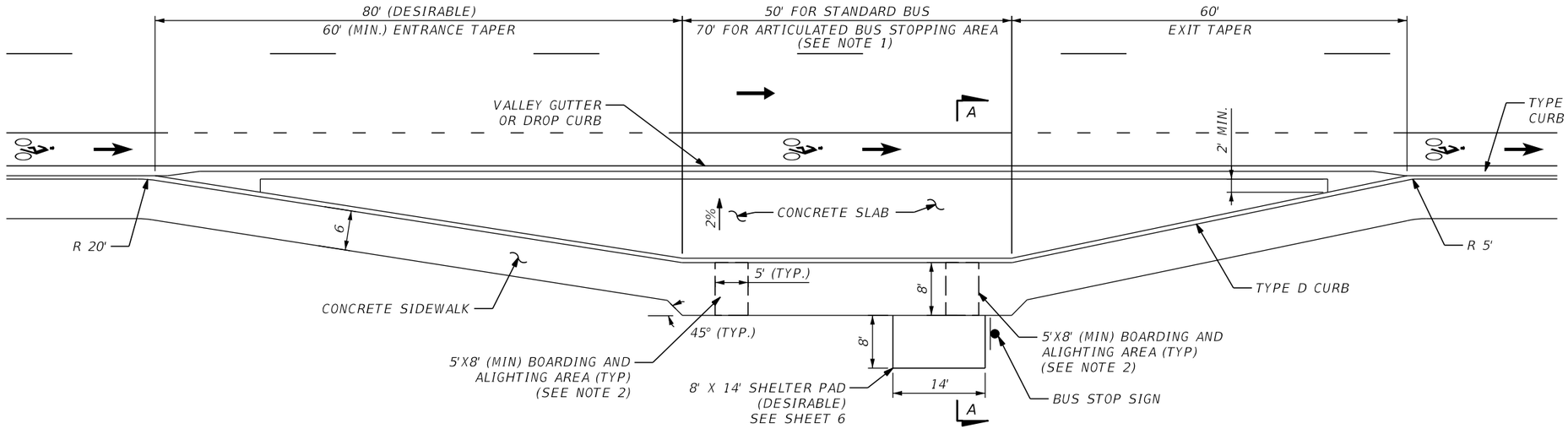
MEDIUM TYPICAL SHELTERS



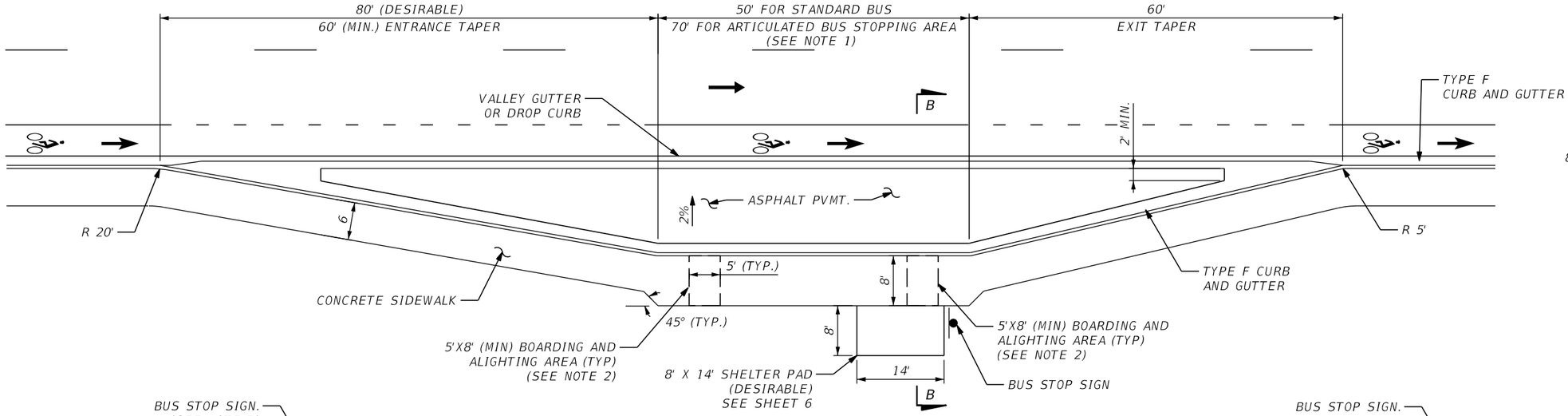


SMALL TYPICAL SHELTERS

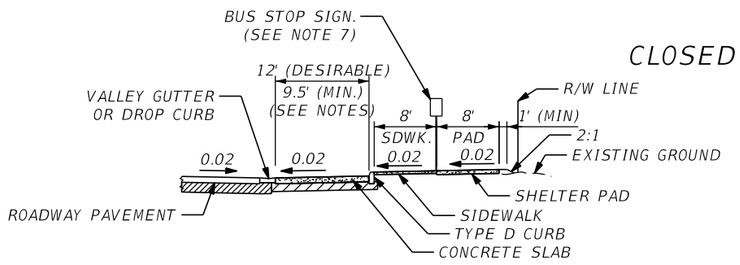
$TIMES
$DATES



CLOSED BUS BAY LAYOUT URBAN/CURB AND GUTTER PLAN
CONCRETE SLAB OPTION

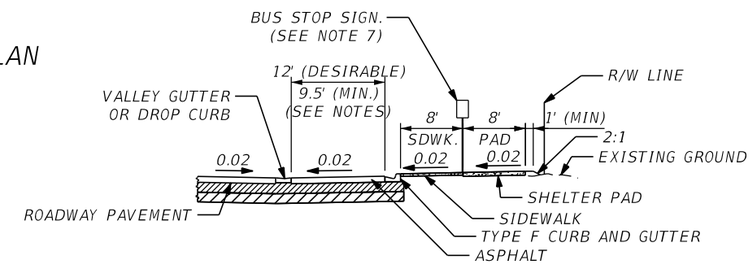


CLOSED BUS BAY LAYOUT URBAN/CURB AND GUTTER PLAN
ASPHALT PAVEMENT OPTION



SECTION A-A

TYPICAL BUS BAY
URBAN/CURB & GUTTER CONDITION
WITH CONCRETE PAVEMENT

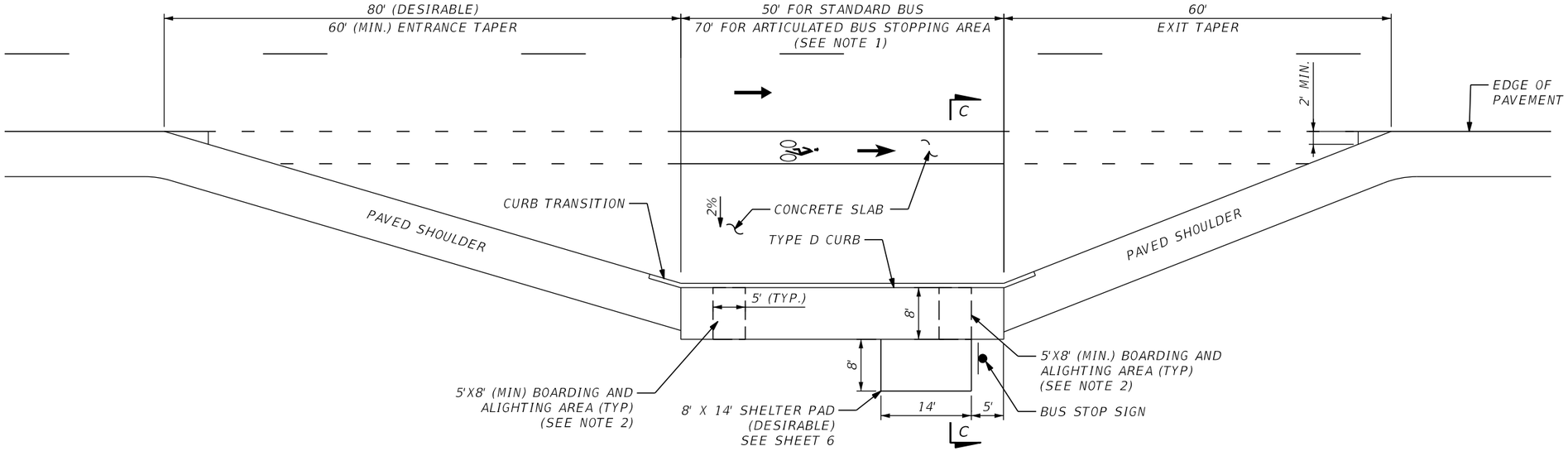


SECTION B-B

TYPICAL BUS BAY
URBAN/CURB & GUTTER CONDITION
WITH ASPHALT PAVEMENT

GENERAL NOTES

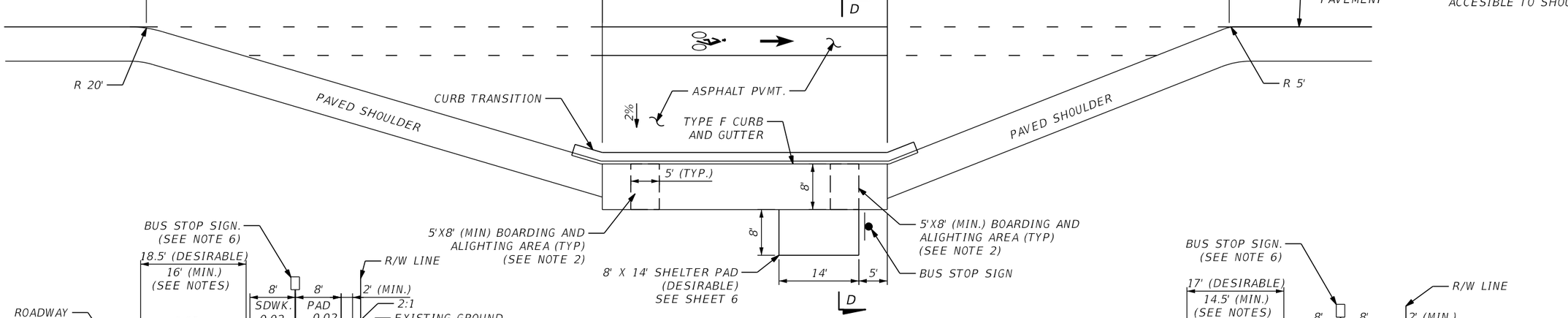
1. DIMENSIONS SHOWN ARE FOR ONE BUS. INCREASE LENGTH OF BUS BAY BY 50' FOR EACH 40-FOOT BUS AND 70' FOR EACH 60-FOOT ARTICULATED BUS EXPECTED TO BE AT THE STOP SIMULTANEOUSLY.
2. WHEN NO BUS SHELTER IS USED, EXTEND THE SIDEWALK TO PROVIDE A BOARDING AND ALIGHTING AREA WITH A MINIMUM CLEAR LENGTH OF 8' AND A MINIMUM CLEAR WIDTH OF 5'.
3. FOR CURB & GUTTER TRANSITION DETAILS, SEE INDEX 300.
4. FOR SHELTER AND SHELTER PAD DETAILS, REFER TO SHEET 6.
5. ALL CONCRETE JOINTS SHALL BE AS PER THE LATEST VERSION OF THE FDOT ROADWAY AND TRAFFIC DESIGN STANDARDS.
6. A MID-BLOCK CROSSWALK CAN BE USED IN LOCATIONS WHERE THERE IS A MAJOR TRANSIT ORIENTED ACTIVITY CENTER OR THE DISTANCE TO THE NEXT INTERSECTION IS GREATER THAN 300 FEET. SIGNALIZATION MAY BE PROVIDED AS PER THE MUTCD.
7. BUS STOP SIGN PANEL MUST BE LOCATED SUCH THAT A MINIMUM CLEARANCE OF 36" IS PROVIDED ON THE SIDEWALK. FOR SIGN DETAILS SEE INDEX 11860.
8. DRAINAGE STRUCTURES ARE NOT TO BE LOCATED WITHIN THE BUS BAY.



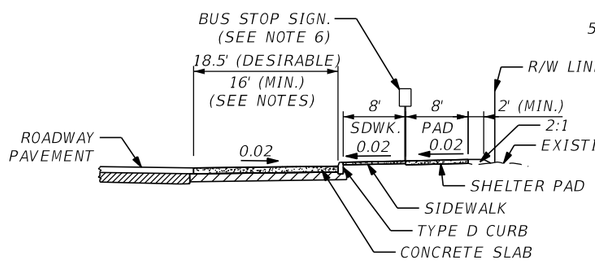
CLOSED BUS BAY LAYOUT RURAL/SHOULDER PLAN CONCRETE SLAB OPTION

GENERAL NOTES

1. DIMENSIONS SHOWN ARE FOR ONE BUS. INCREASE LENGTH OF BUS BAY BY 50' FOR EACH 40-FOOT BUS AND 70' FOR EACH 60-FOOT ARTICULATED BUS EXPECTED TO BE AT THE STOP SIMULTANEOUSLY.
2. WHEN NO BUS SHELTER IS USED, EXTEND THE SIDEWALK TO PROVIDE A BOARDING AND ALIGHTING AREA WITH A MINIMUM CLEAR LENGTH OF 8' AND A MINIMUM CLEAR WIDTH OF 5'. IT IS DESIRABLE TO PROVIDE 8 FT. SIDEWALK CONNECTION TO LOCATION BEING SERVED.
3. FOR CURB & GUTTER TRANSITION DETAILS, SEE INDEX 300.
4. FOR SHELTER AND SHELTER PAD DETAILS, REFER TO SHEET 6.
5. ALL CONCRETE JOINTS SHALL BE AS PER THE LATEST VERSION OF THE FDOT ROADWAY AND TRAFFIC DESIGN STANDARDS.
6. BUS STOP SIGN PANEL MUST BE LOCATED SUCH THAT A MINIMUM CLEARANCE OF 36" IS PROVIDED ON THE SIDEWALK. FOR SIGN DETAILS SEE INDEX 11860.
7. DRAINAGE STRUCTURES ARE NOT TO BE LOCATED WITHIN THE BUS BAY.
8. BUS BAY SIDEWALK SHOULD BE CONNECTED TO EXISTING SIDEWALK OR ACCESIBLE TO SHOULDER.

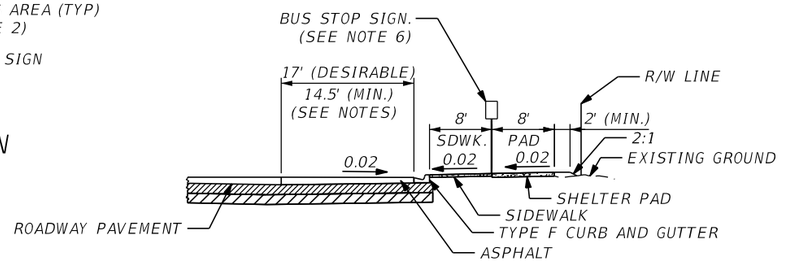


CLOSED BUS BAY LAYOUT RURAL/SHOULDER PLAN ASPHALT PAVEMENT OPTION



SECTION C-C

TYPICAL BUS BAY RURAL/SHOULDER CONDITION WITH CONCRETE PAVEMENT

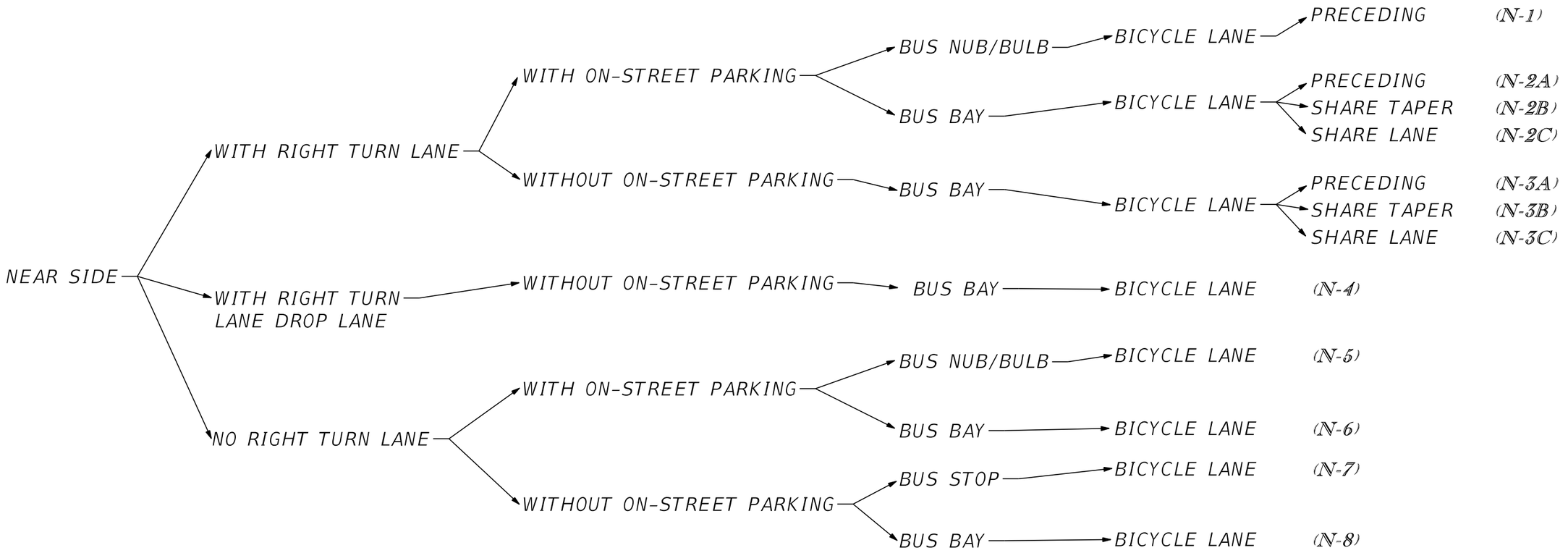


SECTION D-D

TYPICAL BUS BAY RURAL/SHOULDER CONDITION WITH ASPHALT PAVEMENT

$TIME$

$DATE$



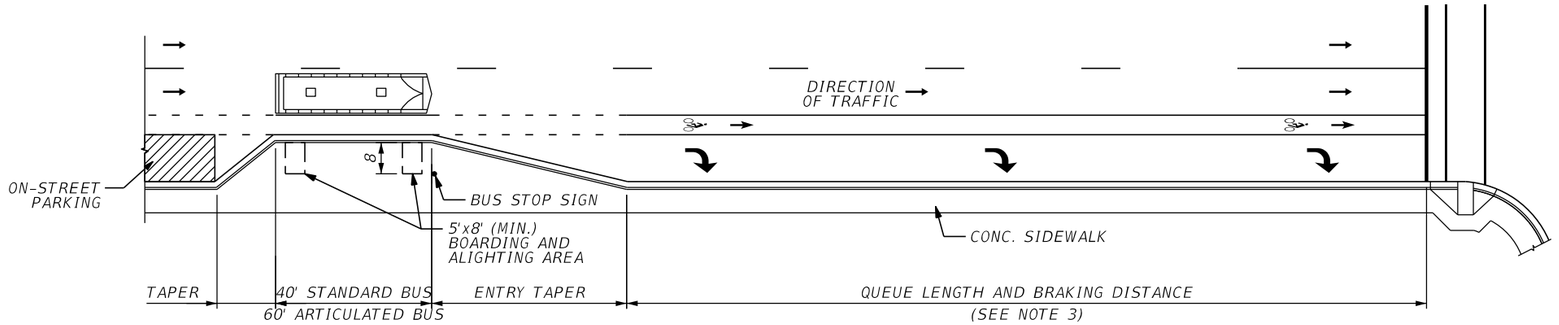
NEAR SIDE BUS FACILITY
DECISION TREE

LEGEND

(N-X) - DETAIL DRAWING

$TIME$

$DATE$



TYPICAL APPLICATION

1. URBAN AREA WHERE PARKING IS CRITICAL.
2. AREAS WITH HIGH VOLUME OF VEHICLES AND/OR PASSENGERS.
3. ON STREET PARALLEL PARKING.
4. RIGHT TURN LANE REQUIRED.

CONDITIONS

1. ARTERIAL DESIGN SPEED ≤40 MPH.

GENERAL NOTES

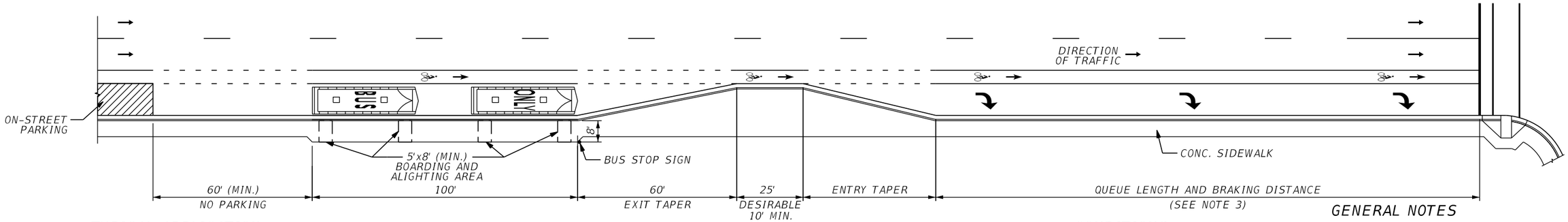
1. BUS STOP AREA SHOWN FOR ONE BUS.
2. CURB RAMP TYPE IS SHOWN ONLY AS AN ILLUSTRATION.
3. ASSUMPTION DESIGN SPEED FOR 40 MPH QUEUE IS 100 FEET PER GREEN BOOK FIGURE 3-13. FOR TURN LANE DETAILS SEE INDEX 301.
4. FOR PAVEMENT MARKINGS SEE INDEX 17346 AND 17347.

N-1

NEAR SIDE NUB/BULB WITH ON STREET PARKING PRECEDING RIGHT TURN LANE

sTIMEs

sDATEs



TYPICAL APPLICATION

1. AREAS WITH HIGH VOLUME OF VEHICLES AND/OR PASSENGERS.
2. ON STREET PARALLEL PARKING.
3. RIGHT TURN LANE REQUIRED.

N-2A

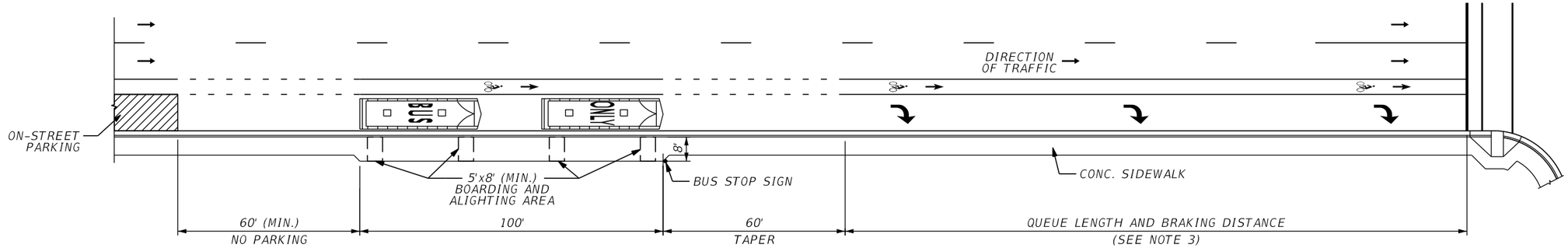
NEAR SIDE BUS BAY WITH ON STREET PARKING PRECEDING RIGHT TURN LANE

CONDITIONS

1. ARTERIAL DESIGN SPEED ≤40 MPH.
2. MOST DESIRABLE DESIGN IF VIABLE.

GENERAL NOTES

1. BUS STOP AREA SHOWN FOR TWO BUSES.
2. CURB RAMP TYPE IS SHOWN ONLY AS AN ILLUSTRATION.
3. ASSUMPTION DESIGN SPEED FOR 40 MPH QUEUE IS 100 FEET PER GREEN BOOK FIGURE 3-13. FOR TURN LANE DETAILS SEE INDEX 301.
4. FOR PAVEMENT MARKINGS SEE INDEX 17346 AND 17347.



TYPICAL APPLICATION

1. AREAS WITH HIGH VOLUME OF VEHICLES AND/OR PASSENGERS.
2. ON STREET PARALLEL PARKING.
3. RIGHT TURN LANE REQUIRED.

N-2B

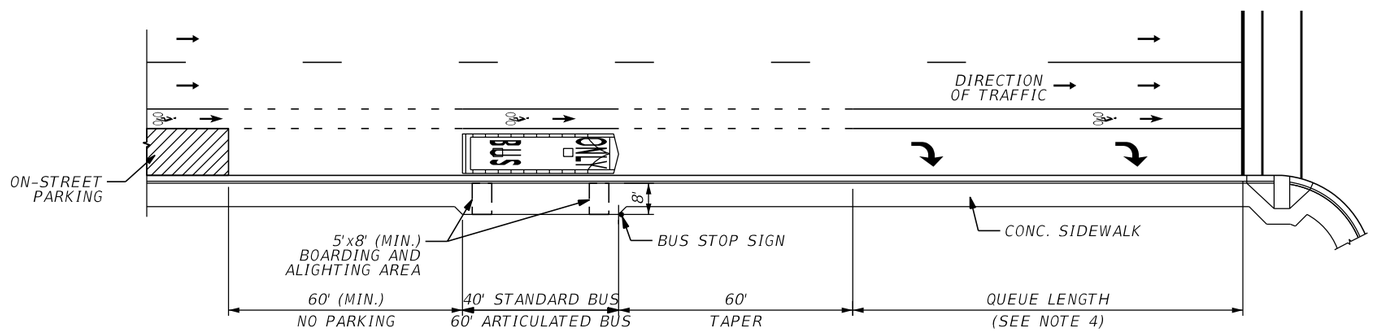
NEAR SIDE BUS BAY WITH ON STREET PARKING PRECEDING RIGHT TURN LANE WITH SHARED TAPER

CONDITIONS

1. ARTERIAL DESIGN SPEED ≤40 MPH.
2. SECOND MOST DESIRABLE DESIGN IF VIABLE.

GENERAL NOTES

1. BUS STOP AREA SHOWN FOR TWO BUSES.
2. CURB RAMP TYPE IS SHOWN ONLY AS AN ILLUSTRATION.
3. ASSUMPTION DESIGN SPEED FOR 40 MPH QUEUE IS 100 FEET PER GREEN BOOK FIGURE 3-13. FOR TURN LANE DETAILS SEE INDEX 301.
4. FOR PAVEMENT MARKINGS SEE INDEX 17346 AND 17347.



TYPICAL APPLICATION

1. AREAS WITH HIGH VOLUME OF VEHICLES AND/OR PASSENGERS.
2. ON STREET PARALLEL PARKING.
3. RIGHT TURN LANE REQUIRED.

N-2C

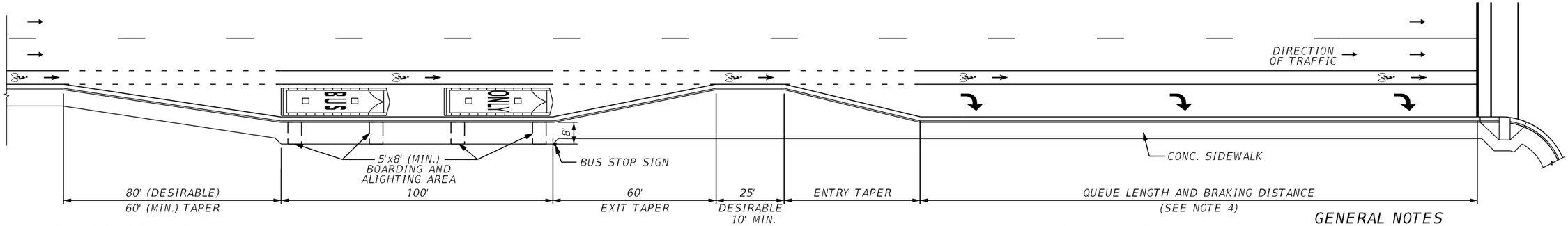
NEAR SIDE BUS BAY WITH ON STREET PARKING SHARED WITH RIGHT TURN LANE

CONDITIONS

1. ARTERIAL DESIGN SPEED ≤40 MPH.
2. MINIMUM DESIGN FOR CONSTRAINED SITES.

GENERAL NOTES

1. BUS STOP AREA SHOWN FOR ONE BUS.
2. SUCCEEDING QUEUE BYPASS TO BE CONSIDERED.
3. CURB RAMP TYPE IS SHOWN ONLY AS AN ILLUSTRATION.
4. ASSUMPTION DESIGN SPEED FOR 40 MPH QUEUE IS 100 FEET PER GREEN BOOK FIGURE 3-13. FOR TURN LANE DETAILS SEE INDEX 301.
5. FOR PAVEMENT MARKINGS SEE INDEX 17346 AND 17347.



TYPICAL APPLICATION

1. AREAS WITH HIGH VOLUME OF VEHICLES AND/OR PASSENGERS.
2. RIGHT TURN LANE REQUIRED.

N-3A

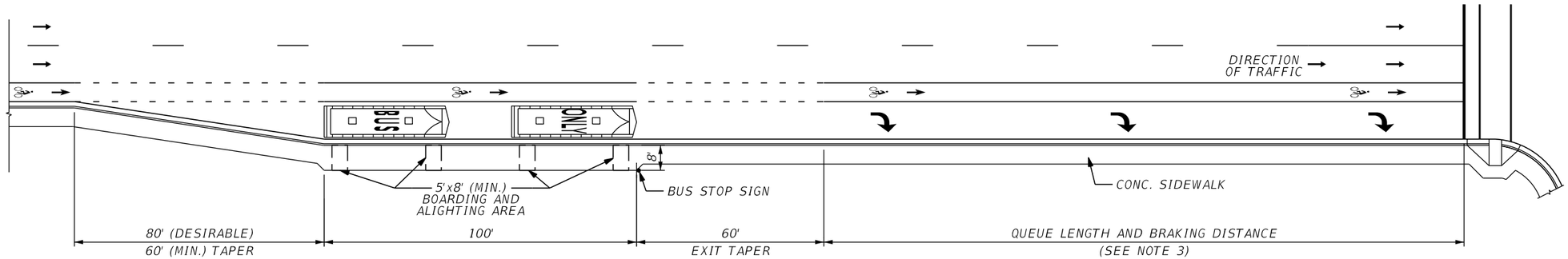
NEAR SIDE BUS BAY
PRECEDING RIGHT TURN LANE

CONDITIONS

1. ARTERIAL DESIGN SPEED ≤45 MPH.
2. MOST DESIRABLE DESIGN IF VIABLE.

GENERAL NOTES

1. FOR BUS BAY DETAILS SEE SHEET 7 & 8.
2. BUS STOP AREA SHOWN FOR TWO BUSES.
3. CURB RAMP TYPE IS SHOWN ONLY AS AN ILLUSTRATION.
4. ASSUMPTION DESIGN SPEED FOR 40 MPH QUEUE IS 100 FEET PER GREEN BOOK FIGURE 3-13. FOR TURN LANE DETAILS SEE INDEX 301.
5. FOR PAVEMENT MARKINGS SEE INDEX 17346 AND 17347.



TYPICAL APPLICATION

1. AREAS WITH HIGH VOLUME OF VEHICLES AND/OR PASSENGERS.
2. RIGHT TURN LANE REQUIRED.

N-3B

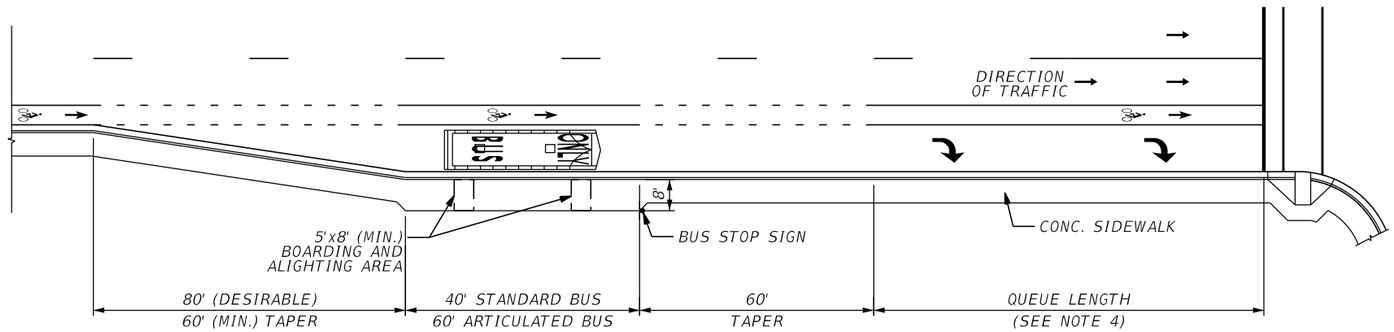
NEAR SIDE BUS BAY PRECEDING
RIGHT TURN LANE WITH SHARED TAPER

CONDITIONS

1. ARTERIAL DESIGN SPEED ≤45 MPH.
2. SECOND MOST DESIRABLE DESIGN IF VIABLE.

GENERAL NOTES

1. BUS STOP AREA SHOWN FOR TWO BUSES.
2. CURB RAMP TYPE IS SHOWN ONLY AS AN ILLUSTRATION.
3. ASSUMPTION DESIGN SPEED FOR 40 MPH QUEUE IS 100 FEET PER GREEN BOOK FIGURE 3-13. FOR TURN LANE DETAILS SEE INDEX 301.
4. FOR PAVEMENT MARKINGS SEE INDEX 17346 AND 17347.



TYPICAL APPLICATION

1. AREAS WITH HIGH VOLUME OF VEHICLES AND/OR PASSENGERS.
2. RIGHT TURN LANE REQUIRED.

N-3C

NEAR SIDE BUS BAY
SHARED WITH RIGHT TURN LANE

CONDITIONS

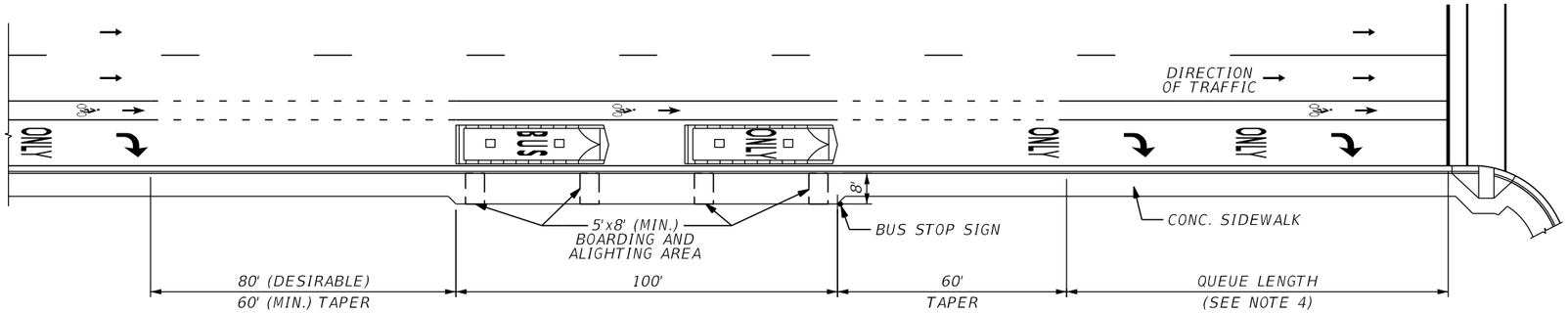
1. ARTERIAL DESIGN SPEED ≤45 MPH.
2. MINIMUM DESIGN FOR CONSTRAINED SITES.

GENERAL NOTES

1. BUS STOP AREA SHOWN FOR ONE BUS.
2. SUCCEEDING QUEUE BYPASS TO BE CONSIDERED.
3. CURB RAMP TYPE IS SHOWN ONLY AS AN ILLUSTRATION.
4. ASSUMPTION DESIGN SPEED FOR 40 MPH QUEUE IS 100 FEET PER GREEN BOOK FIGURE 3-13. FOR TURN LANE DETAILS SEE INDEX 301.
5. FOR PAVEMENT MARKINGS SEE INDEX 17346 AND 17347.

sTIMES

sDATES



TYPICAL APPLICATION

1. RIGHT TURN LANE DROP LANE.

CONDITIONS

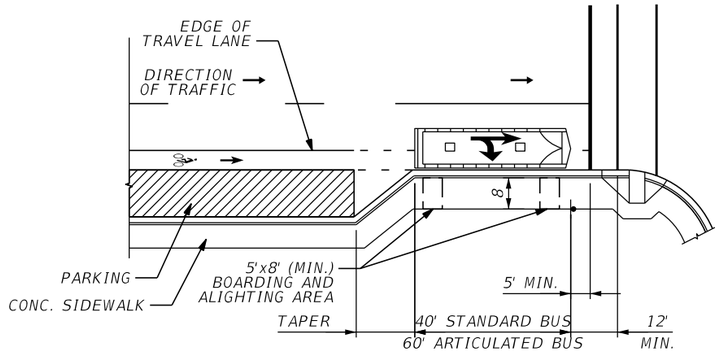
1. ARTERIAL DESIGN SPEED ≤40 MPH.

GENERAL NOTES

1. BUS STOP AREA SHOWN FOR TWO BUSES.
2. SUCCEEDING QUEUE BYPASS TO BE CONSIDERED.
3. CURB RAMP TYPE IS SHOWN ONLY AS AN ILLUSTRATION.
4. ASSUMPTION DESIGN SPEED FOR 40 MPH QUEUE IS 100 FEET PER GREEN BOOK FIGURE 3-13. FOR TURN LANE DETAILS SEE INDEX 301.
5. FOR PAVEMENT MARKINGS SEE INDEX 17346 AND 17347.

N-4

NEAR SIDE BUS BAY WITH PRECEDING LANE DROP RIGHT TURN LANE



TYPICAL APPLICATION

1. URBAN AREA WHERE PARKING IS CRITICAL.
2. AREAS WITH HIGH VOLUME OF PEDESTRIANS ON SIDEWALK SUCH AS CENTRAL BUSINESS DISTRICTS.
3. ON STREET PARALLEL PARKING.
4. NO RIGHT TURN LANE.

CONDITIONS

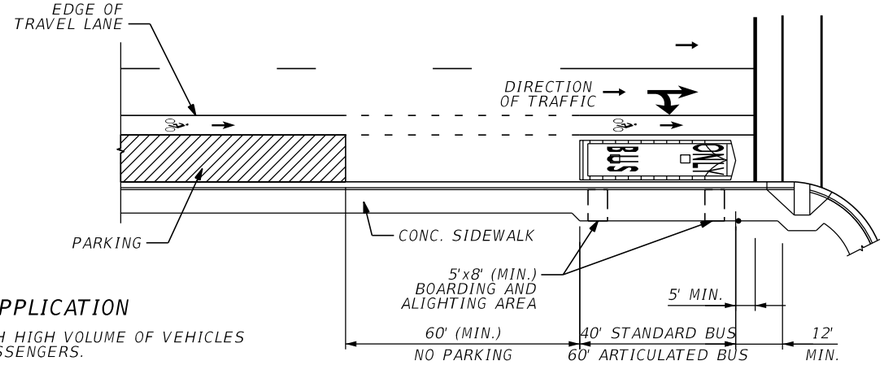
1. ARTERIAL DESIGN SPEED ≤40 MPH.
2. NO RIGHT TURN BAY ON NEAR SIDE OF INTERSECTION.

GENERAL NOTES

1. BUS STOP AREA SHOWN FOR ONE BUS.
2. CURB RAMP TYPE IS SHOWN ONLY AS AN ILLUSTRATION.
3. FOR PAVEMENT MARKINGS SEE INDEX 17346 AND 17347.

N-5

NEAR SIDE NUB/BULB WITH ON-STREET PARKING



TYPICAL APPLICATION

1. AREAS WITH HIGH VOLUME OF VEHICLES AND/OR PASSENGERS.
2. NO RIGHT TURN LANE.
3. ON STREET PARALLEL PARKING.

CONDITIONS

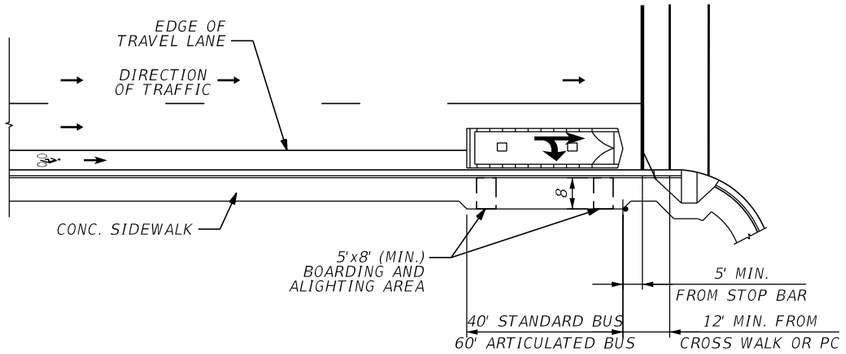
1. ARTERIAL DESIGN SPEED ≤40 MPH.

GENERAL NOTES

1. BUS STOP AREA SHOWN FOR ONE BUS.
2. CURB RAMP TYPE IS SHOWN ONLY AS AN ILLUSTRATION.
3. FOR PAVEMENT MARKINGS SEE INDEX 17346 AND 17347.

N-6

NEAR SIDE BUS BAY WITH ON-STREET PARKING



TYPICAL APPLICATION

1. AREAS WITH LOWER VOLUME OF VEHICLES AND/OR PASSENGERS.
2. NO RIGHT TURN LANE.

CONDITIONS

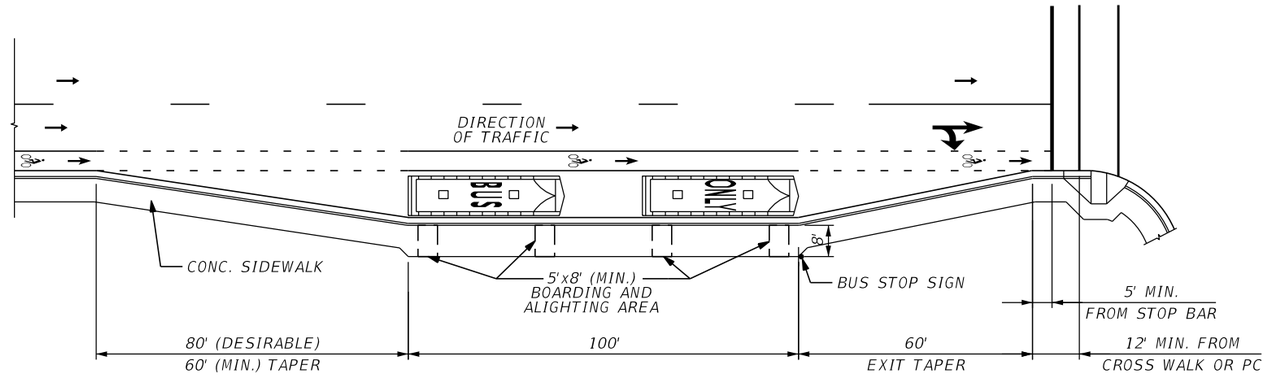
1. ARTERIAL DESIGN SPEED ≤45 MPH.

GENERAL NOTES

1. BUS STOP AREA SHOWN FOR ONE BUS.
2. CURB RAMP TYPE IS SHOWN ONLY AS AN ILLUSTRATION.
3. FOR PAVEMENT MARKINGS SEE INDEX 17346 AND 17347.

N-7

NEAR SIDE BUS STOP



TYPICAL APPLICATION

1. AREAS WITH HIGH VOLUME OF VEHICLES AND/OR PASSENGERS.
2. NO RIGHT TURN LANE.

CONDITIONS

1. ARTERIAL DESIGN SPEED ≤45 MPH.

GENERAL NOTES

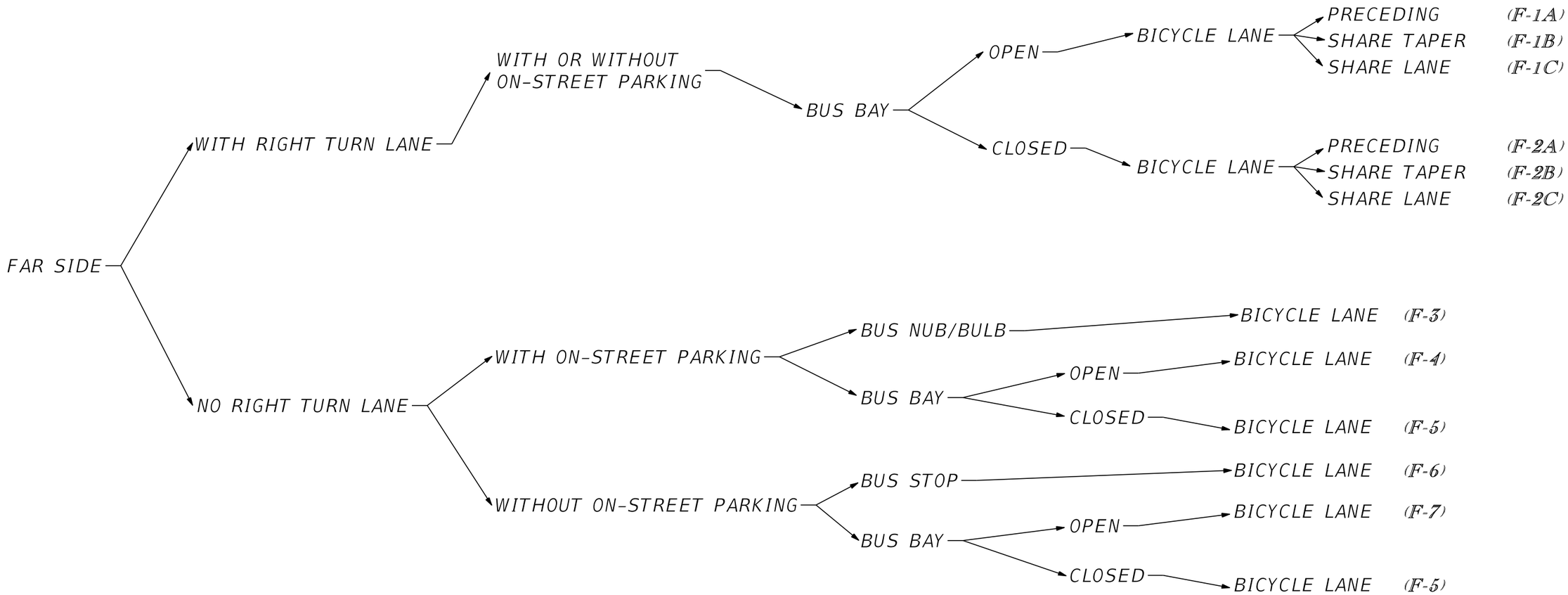
1. FOR BUS BAY DETAILS SEE SHEET 7 & 8.
2. BUS STOP AREA SHOWN FOR TWO BUSES.
3. CURB RAMP TYPE IS SHOWN ONLY AS AN ILLUSTRATION.
4. FOR PAVEMENT MARKINGS SEE INDEX 17346 AND 17347.

N-8

NEAR SIDE BUS BAY

$TIMES

$DATES



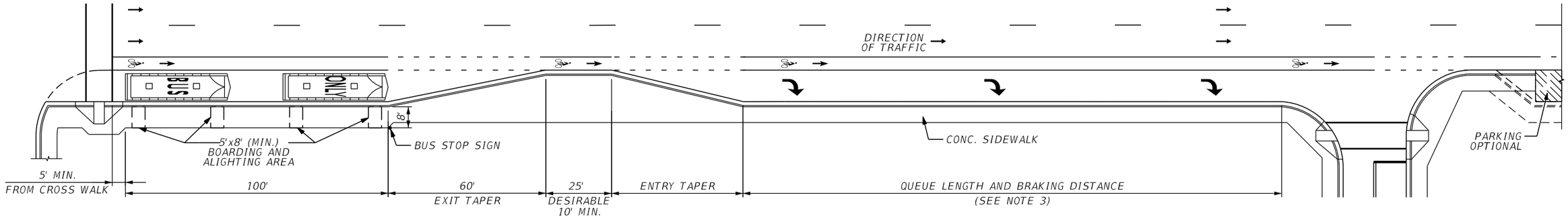
FAR SIDE BUS FACILITY
DECISION TREE

LEGEND

(F-X) – DETAIL DRAWING

$TIME$

$DATE$



TYPICAL APPLICATION

1. AREAS WITH HIGH VOLUME OF VEHICLES AND/OR PASSENGERS.
2. RIGHT TURN BAY AT NEAR SIDE AND FAR SIDE OF INTERSECTION.

F-1A

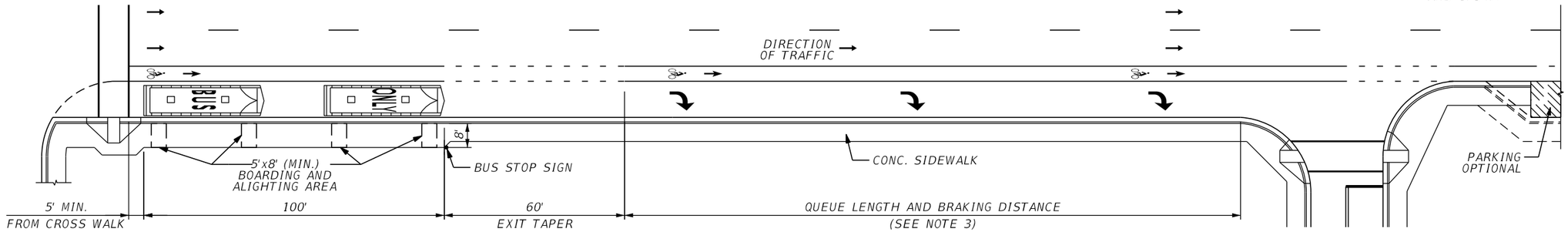
FAR SIDE OPEN BUS BAY PRECEDING RIGHT TURN LANE

CONDITIONS

1. ARTERIAL DESIGN SPEED ≤45 MPH.
2. OPEN BUS BAY DESIGN RECOMMENDED FOR 4 LANE OR LESS ROADWAY.
3. MOST DESIRABLE DESIGN IF VIABLE.

GENERAL NOTES

1. BUS STOP AREA SHOWN FOR TWO BUSES.
2. CURB RAMP TYPE IS SHOWN ONLY AS AN ILLUSTRATION.
3. ASSUMPTION DESIGN SPEED FOR 40 MPH QUEUE IS 100 FEET PER GREEN BOOK FIGURE 3-13. FOR TURN LANE DETAILS SEE INDEX 301.
4. FOR PAVEMENT MARKINGS SEE INDEX 17346 AND 17347.



TYPICAL APPLICATION

1. AREAS WITH HIGH VOLUME OF VEHICLES AND/OR PASSENGERS.
2. RIGHT TURN BAY AT NEAR SIDE AND FAR SIDE OF INTERSECTION.

F-1B

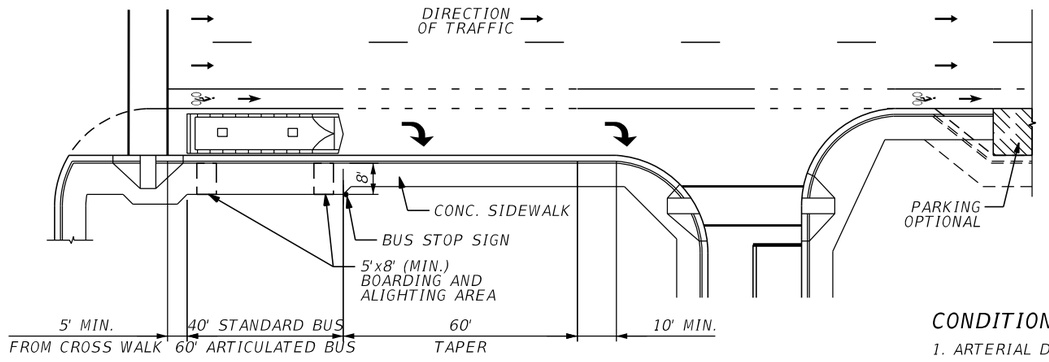
FAR SIDE OPEN BUS BAY PRECEDING RIGHT TURN LANE WITH SHARED TAPER

CONDITIONS

1. ARTERIAL DESIGN SPEED ≤45 MPH.
2. OPEN BUS BAY DESIGN RECOMMENDED FOR 4 LANE OR LESS ROADWAY.
3. SECOND MOST DESIRABLE DESIGN IF VIABLE.

GENERAL NOTES

1. BUS STOP AREA SHOWN FOR TWO BUSES.
2. CURB RAMP TYPE IS SHOWN ONLY AS AN ILLUSTRATION.
3. ASSUMPTION DESIGN SPEED FOR 40 MPH QUEUE IS 100 FEET PER GREEN BOOK FIGURE 3-13. FOR TURN LANE DETAILS SEE INDEX 301.
4. FOR PAVEMENT MARKINGS SEE INDEX 17346 AND 17347.



TYPICAL APPLICATION

1. AREAS WITH HIGH VOLUME OF VEHICLES AND/OR PASSENGERS.
2. RIGHT TURN BAY AT NEAR SIDE AND FAR SIDE OF INTERSECTION.

F-1C

FAR SIDE OPEN BUS BAY SHARED WITH RIGHT TURN LANE

CONDITIONS

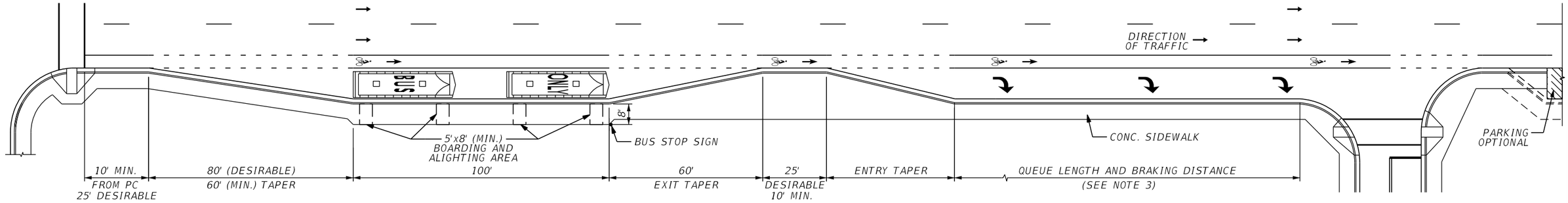
1. ARTERIAL DESIGN SPEED ≤45 MPH.
2. OPEN BUS BAY DESIGN RECOMMENDED FOR 4 LANE OR LESS ROADWAY.
3. MINIMUM DESIGN FOR CONSTRAINED SITES.

GENERAL NOTES

1. BUS STOP AREA SHOWN FOR ONE BUS.
2. CURB RAMP TYPE IS SHOWN ONLY AS AN ILLUSTRATION.
3. ASSUMPTION DESIGN SPEED FOR 40 MPH QUEUE IS 100 FEET PER GREEN BOOK FIGURE 3-13. FOR TURN LANE DETAILS SEE INDEX 301.
4. FOR PAVEMENT MARKINGS SEE INDEX 17346 AND 17347.

$TIMES

$DATES



TYPICAL APPLICATION

1. AREAS WITH HIGH VOLUME OF VEHICLES AND/OR PASSENGERS.
2. RIGHT TURN BAY AT FAR SIDE OF INTERSECTION.

F-2A

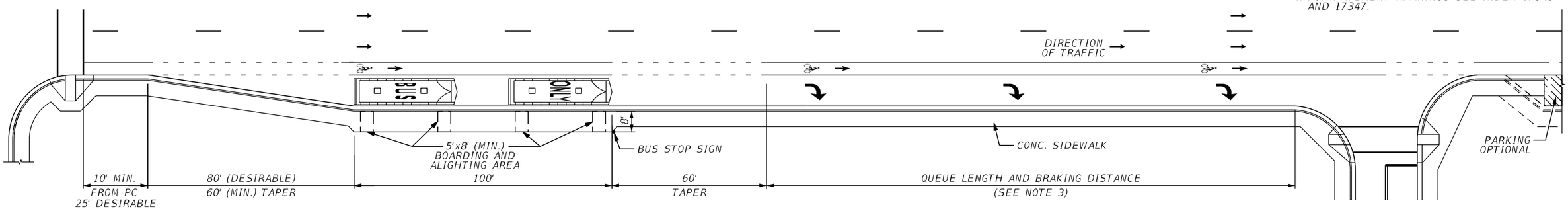
FAR SIDE CLOSED BUS BAY
PRECEDING RIGHT TURN LANE

CONDITIONS

1. ARTERIAL DESIGN SPEED ≤45 MPH.
2. MOST DESIRABLE DESIGN IF VIABLE.

GENERAL NOTES

1. FOR BUS BAY DETAILS SEE SHEET 7 & 8.
2. CURB RAMP TYPE IS SHOWN ONLY AS AN ILLUSTRATION.
3. ASSUMPTION DESIGN SPEED FOR 40 MPH QUEUE IS 100 FEET PER GREEN BOOK FIGURE 3-13. FOR TURN LANE DETAILS SEE INDEX 301.
4. FOR PAVEMENT MARKINGS SEE INDEX 17346 AND 17347.



TYPICAL APPLICATION

1. AREAS WITH HIGH VOLUME OF VEHICLES AND/OR PASSENGERS.
2. RIGHT TURN BAY AT FAR SIDE OF INTERSECTION.

F-2B

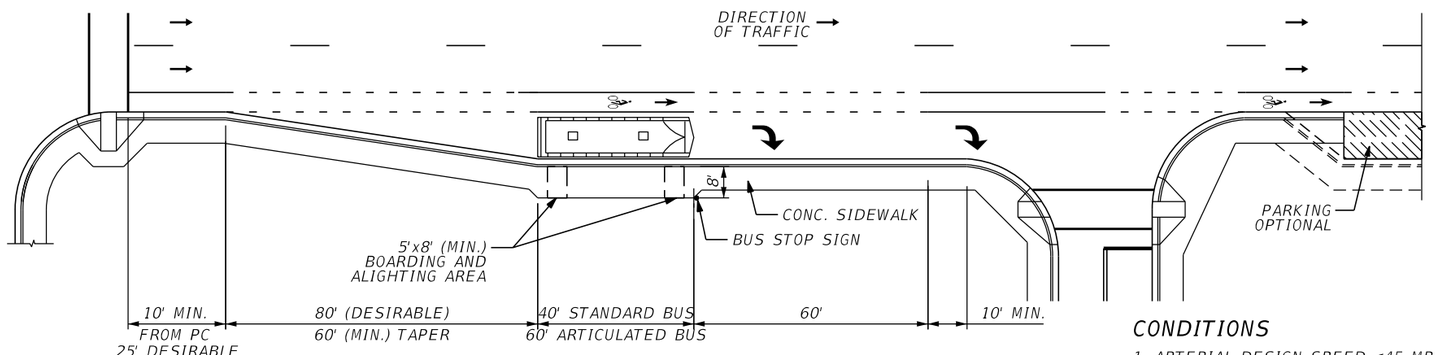
FAR SIDE CLOSED BUS BAY PRECEDING
RIGHT TURN LANE WITH SHARED TAPER

CONDITIONS

1. ARTERIAL DESIGN SPEED ≤45 MPH.
2. SECOND MOST DESIRABLE DESIGN IF VIABLE.

GENERAL NOTES

1. BUS STOP AREA SHOWN FOR TWO BUSES.
2. CURB RAMP TYPE IS SHOWN ONLY AS AN ILLUSTRATION.
3. ASSUMPTION DESIGN SPEED FOR 40 MPH QUEUE IS 100 FEET PER GREEN BOOK FIGURE 3-13. FOR TURN LANE DETAILS SEE INDEX 301.
4. FOR PAVEMENT MARKINGS SEE INDEX 17346 AND 17347.



TYPICAL APPLICATION

1. AREAS WITH HIGH VOLUME OF VEHICLES AND/OR PASSENGERS.
2. RIGHT TURN BAY AT FAR SIDE OF INTERSECTION.

F-2C

FAR SIDE CLOSED BUS BAY
SHARED WITH RIGHT TURN LANE

CONDITIONS

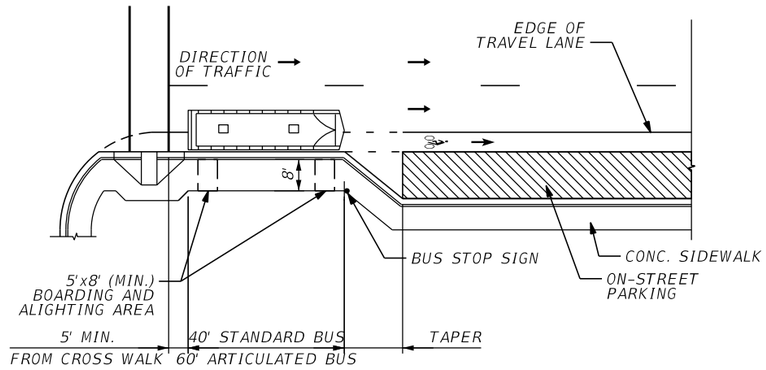
1. ARTERIAL DESIGN SPEED ≤45 MPH.
2. MINIMUM DESIGN FOR CONSTRAINED SITES.

GENERAL NOTES

1. BUS STOP AREA SHOWN FOR ONE BUS.
2. CURB RAMP TYPE IS SHOWN ONLY AS AN ILLUSTRATION.
3. ASSUMPTION DESIGN SPEED FOR 40 MPH QUEUE IS 100 FEET PER GREEN BOOK FIGURE 3-13. FOR TURN LANE DETAILS SEE INDEX 301.
4. FOR PAVEMENT MARKINGS SEE INDEX 17346 AND 17347.

STIMES

SDATES



TYPICAL APPLICATION

1. URBAN AREA WHERE PARKING IS CRITICAL.
2. AREAS WITH HIGH VOLUME OF PEDESTRIANS ON SIDEWALK.
3. ON STREET PARALLEL PARKING.

CONDITIONS

1. ARTERIAL DESIGN SPEED ≤40 MPH.

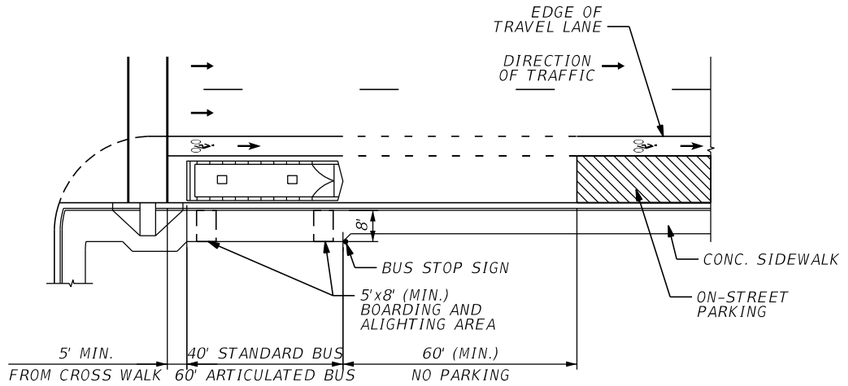
GENERAL NOTES

1. BUS STOP AREA SHOWN FOR ONE BUS.
2. CURB RAMP TYPE IS SHOWN ONLY AS AN ILLUSTRATION.
3. FOR PAVEMENT MARKINGS SEE INDEX 17346 AND 17347.

F-3

FAR SIDE NUB/BULB
WITH ON-STREET PARKING

$DATES
$TIMES



TYPICAL APPLICATION

1. AREAS WITH HIGH VOLUME OF VEHICLES AND/OR PASSENGERS.
2. RIGHT TURN BAY AT NEAR SIDE OF INTERSECTION, AND NO FAR SIDE RIGHT TURN LANE.
3. ON STREET PARALLEL PARKING.

F-4

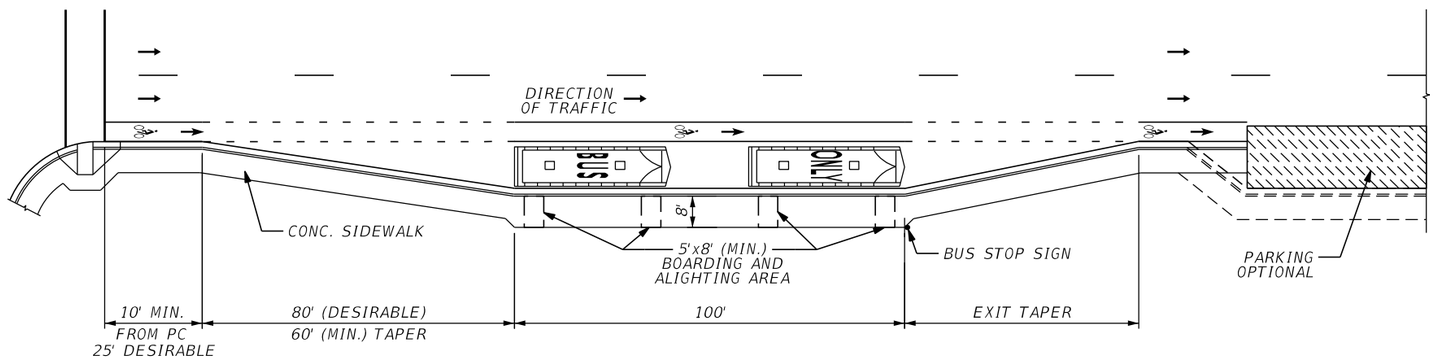
FAR SIDE OPEN BUS BAY
WITH ON STREET PARKING

CONDITIONS

1. ARTERIAL DESIGN SPEED ≤40 MPH.
2. OPEN BUS BAY DESIGN RECOMMENDED FOR 4 LANE OR LESS DIVIDED ROADWAYS.

GENERAL NOTES

1. BUS STOP AREA SHOWN FOR ONE BUS.
2. CURB RAMP TYPE IS SHOWN ONLY AS AN ILLUSTRATION.
3. FOR PAVEMENT MARKINGS SEE INDEX 17346 AND 17347.



TYPICAL APPLICATION

1. AREAS WITH HIGHER VOLUME OF VEHICLES AND/OR PASSENGERS.
2. NO FAR SIDE RIGHT TURN LANE.

F-5

FAR SIDE CLOSED BUS BAY

CONDITIONS

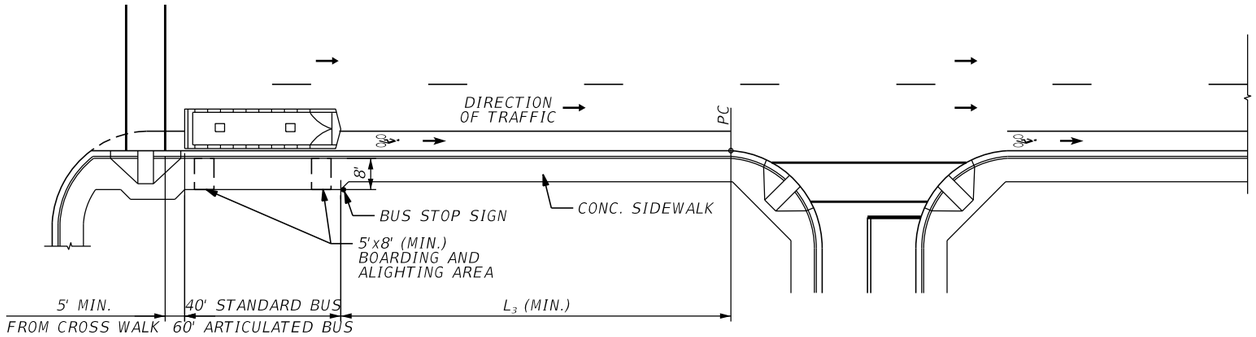
1. ARTERIAL DESIGN SPEED ≤45 MPH.

GENERAL NOTES

1. FOR BUS BAY DETAILS SEE SHEET 7 & 8.
2. BUS STOP AREA SHOWN FOR TWO BUSES.
3. CURB RAMP TYPE IS SHOWN ONLY AS AN ILLUSTRATION.
4. FOR PAVEMENT MARKINGS SEE INDEX 17346 AND 17347.

$TIMES
$DATE$

FAR SIDE BUS STOP PLACEMENT	
DESIGN SPEED (MPH)	L_3
35	75'
40	75'
45	100'
50	135'



TYPICAL APPLICATION

1. AREAS WITH LOWER VOLUME OF VEHICLES AND/OR PASSENGERS.
2. NO FAR SIDE RIGHT TURN LANE.

CONDITIONS

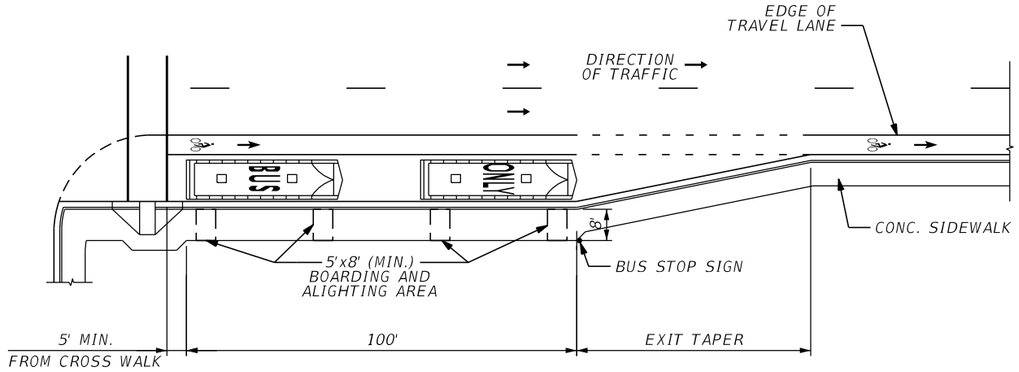
1. ARTERIAL DESIGN SPEED ≤45 MPH.

GENERAL NOTES

1. BUS STOP AREA SHOWN FOR ONE BUS.
2. CURB RAMP TYPE IS SHOWN ONLY AS AN ILLUSTRATION.
3. FOR PAVEMENT MARKINGS SEE INDEX 17346 AND 17347.

F-6

FAR SIDE BUS STOP
WITHOUT RIGHT TURN LANE



TYPICAL APPLICATION

1. AREAS WITH HIGHER VOLUME OF VEHICLES AND/OR PASSENGERS.
2. RIGHT TURN BAY AT NEAR SIDE OF INTERSECTION AND NO RIGHT TURN LANE AT FAR SIDE OF INTERSECTION.

CONDITIONS

1. ARTERIAL DESIGN SPEED ≤45 MPH.
2. OPEN BUS BAY DESIGN RECOMMENDED FOR 4 LANE OR LESS DIVIDED ROADWAYS.

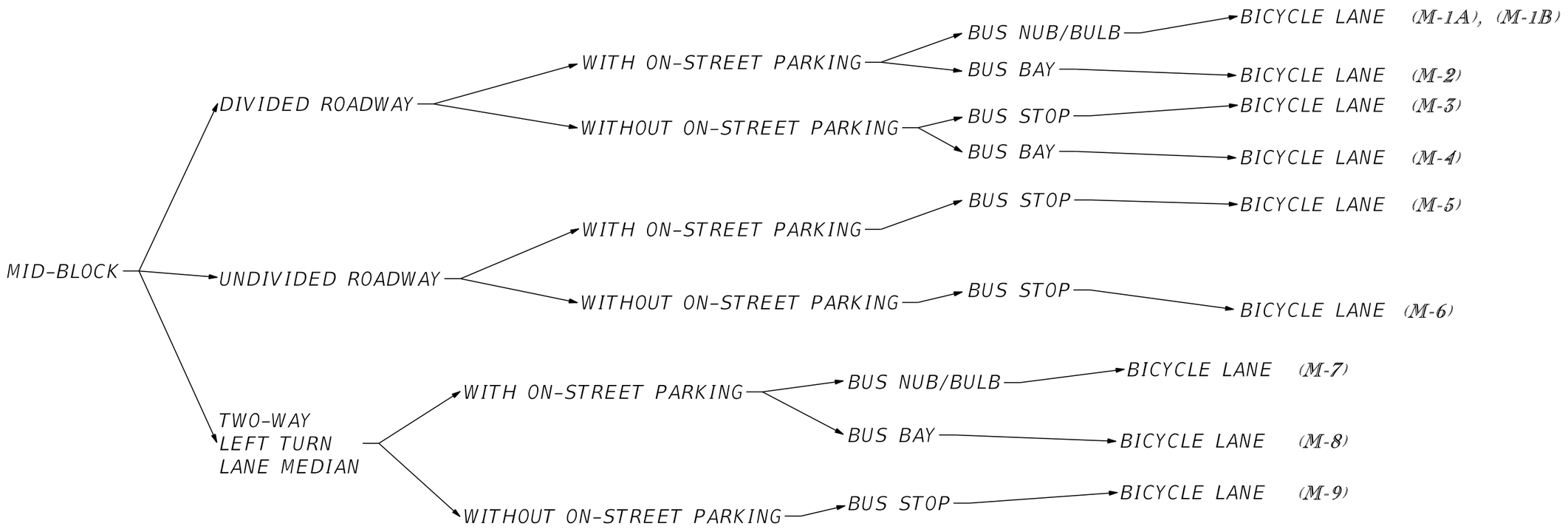
GENERAL NOTES

1. BUS STOP AREA SHOWN FOR TWO BUSES.
2. CURB RAMP TYPE IS SHOWN ONLY AS AN ILLUSTRATION.
3. FOR PAVEMENT MARKINGS SEE INDEX 17346 AND 17347.

F-7

FAR SIDE OPEN BUS BAY

$TIMES
$DATES



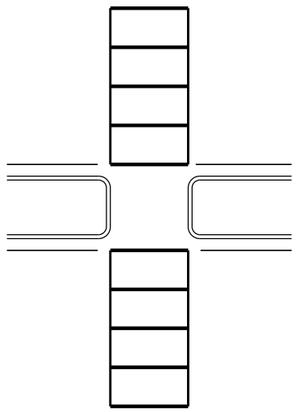
MID-BLOCK BUS FACILITY
DECISION TREE

LEGEND

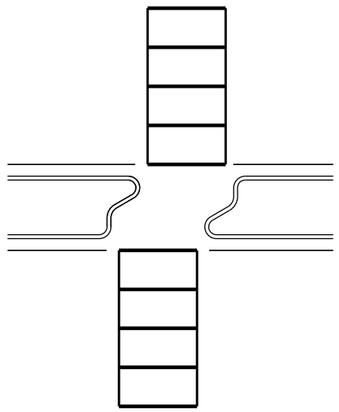
(M-X) - DETAIL DRAWING

$TIMES

$DATE$



MEDIAN OPTION 1
(CROSSWALK WITH STRAIGHT MEDIAN OPENING)



MEDIAN OPTION 2
(CROSSWALK WITH ANGLED MEDIAN OPENING)

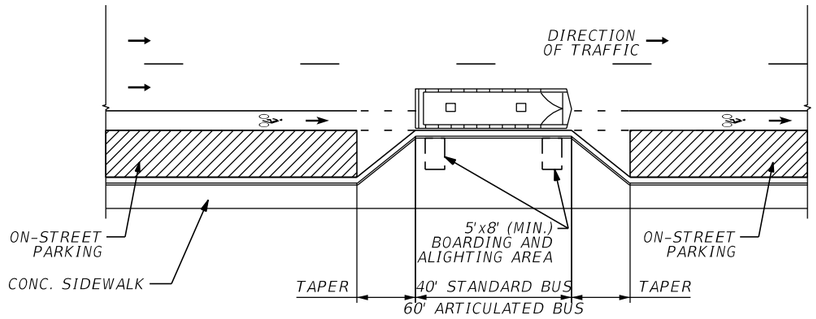
CROSSWALK CLEAR ZONE REQUIREMENTS							
		DIMENSION DL (FT)					
DESIGN SPEED (MPH)	**S.S.D. (FT)	MEDIAN/SIDEWALK WIDTH					
		2'	4'	6'	8'	10'	≥ 12'
30	200	100	135	150	160	165	170
35	250	125	165	190	200	210	215
40	300	150	200	225	240	250	260
45	350	175	235	265	280	290	300

** S.S.D. – STOPPING SIGHT DISTANCE

GENERAL NOTES

1. USE THE SAME PARAMETERS FOR ROADWAYS WITH MORE THAN FOUR LANES.
2. INSTALL ADVANCE WARNING SIGNS AS PER THE MUTCD AND FDOT STANDARDS TO WARN MOTORISTS OF ONCOMING CROSSWALK.
3. CLEAR AREA SHOULD BE FREE OF ALL FIXED OBJECTS SUCH AS LIGHT/UTILITY POLES, SIGNAL EQUIPMENT, TREES, VEGETATION, STREET FURNITURE, ETC. THAT WOULD OBSTRUCT THE VIEW OF PEDESTRIANS. CROSSING WARNING SIGNS ARE EXCLUDED FROM THE CLEAR AREA REQUIREMENTS.
4. REFER TO FDOT INDEX 17346 OF THE LATEST EDITION OF THE FDOT ROADWAY AND TRAFFIC DESIGN STANDARDS FOR TRAFFIC SIGN AND CROSSING SIGNAL LOCATION.
5. PARKING IS PROHIBITED FOR 100' IN ADVANCE OF THE CROSSWALK.
6. MEDIAN SHOULD BE DEPRESSED AT CROSSING LOCATION TO PROVIDE EVEN WALKING SURFACE RATHER THAN PEDESTRIAN RAMPS.
7. FOR 3 OR MORE LANE ROADWAY SECTIONS MIDBLOCK CROSSINGS MUST BE SIGNALIZED AND APPROVED BY FDOT'S TRAFFIC OPERATIONS DEPARTMENT.

$TIMES
$DATE$



TYPICAL APPLICATION

1. AREAS WITH HIGH VOLUME OF PEDESTRIANS ON SIDEWALK SUCH AS CENTRAL BUSINESS DISTRICTS.
2. MID-BLOCK BUS STOP NEEDED TO ACCESS A TRANSIT DEMAND GENERATOR.
3. ON-STREET PARALLEL PARKING.

M-1A

MID-BLOCK NUB/BULB WITH ON-STREET PARKING

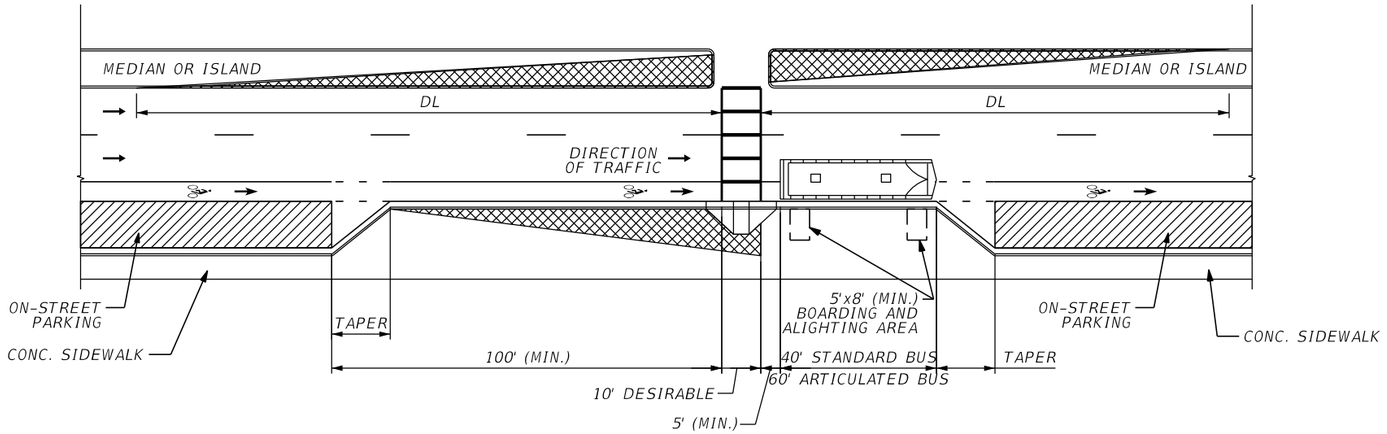
(NO MID-BLOCK CROSSWALK)

CONDITIONS

1. ARTERIAL DESIGN SPEED ≤40 MPH.
2. ON STREET PARKING PROVIDED.

GENERAL NOTES

1. PEDESTRIAN RAMP AND CROSSWALK (WITH OPTIONAL PEDESTRIAN SIGNAL) CAN BE PROVIDED IF NO CONFLICT IS CREATED WITH BOARDING AND ALIGHTING AREAS.
2. BUS STOP AREA SHOWN FOR ONE BUS.
3. SEE SHEET 22 FOR FURTHER DETAILS.
4. FOR PAVEMENT MARKINGS SEE INDEX 17346 AND 17347.



LEGEND

PARKING AREA

CLEAR AREA
(SEE SHEET 22 FOR DETAILS)

TYPICAL APPLICATION

1. AREAS WITH HIGH VOLUME OF PEDESTRIANS ON SIDEWALK SUCH AS CENTRAL BUSINESS DISTRICTS.
2. MID-BLOCK BUS STOP NEEDED TO ACCESS A TRANSIT DEMAND GENERATOR.
3. ON-STREET PARALLEL PARKING.

M-1B

MID-BLOCK NUB/BULB ON A DIVIDED ROADWAY WITH ON-STREET PARKING

CONDITIONS

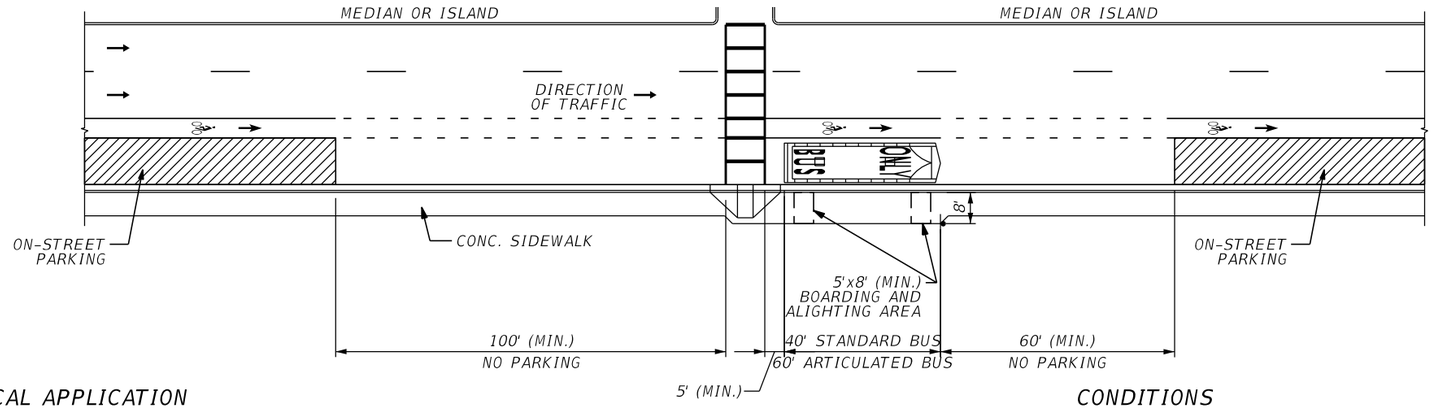
1. ARTERIAL DESIGN SPEED ≤40 MPH.
2. ON STREET PARKING PROVIDED.

GENERAL NOTES

1. BUS STOP AREA SHOWN FOR ONE BUS.
2. SEE SHEET 22 FOR FURTHER DETAILS.
3. CURB RAMP TYPE IS SHOWN ONLY AS AN ILLUSTRATION.
4. DIMENSION "DL" CAN BE FOUND ON SHEET 22.
5. FOR PAVEMENT MARKINGS SEE INDEX 17346 AND 17347.

sTIMEs

sDATEs



TYPICAL APPLICATION

1. RIGHT TURN BAY EXIST ON NEAR SIDE OF INTERSECTION AND FAR SIDE BUS STOP CAN NOT BE PROVIDED.
2. MID-BLOCK BUS STOP NEEDED TO ACCESS A TRANSIT DEMAND GENERATOR.
3. LOW VOLUME OF PEDESTRIANS ON SIDEWALK AND STREET PARKING EXISTS. WHEN PEDESTRIANS VOLUME IS HIGH, NUB ALTERNATIVE IS PREFERABLE.

M-2

MID-BLOCK BUS BAY ON A DIVIDED ROADWAY WITH ON-STREET PARKING

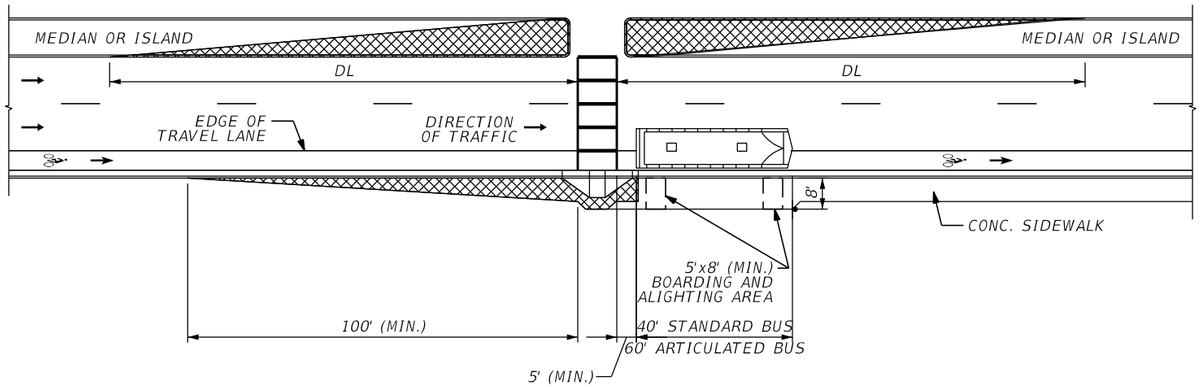
(NO NUB ALTERNATIVE)

CONDITIONS

1. ARTERIAL DESIGN SPEED ≤45 MPH.

GENERAL NOTES

1. BUS STOP AREA SHOWN FOR ONE BUS.
2. SEE SHEET 22 FOR FURTHER DETAILS.
3. CURB RAMP TYPE IS SHOWN ONLY AS AN ILLUSTRATION.
4. FOR PAVEMENT MARKINGS SEE INDEX 17346 AND 17347.



LEGEND

PARKING AREA

CLEAR AREA (SEE SHEET 22 FOR DETAILS)

TYPICAL APPLICATION

1. RIGHT TURN BAY EXIST ON NEAR SIDE OF INTERSECTION AND FAR SIDE BUS STOP CAN NOT BE PROVIDED.
2. MID-BLOCK CROSSING NEEDED TO ACCESS A TRANSIT DEMAND GENERATOR.
3. LOW VOLUME OF PEDESTRIANS ON SIDEWALK AND LOW VOLUME OF VEHICLES.

M-3

MID-BLOCK BUS STOP ON A DIVIDED ROADWAY WITHOUT ON-STREET PARKING

CONDITIONS

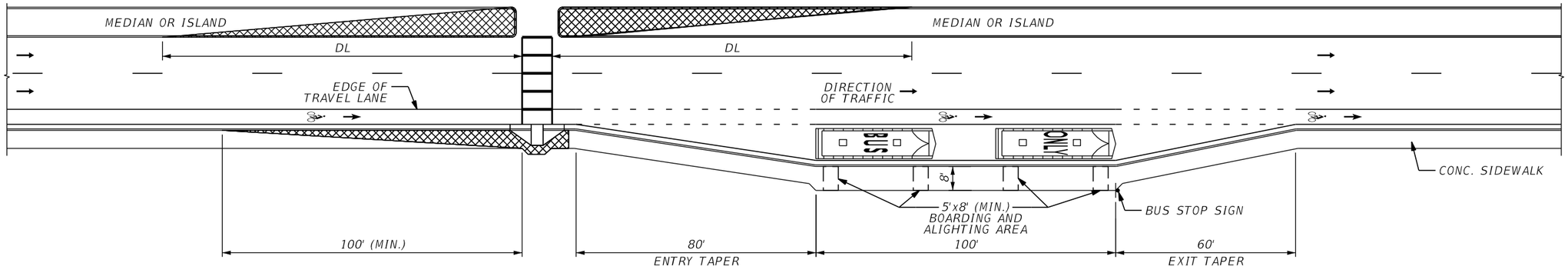
1. ARTERIAL DESIGN SPEED ≤45 MPH.

GENERAL NOTES

1. BUS STOP AREA SHOWN FOR ONE BUS.
2. SEE SHEET 22 FOR FURTHER DETAILS.
3. CURB RAMP TYPE IS SHOWN ONLY AS AN ILLUSTRATION.
4. DIMENSION "DL" CAN BE FOUND ON SHEET 22.
5. FOR PAVEMENT MARKINGS SEE INDEX 17346 AND 17347.

$TIMES

$DATE$



TYPICAL APPLICATION

1. RIGHT TURN BAY EXISTS ON NEAR SIDE OF INTERSECTION, AND FAR SIDE BUS STOP CAN NOT BE PROVIDED.
2. MID-BLOCK CROSSING NEEDED TO ACCESS A TRANSIT DEMAND GENERATOR.
3. AREAS WITH HIGH VOLUME OF VEHICLES AND/OR PASSENGERS.

M-4

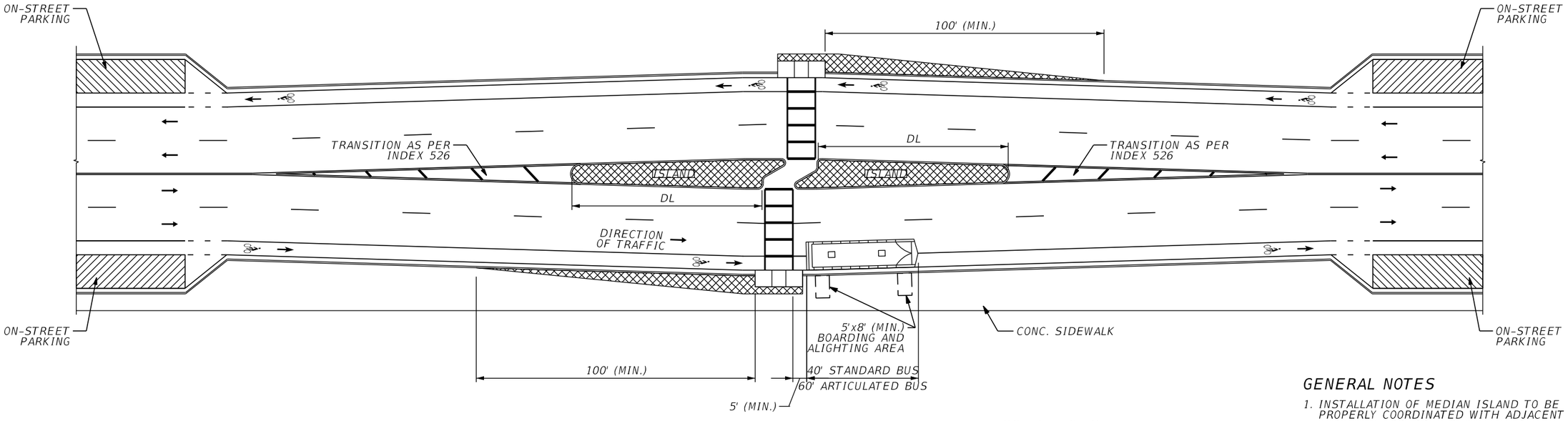
MID-BLOCK BUS BAY ON A DIVIDED ROADWAY WITHOUT ON-STREET PARKING

CONDITIONS

1. ARTERIAL DESIGN SPEED ≤45 MPH.

GENERAL NOTES

1. FOR BUS BAY DETAILS SEE SHEET 7 & 8.
2. BUS STOP AREA SHOWN FOR TWO BUSES.
3. SEE SHEET 22 FOR FURTHER DETAILS.
4. CURB RAMP TYPE IS SHOWN ONLY AS AN ILLUSTRATION.
5. DIMENSION "DL" CAN BE FOUND ON SHEET 22.
6. FOR PAVEMENT MARKINGS SEE INDEX 17346 AND 17347.



LEGEND

PARKING AREA

CLEAR AREA (SEE SHEET 22 FOR DETAILS)

TYPICAL APPLICATION

1. RIGHT TURN BAY EXISTS ON NEAR SIDE OF INTERSECTION AND FAR SIDE BUS STOP CAN NOT BE PROVIDED.
2. MID-BLOCK CROSSING NEEDED TO ACCESS A TRANSIT DEMAND GENERATOR.
3. LOW VULUME OF PEDESTRIANS ON SIDEWALK, LOW VOLUME OF VEHICLES AND STREET PARKING EXITS.

M-5

MID-BLOCK BUS STOP ON AN UNDIVIDED ROADWAY WITH ON-STREET PARKING

CONDITIONS

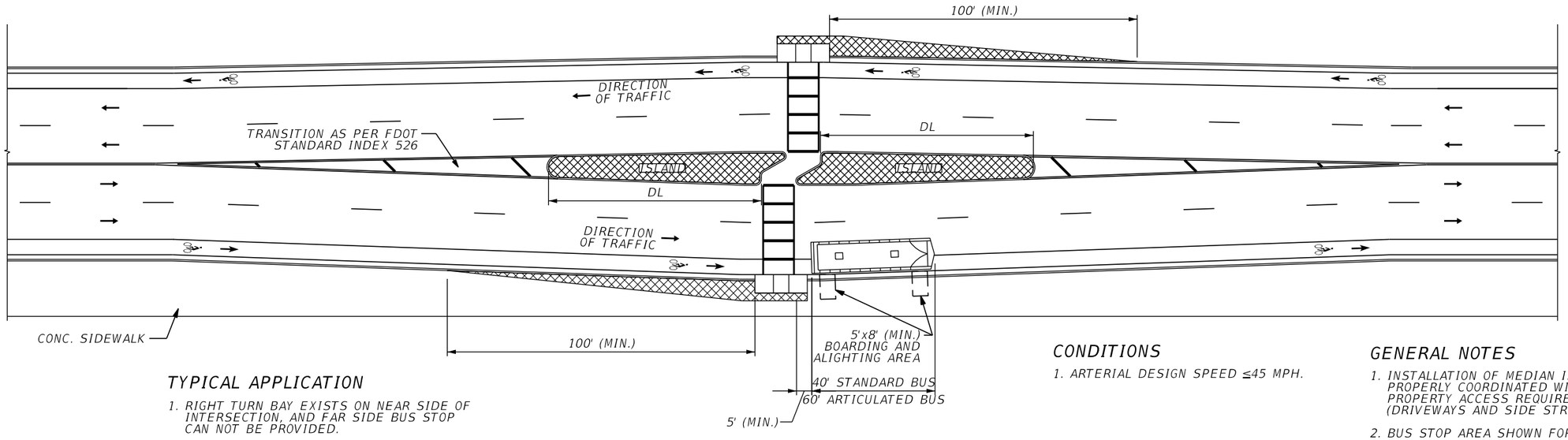
1. ARTERIAL DESIGN SPEED ≤45 MPH.

GENERAL NOTES

1. INSTALLATION OF MEDIAN ISLAND TO BE PROPERLY COORDINATED WITH ADJACENT PROPERTY ACCESS REQUIREMENTS (DRIVEWAYS AND SIDE STREETS).
2. BUS STOP AREA SHOWN FOR ONE BUS.
3. SEE SHEET 22 FOR FURTHER DETAILS.
4. CURB RAMP TYPE IS SHOWN ONLY AS AN ILLUSTRATION.
5. DIMENSION "DL" CAN BE FOUND ON SHEET 22.
6. FOR PAVEMENT MARKINGS SEE INDEX 17346 AND 17347.

$TIMES

$DATES



TYPICAL APPLICATION

1. RIGHT TURN BAY EXISTS ON NEAR SIDE OF INTERSECTION, AND FAR SIDE BUS STOP CAN NOT BE PROVIDED.
2. MID-BLOCK CROSSING NEEDED TO ACCESS A TRANSIT DEMAND GENERATOR.
3. LOW VOLUME OF PEDESTRIANS ON SIDEWALK AND LOW VOLUME OF VEHICLES.

CONDITIONS

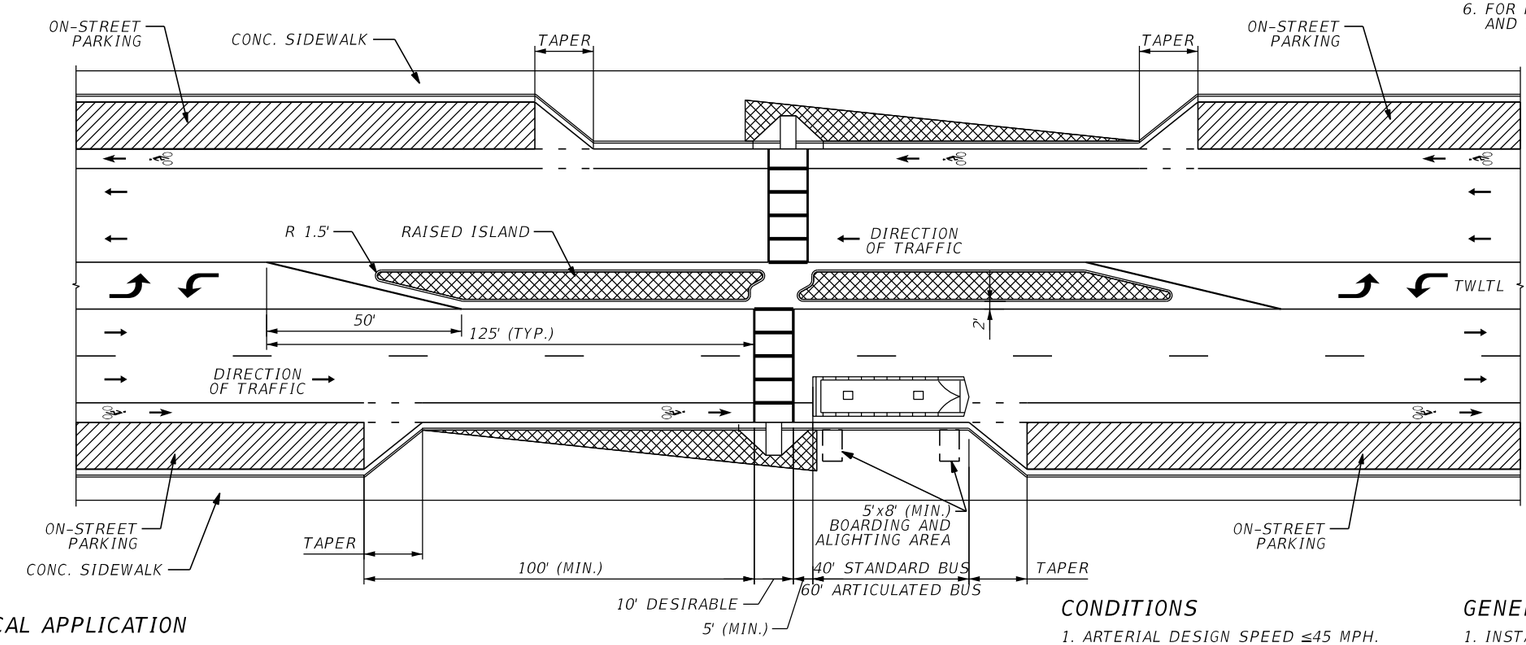
1. ARTERIAL DESIGN SPEED ≤45 MPH.

GENERAL NOTES

1. INSTALLATION OF MEDIAN ISLAND TO BE PROPERLY COORDINATED WITH ADJACENT PROPERTY ACCESS REQUIREMENTS (DRIVEWAYS AND SIDE STREETS).
2. BUS STOP AREA SHOWN FOR ONE BUS.
3. SEE SHEET 22 FOR FURTHER DETAILS.
4. CURB RAMP TYPE IS SHOWN ONLY AS AN ILLUSTRATION.
5. DIMENSIONS "DL" CAN BE FOUND ON SHEET 22.
6. FOR PAVEMENT MARKINGS SEE INDEX 17346 AND 17347.

M-6

MID-BLOCK BUS STOP ON AN UNDIVIDED ROADWAY WITHOUT ON-STREET PARKING



LEGEND

PARKING AREA

CLEAR AREA (SEE SHEET 22 FOR DETAILS)

TWLTL TWO WAY LEFT TURN LANE

TYPICAL APPLICATION

1. AREAS WITH HIGH VOLUME OF PEDESTRIANS ON SIDEWALK SUCH AS CENTRAL BUSINESS DISTRICTS.
2. MID-BLOCK CROSSING NEEDED TO ACCESS A TRANSIT DEMAND GENERATOR.
3. ON STREET PARALLEL PARKING.

CONDITIONS

1. ARTERIAL DESIGN SPEED ≤45 MPH.

GENERAL NOTES

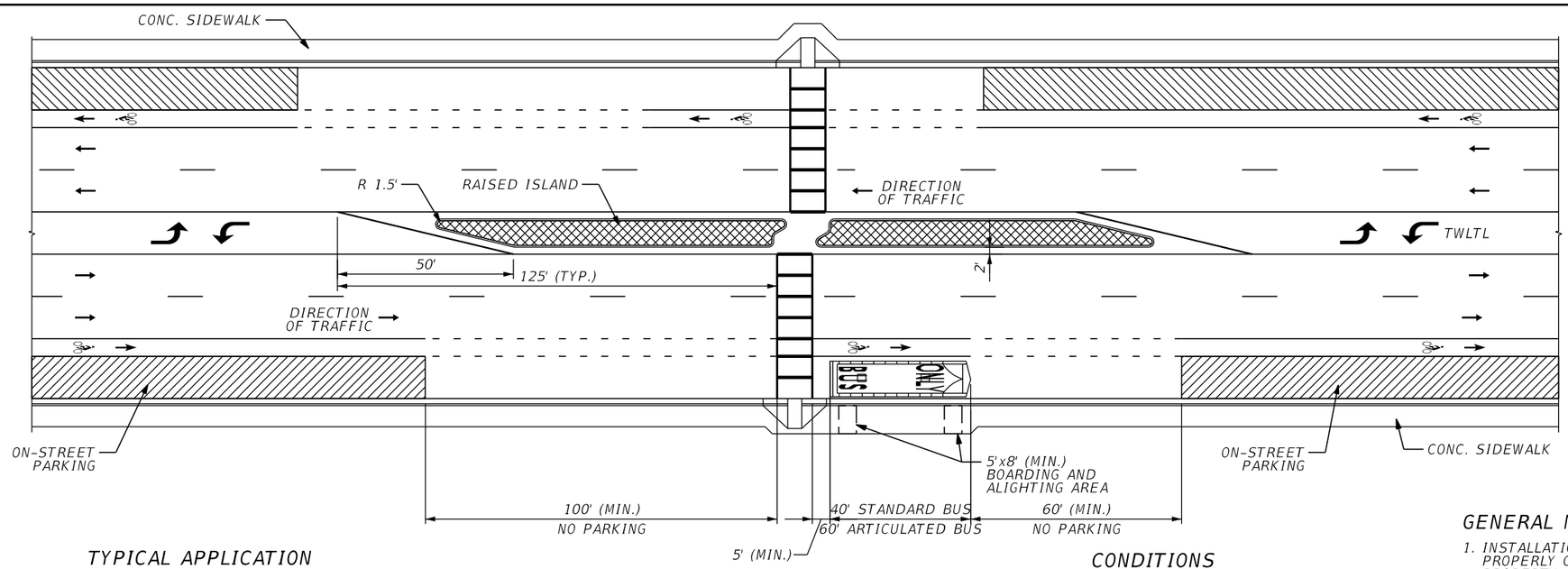
1. INSTALLATION OF MEDIAN ISLAND TO BE PROPERLY COORDINATED WITH ADJACENT PROPERTY ACCESS REQUIREMENTS (DRIVEWAYS AND SIDE STREETS).
2. BUS STOP AREA SHOWN FOR ONE BUS.
3. SEE SHEET 22 FOR FURTHER DETAILS.
4. CURB RAMP TYPE IS SHOWN ONLY AS AN ILLUSTRATION.
5. FOR PAVEMENT MARKINGS SEE INDEX 17346 AND 17347.

M-7

MID-BLOCK NUB/BULB WITH TWO-WAY-LEFT-TURN-LANE MEDIAN AND ON-STREET PARKING

$TIMES

$DATE$



TYPICAL APPLICATION

1. RIGHT TURN BAY EXISTS ON NEAR SIDE OF INTERSECTION AND FAR SIDE BUS STOP CAN NOT BE PROVIDED.
2. MID-BLOCK CROSSING NEEDED TO ACCESS A TRANSIT DEMAND GENERATOR.
3. LOW VOLUME OF PEDESTRIANS ON SIDEWALK AND STREET PARKING EXISTS. WHEN PEDESTRIAN VOLUME IS HIGH, NUB ALTERNATIVE IS PREFERABLE.
4. ON STREET PARALLEL PARKING.

CONDITIONS

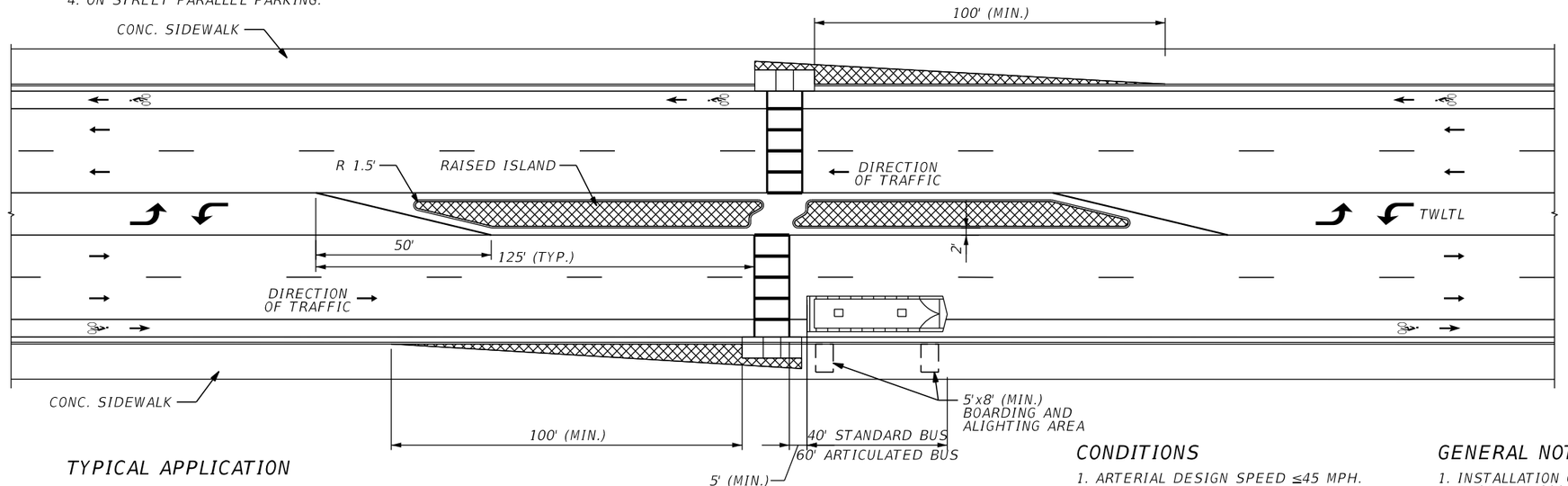
1. ARTERIAL DESIGN SPEED ≤45 MPH.

GENERAL NOTES

1. INSTALLATION OF MEDIAN ISLAND TO BE PROPERLY COORDINATED WITH ADJACENT PROPERTY ACCESS REQUIREMENTS (DRIVEWAYS AND SIDE STREETS).
2. BUS STOP AREA SHOWN FOR ONE BUS.
3. SEE SHEET 22 FOR FURTHER DETAILS.
4. CURB RAMP TYPE IS SHOWN ONLY AS AN ILLUSTRATION.
5. FOR PAVEMENT MARKINGS SEE INDEX 17346 AND 17347.

M-8

MID-BLOCK BUS BAY WITH TWO-WAY-LEFT-TURN-LANE MEDIAN AND ON-STREET PARKING



LEGEND

- PARKING AREA
- CLEAR AREA (SEE SHEET 22 FOR DETAILS)

TWLTL TWO WAY LEFT TURN LANE

TYPICAL APPLICATION

1. RIGHT TURN BAY EXISTS ON NEAR SIDE OF INTERSECTION AND FAR SIDE BUS STOP CAN NOT BE PROVIDED.
2. MID-BLOCK CROSSING NEEDED TO ACCESS A TRANSIT DEMAND GENERATOR.
3. LOW VOLUME OF PEDESTRIANS ON SIDEWALK AND LOW VOLUME OF VEHICLES.

CONDITIONS

1. ARTERIAL DESIGN SPEED ≤45 MPH.

GENERAL NOTES

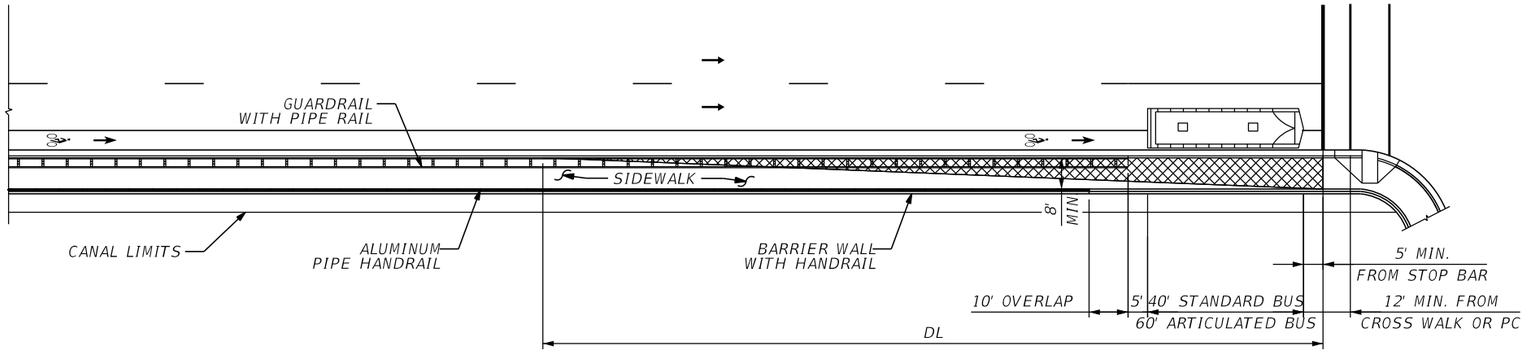
1. INSTALLATION OF MEDIAN ISLAND TO BE PROPERLY COORDINATED WITH ADJACENT PROPERTY ACCESS REQUIREMENTS (DRIVEWAYS AND SIDE STREETS).
2. BUS STOP AREA SHOWN FOR ONE BUS.
3. SEE SHEET 22 FOR FURTHER DETAILS.
4. CURB RAMP TYPE IS SHOWN ONLY AS AN ILLUSTRATION.
5. FOR PAVEMENT MARKINGS SEE INDEX 17346 AND 17347.

M-9

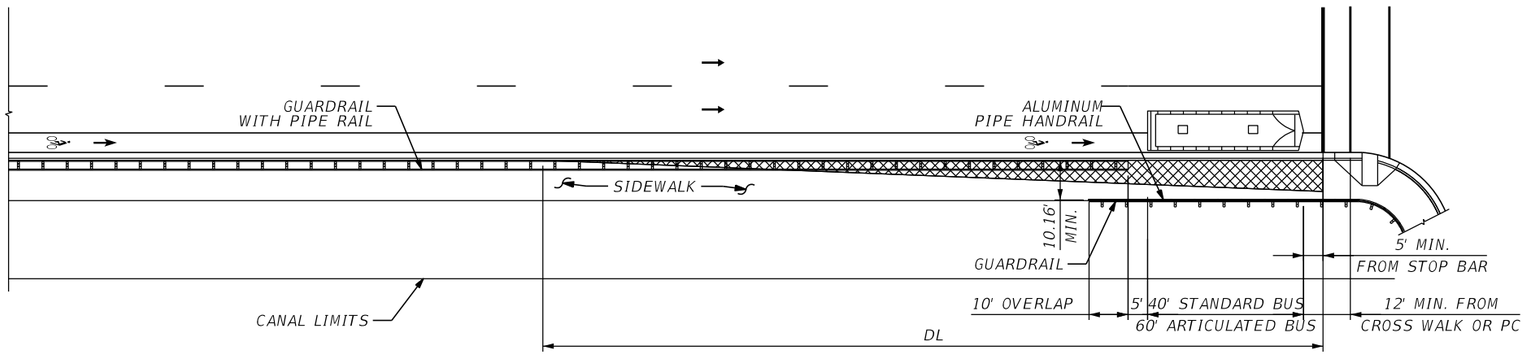
MID-BLOCK BUS STOP WITH TWO-WAY-LEFT-TURN-LANE MEDIAN WITHOUT ON-STREET PARKING

sTIMES

sDATEs



NEAR SIDE BUS STOP ADJACENT
TO CANAL BARRIER WALL



NEAR SIDE BUS STOP
ADJACENT TO CANAL GUARDRAIL

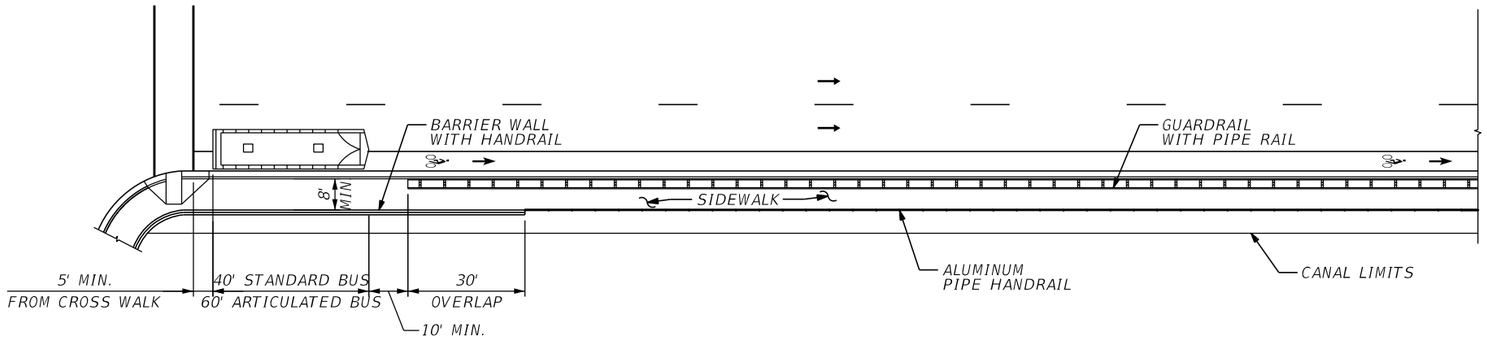
LEGEND

CLEAR AREA
(SEE SHEET 22 FOR DETAILS)

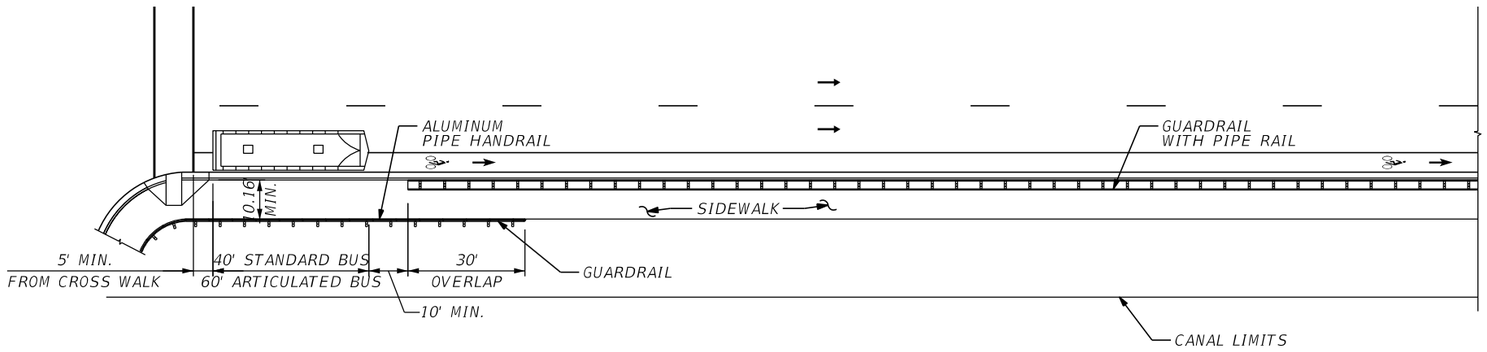
GENERAL NOTES

1. SEE FDOT STANDARD INDEX 17346 FOR TRAFFIC CONTROL DEVICE DETAILS.
2. SEE FDOT STANDARD INDEXES FOR DETAILS ON GUARDRAIL AND BARRIER WALL.
3. BUS STOP AREA SHOWN FOR ONE BUS.
4. CURB RAMP TYPE IS SHOWN ONLY AS AN ILLUSTRATION.
5. DIMENSION "DL" CAN BE FOUND ON SHEET 22.
6. FOR PAVEMENT MARKINGS SEE INDEX 17346 AND 17347.

$TIMES
$DATES



FAR SIDE BUS STOP ADJACENT
TO CANAL BARRIER WALL

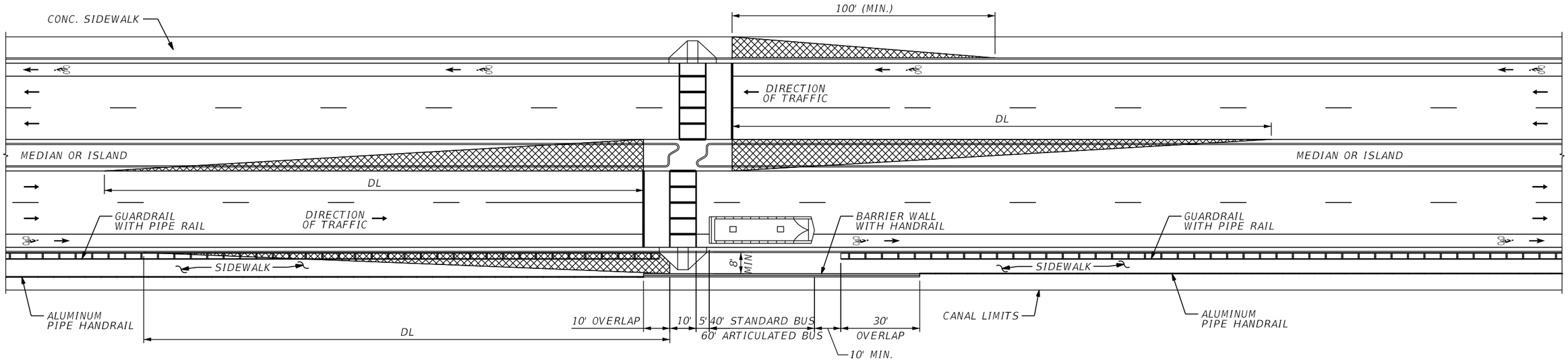


FAR SIDE BUS STOP
ADJACENT TO CANAL GUARDRAIL

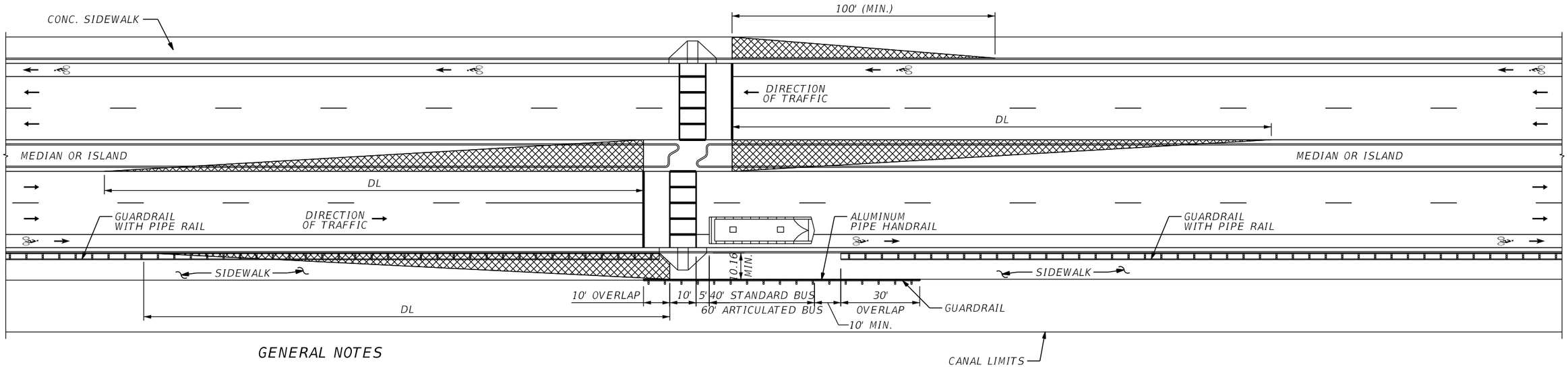
GENERAL NOTES

1. SEE FDOT STANDARD INDEX 17346 FOR TRAFFIC CONTROL DEVICE DETAILS.
2. SEE FDOT STANDARD INDEXES FOR DETAILS ON GUARDRAIL AND BARRIER WALL.
3. BUS STOP AREA SHOWN FOR ONE BUS.
4. CURB RAMP TYPE IS SHOWN ONLY AS AN ILLUSTRATION.
5. FOR PAVEMENT MARKINGS SEE INDEX 17346 AND 17347.

$TIMES
$DATES



MID-BLOCK BUS STOP ADJACENT TO CANAL BARRIER WALL OPTION



GENERAL NOTES

1. SEE FDOT STANDARD INDEX 17346 FOR TRAFFIC CONTROL DEVICE DETAILS.
2. SEE FDOT STANDARD INDEXES FOR DETAILS ON GUARDRAIL AND BARRIER WALL.
3. BUS STOP AREA SHOWN FOR ONE BUS.
4. SEE SHEET 22 FOR FURTHER DETAILS..
5. CURB RAMP TYPE IS SHOWN ONLY AS AN ILLUSTRATION.
6. DIMENSION "DL" CAN BE FOUND ON SHEET 22.
7. FOR PAVEMENT MARKINGS SEE INDEX 17346 AND 17347.

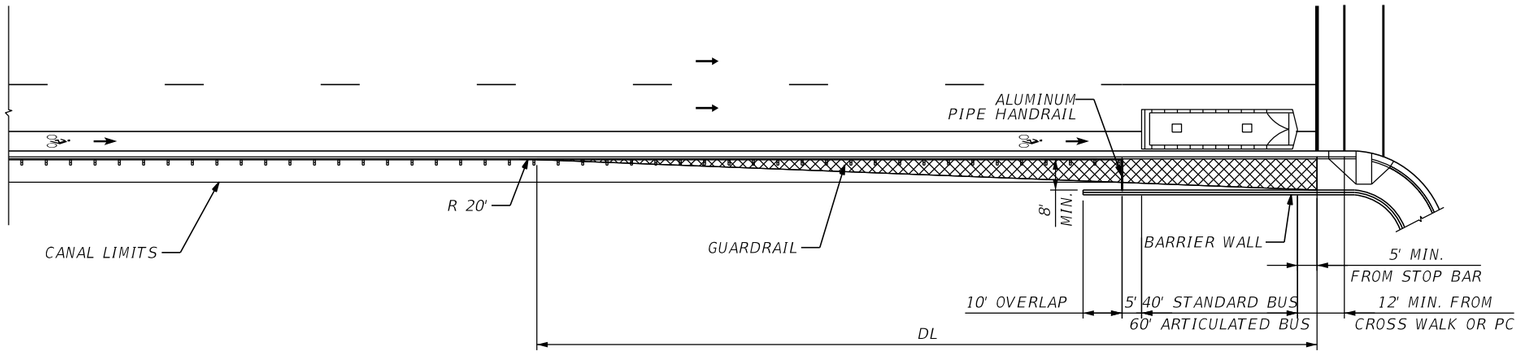
MID-BLOCK BUS STOP ADJACENT TO CANAL GUARDRAIL

LEGEND

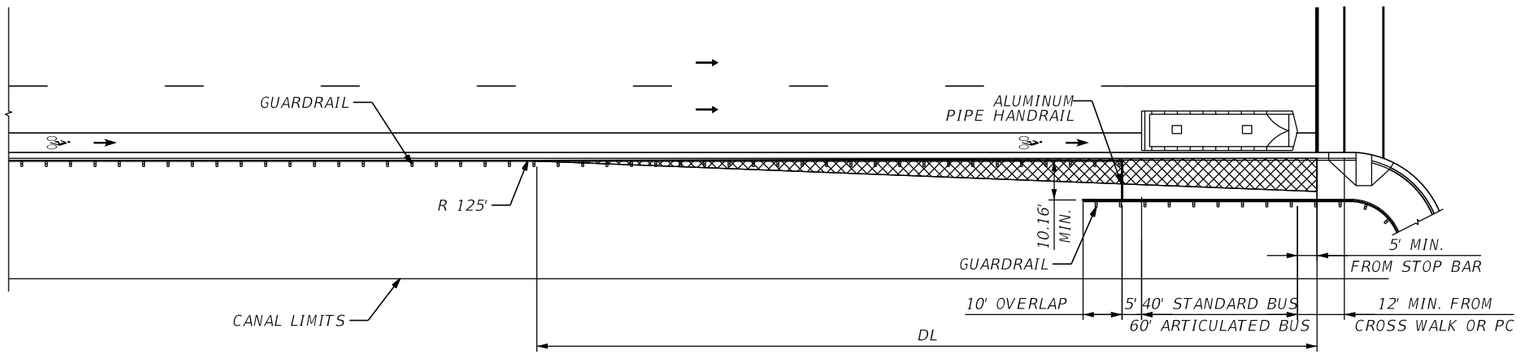
CLEAR AREA (SEE SHEET 22 FOR DETAILS)

sTIMEs

sDATEs



NEAR SIDE ISOLATED BUS STOP
ADJACENT TO CANAL BARRIER WALL



NEAR SIDE ISOLATED BUS STOP
ADJACENT TO CANAL GUARDRAIL

LEGEND:

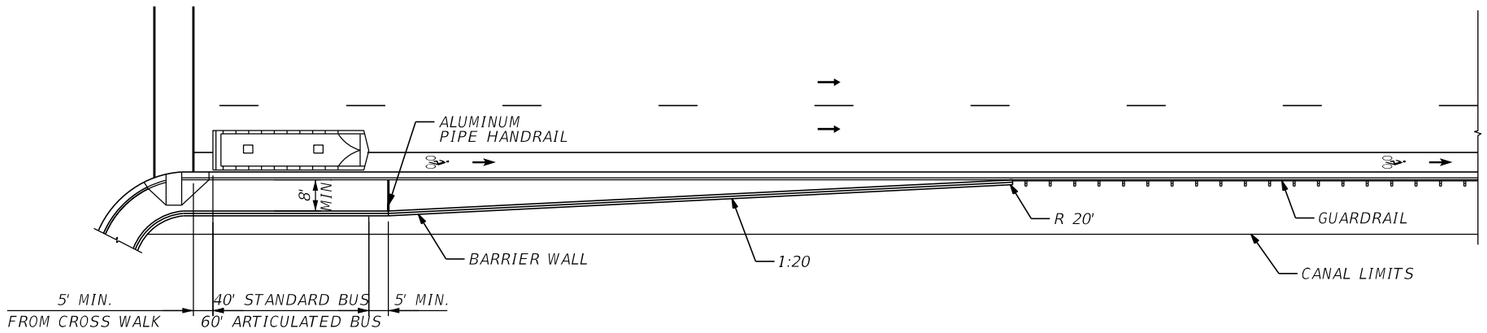
CLEAR AREA
(SEE SHEET 22 FOR DETAILS)

GENERAL NOTES

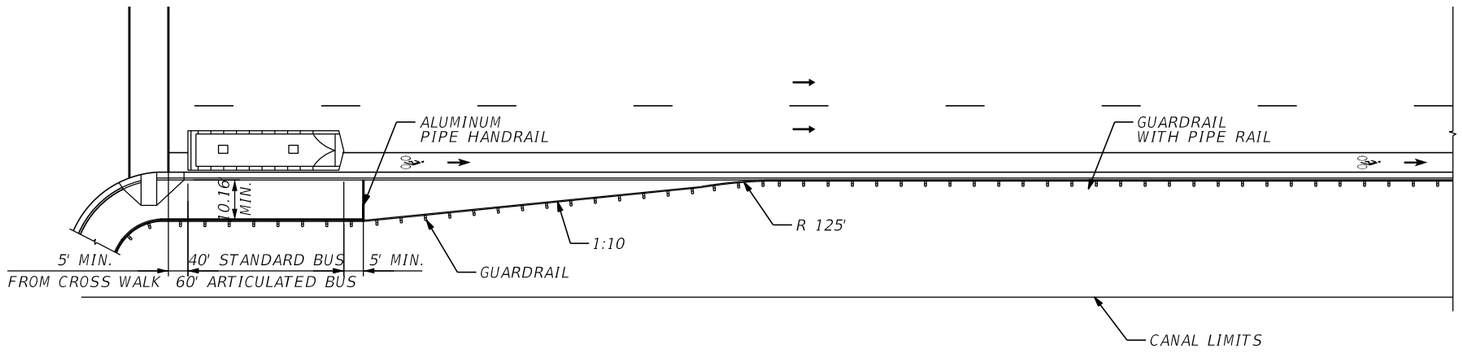
1. SEE FDOT STANDARD INDEX 17346 FOR TRAFFIC CONTROL DEVICE DETAILS.
2. SEE FDOT STANDARD INDEXES FOR DETAILS ON GUARDRAIL AND BARRIER WALL.
3. BUS STOP AREA SHOWN FOR ONE BUS.
4. CURB RAMP TYPE IS SHOWN ONLY AS AN ILLUSTRATION.
5. DIMENSION "DL" CAN BE FOUND ON SHEET 22.
6. FOR PAVEMENT MARKINGS SEE INDEX 17346 AND 17347.

$TIMES

$DATES



FAR SIDE ISOLATED BUS STOP
ADJACENT TO CANAL BARRIER WALL



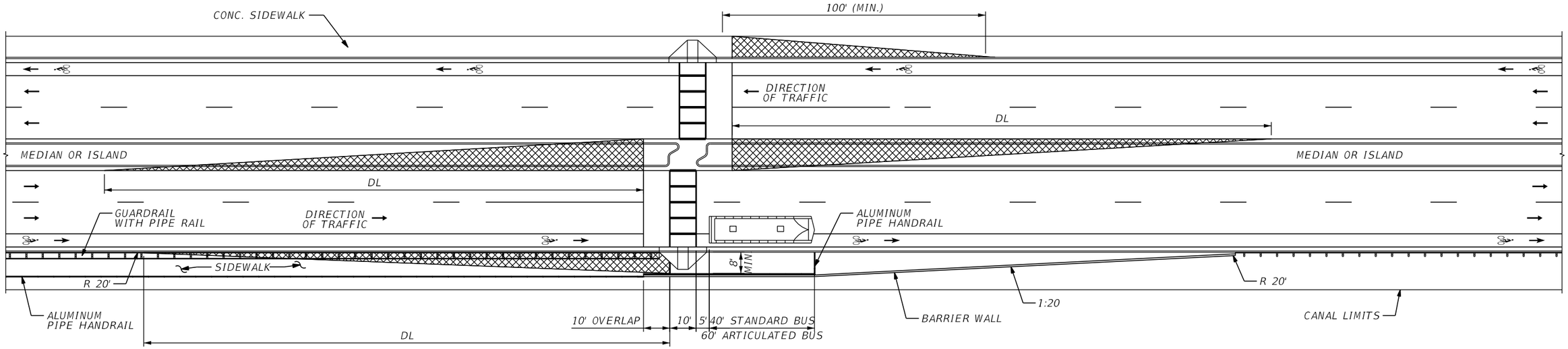
FAR SIDE ISOLATED BUS STOP
ADJACENT TO CANAL GUARDRAIL

GENERAL NOTES

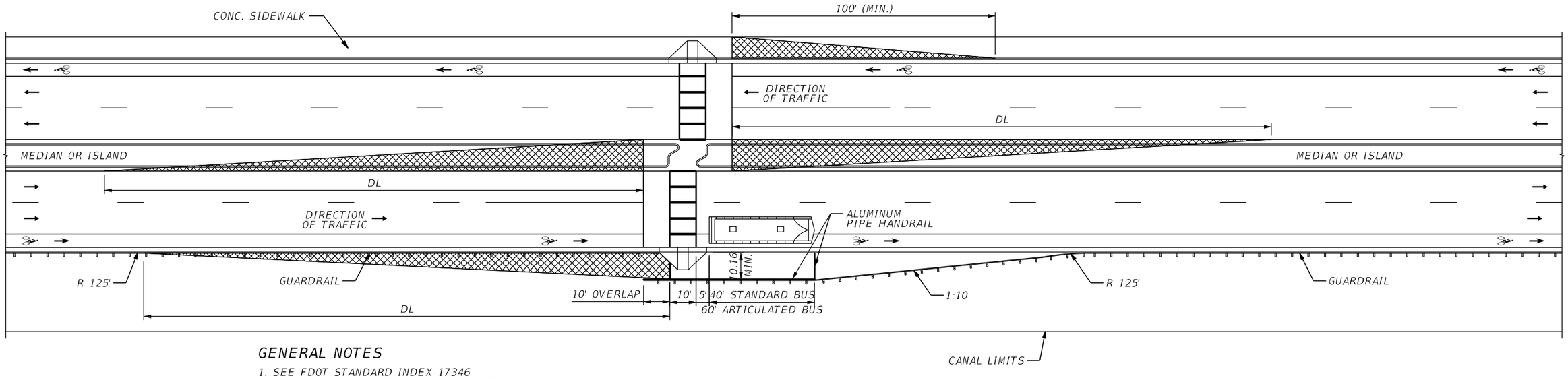
1. SEE FDOT STANDARD INDEX 17346 FOR TRAFFIC CONTROL DEVICE DETAILS.
2. SEE FDOT STANDARD INDEXES FOR DETAILS ON GUARDRAIL AND BARRIER WALL.
3. BUS STOP AREA SHOWN FOR ONE BUS.
4. CURB RAMP TYPE IS SHOWN ONLY AS AN ILLUSTRATION.
5. FOR PAVEMENT MARKINGS SEE INDEX 17346 AND 17347.

sTIMES

sDATES



MID-BLOCK ISOLATED BUS STOP ADJACENT TO CANAL BARRIER WALL



GENERAL NOTES

1. SEE FDOT STANDARD INDEX 17346 FOR TRAFFIC CONTROL DEVICE DETAILS.
2. SEE FDOT STANDARD INDEXES FOR DETAILS ON GUARDRAIL AND BARRIER WALL.
3. BUS STOP AREA SHOWN FOR ONE BUS.
4. SEE SHEET 22 FOR FURTHER DETAILS.
5. CURB RAMP TYPE IS SHOWN ONLY AS AN ILLUSTRATION.
6. DIMENSION "DL" CAN BE FOUND ON SHEET 22.
7. FOR PAVEMENT MARKINGS SEE INDEX 17346 AND 17347.

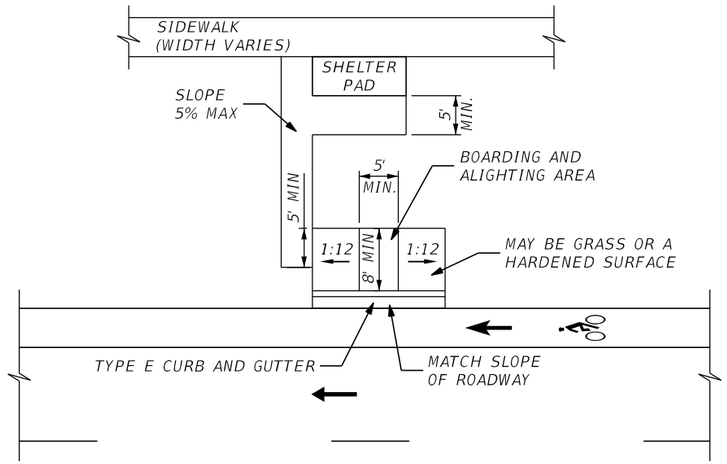
MID-BLOCK ISOLATED BUS STOP ADJACENT TO CANAL GUARDRAIL

LEGEND

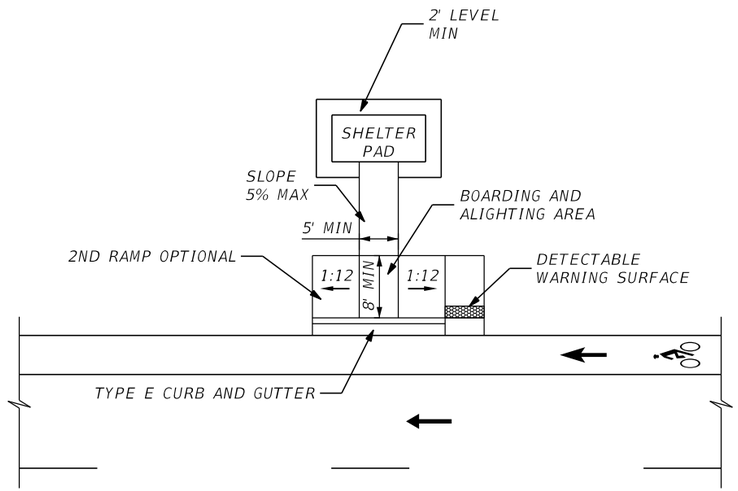
CLEAR AREA (SEE SHEET 22 FOR DETAILS)

$TIMES

$DATES



BOARDING AND ALIGHTING AREA
WITH A SUBURBAN BUS STOP



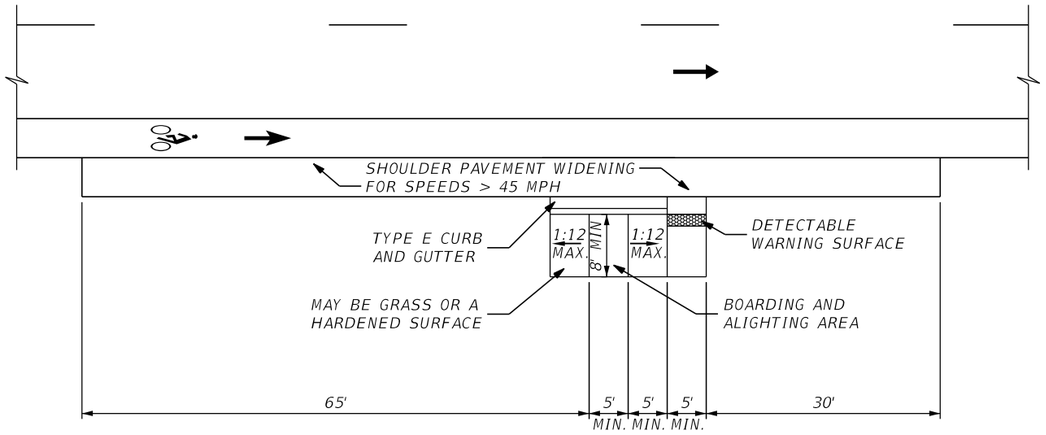
BOARDING AND ALIGHTING AREA
WITH A RURAL BUS STOP

GENERAL NOTES

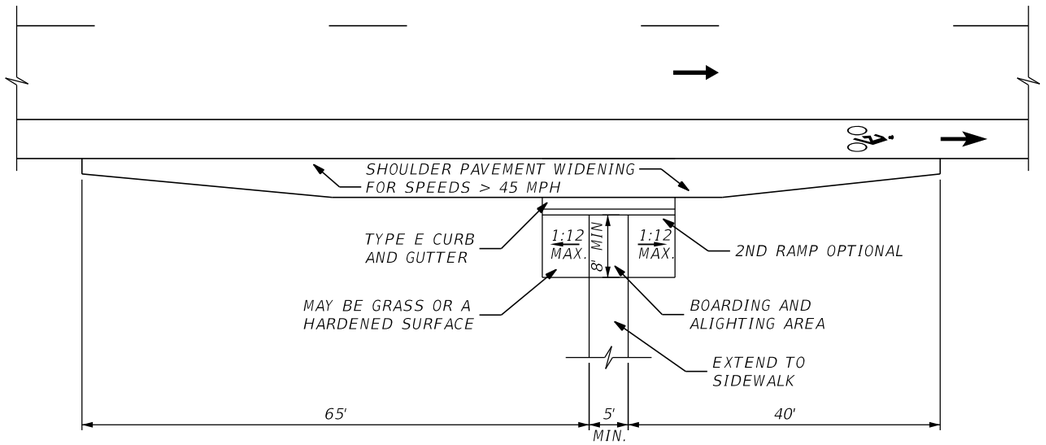
1. BOARDING AND ALIGHTING AREA RAISED 5".

$TIMES

$DATES



ACCESSIBLE BOARDING AND ALIGHTING AREA
FOR FLUSH SHOULDER ROADWAYS WITH A CONNECTION
TO ROADWAY



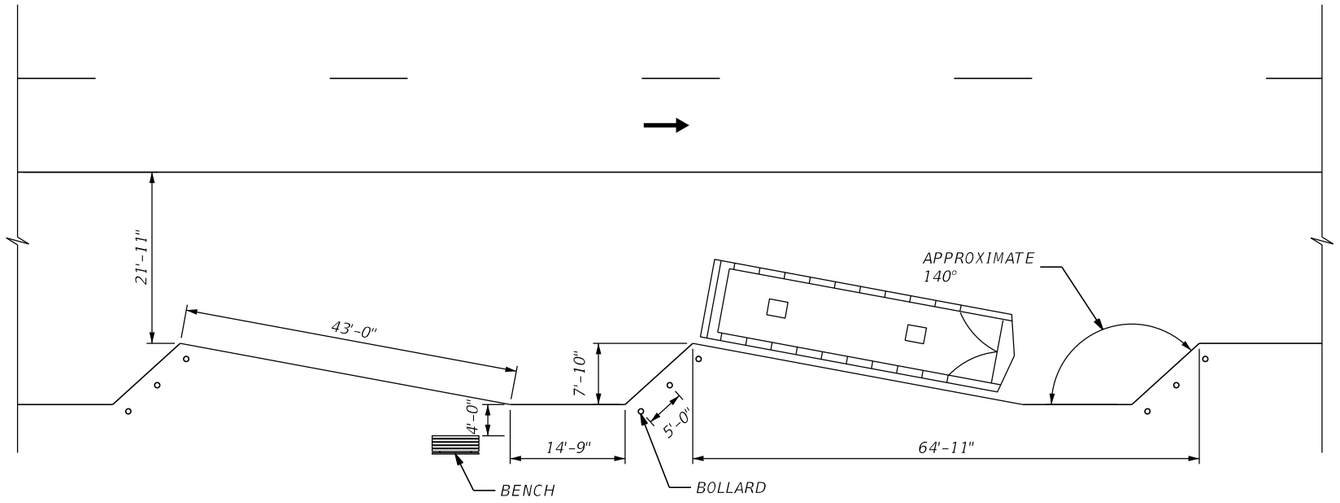
ACCESSIBLE BOARDING AND ALIGHTING AREA
FOR FLUSH SHOULDER ROADWAYS WITH A CONNECTION
TO A SIDEWALK

GENERAL NOTES

1. BOARDING AND ALIGHTING AREA RAISED 5"

$TIMES

$DATES



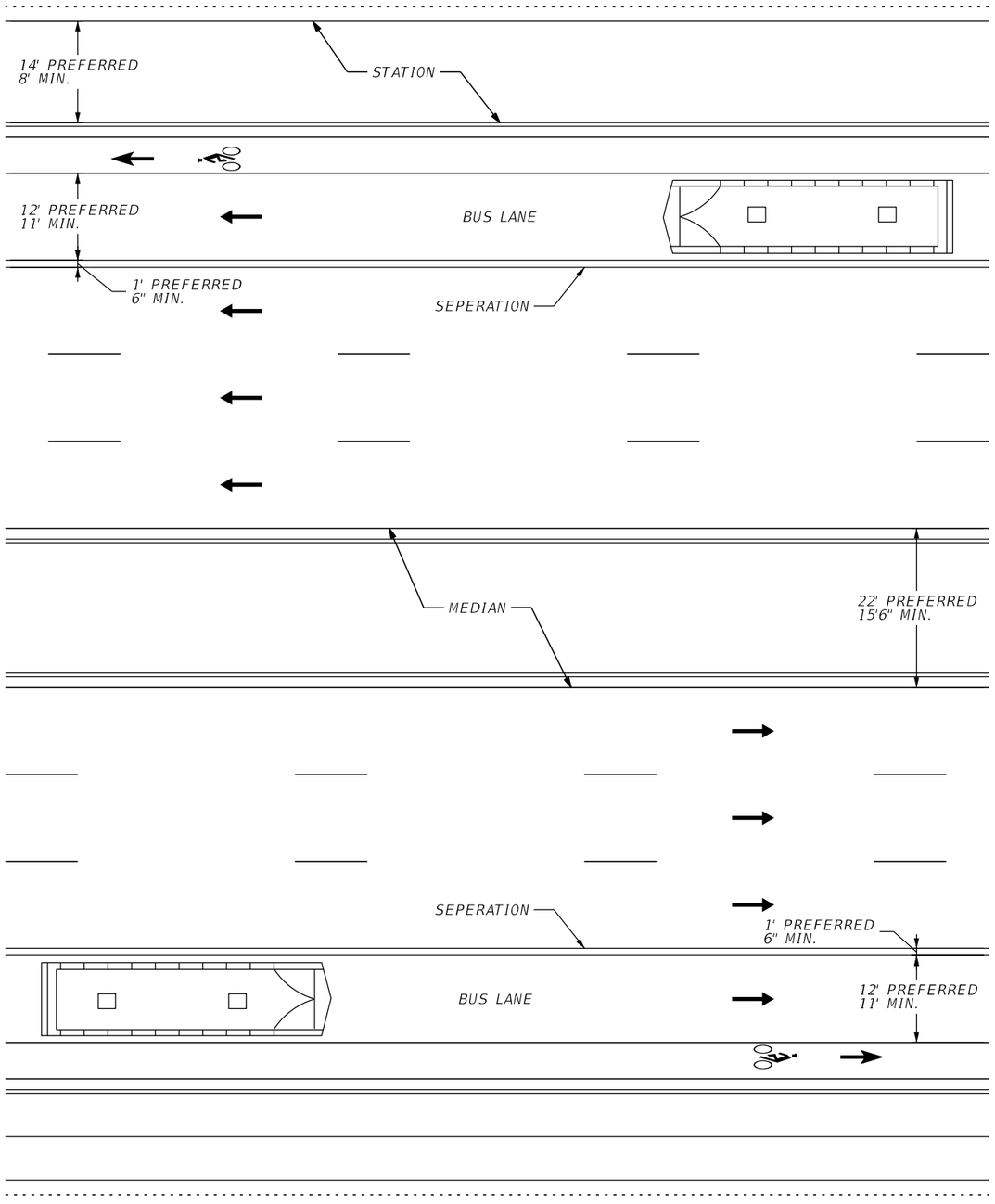
OFF-STREET HALF-SAWTOOTH BUS BAY

GENERAL NOTES

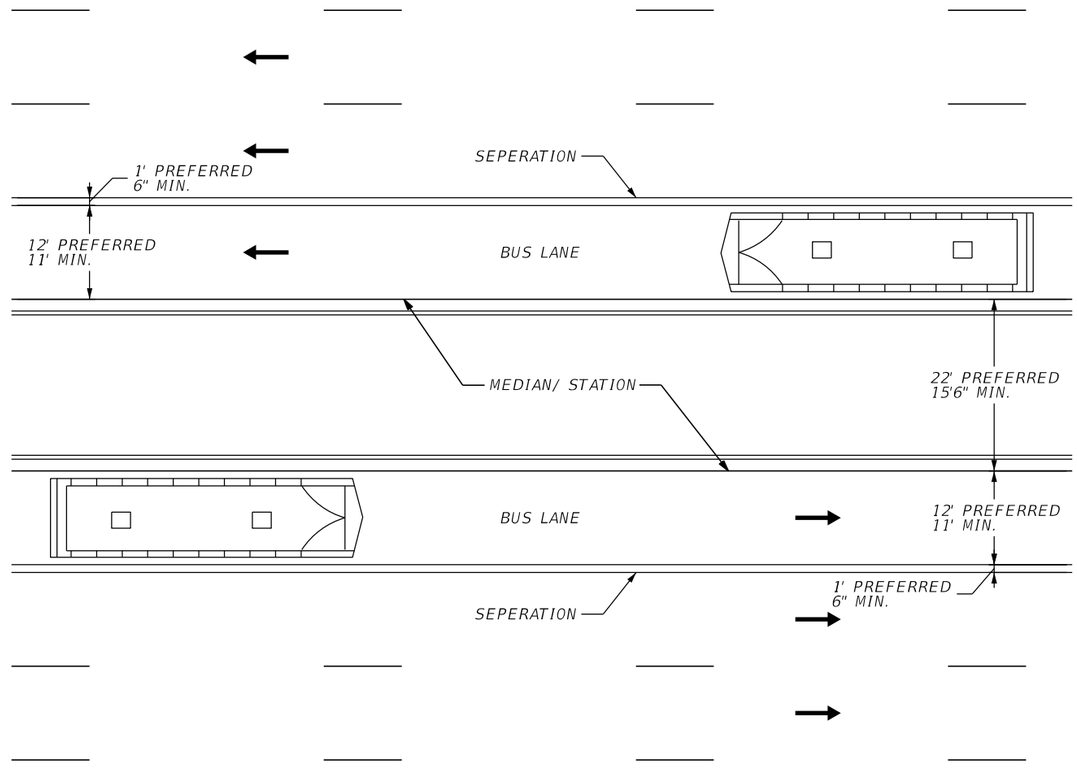
1. TYPICALLY LOCATED IN BUS WAITING OR PARKING FACILITIES AND ARE NOT LOCATED IN THE ROADWAY RIGHT-OF-WAY.
2. THE LENGTHS SHOULD BE EXTENDED BY 20 FEET FOR ARTICULATED BUSES.

GENERAL NOTES

1. CURB TYPE IS SHOWN ONLY AS AN ILLUSTRATION.
2. FOR PAVEMENT MARKINGS SEE INDEX 17346 AND 17347.
3. FOR SHELTER AND SHELTER PAD DETAILS, REFER TO SHEET 6.



CONCURRENT FLOW CURB BUS LANES MIDBLOCK, TWO-WAY STREET



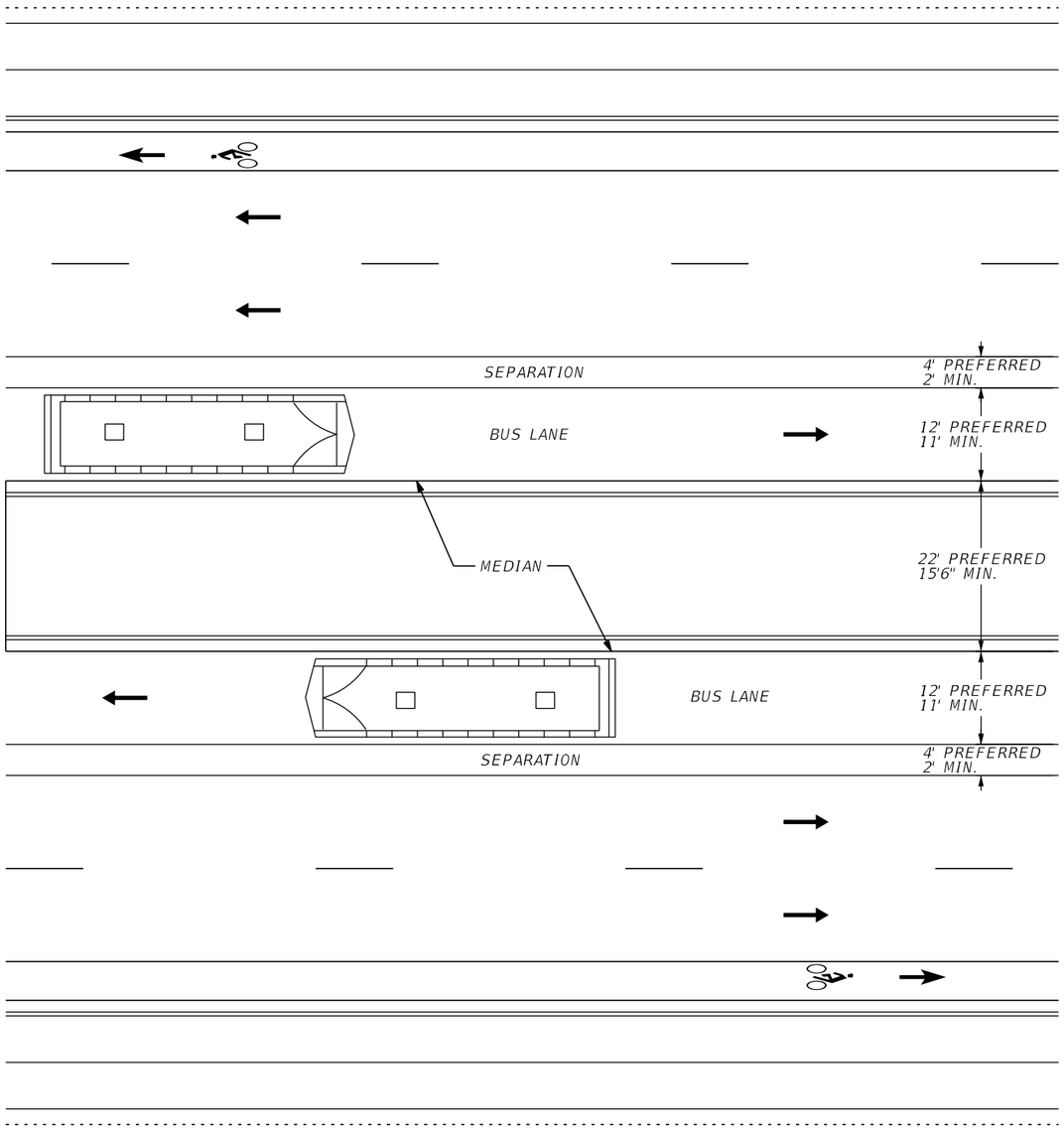
CONCURRENT FLOW MEDIAN BUS LANES MIDBLOCK, TWO-WAY STREET
* REQUIRES VEHICLES WITH LEFT SIDE DOORS

$TIME$

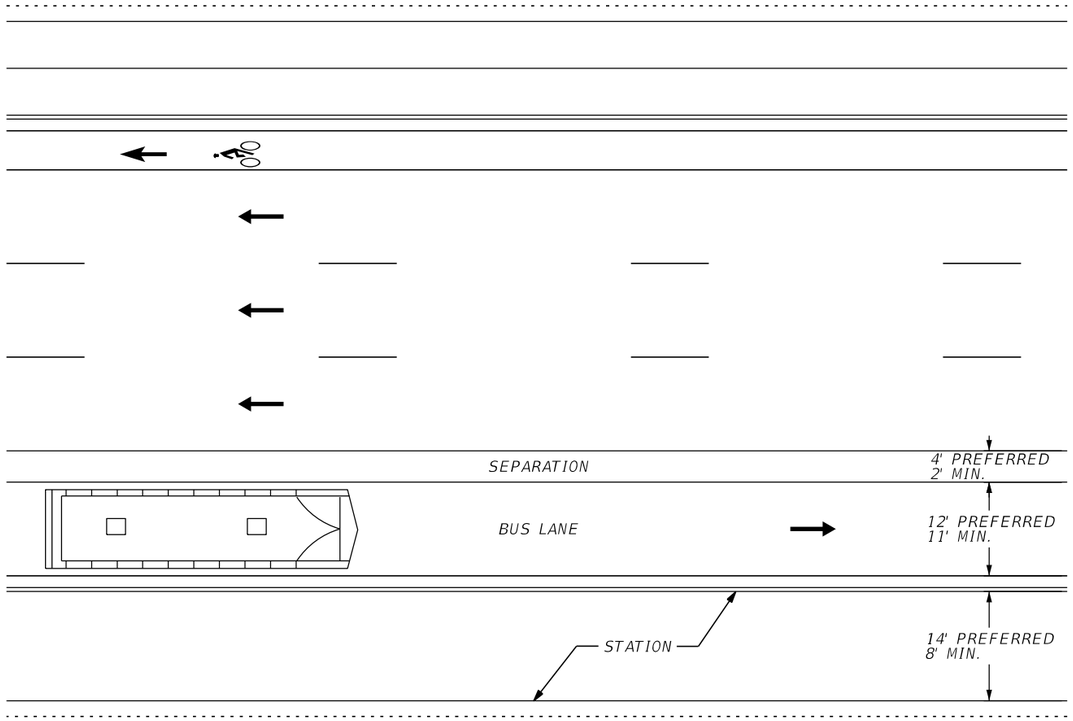
$DATE$

GENERAL NOTES

1. CURB TYPE IS SHOWN ONLY AS AN ILLUSTRATION.
2. FOR PAVEMENT MARKINGS SEE INDEX 17346 AND 17347.
3. FOR SHELTER AND SHELTER PAD DETAILS, REFER TO SHEET 6.



CONTRA-FLOW BUS LANES MIDBLOCK, TWO-WAY STREET



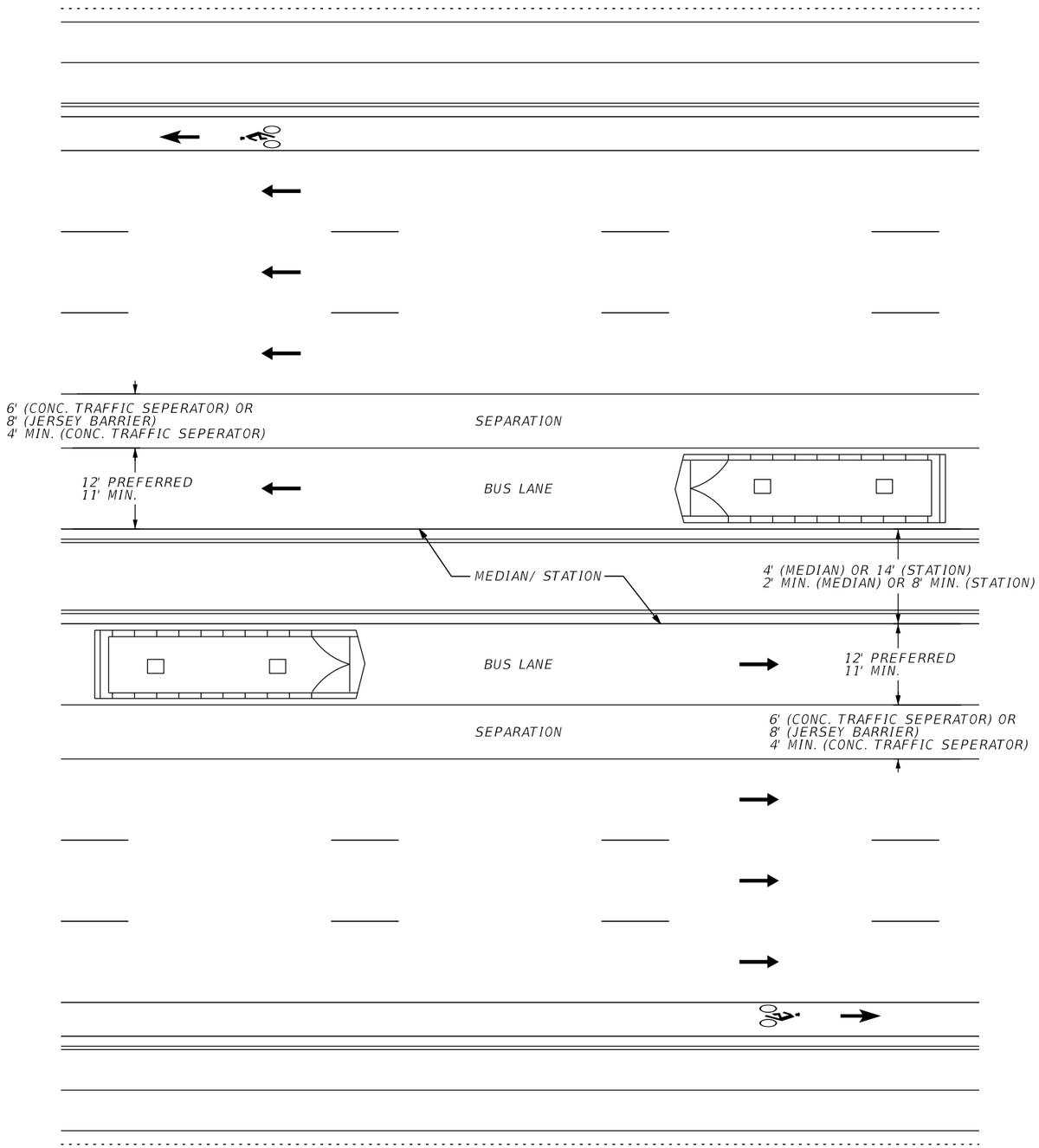
CONTRA-FLOW BUS LANE MIDBLOCK, ONE-WAY STREET

STIMES

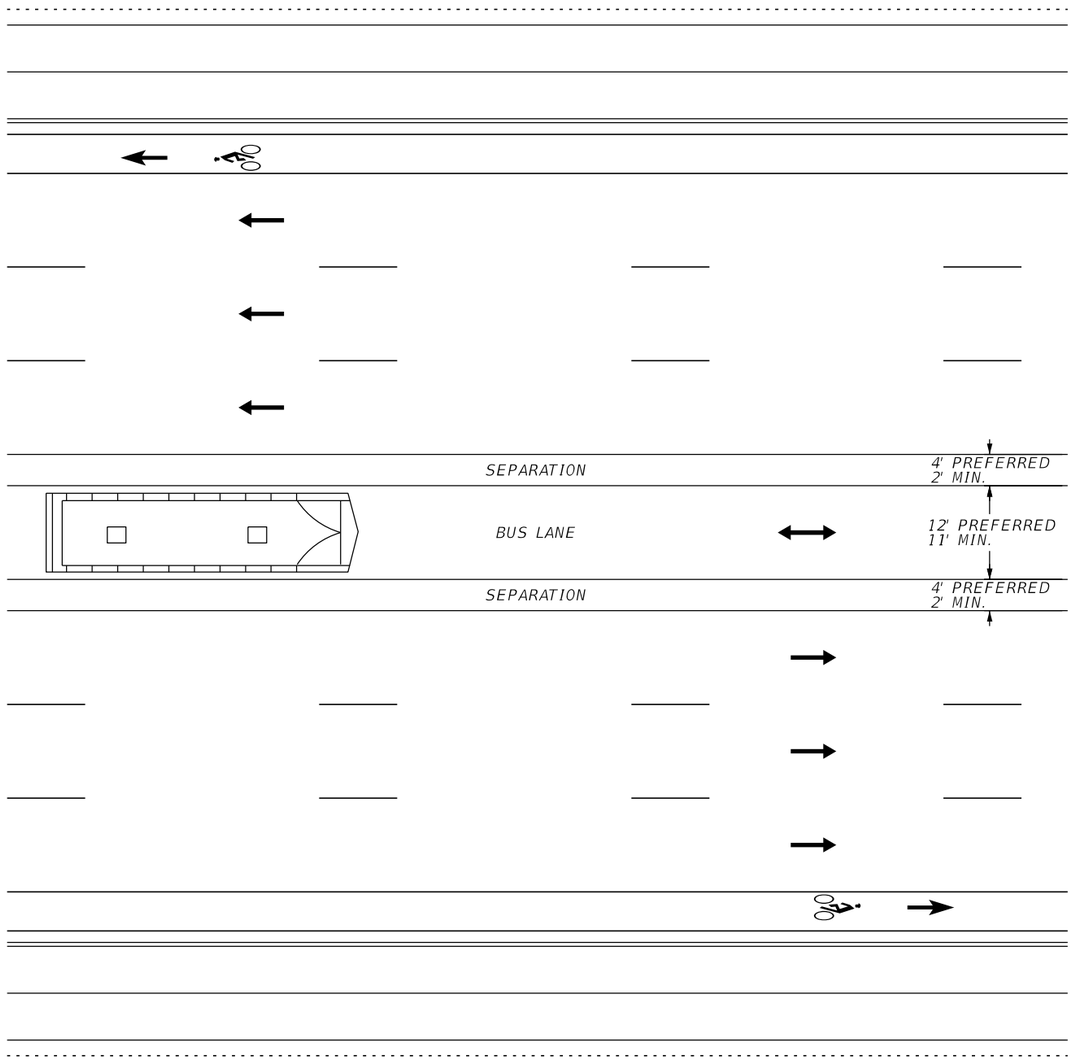
SDATES

GENERAL NOTES

1. CURB TYPE IS SHOWN ONLY AS AN ILLUSTRATION.
2. FOR PAVEMENT MARKINGS SEE INDEX 17346 AND 17347.
3. FOR SHELTER AND SHELTER PAD DETAILS, REFER TO SHEET 6.

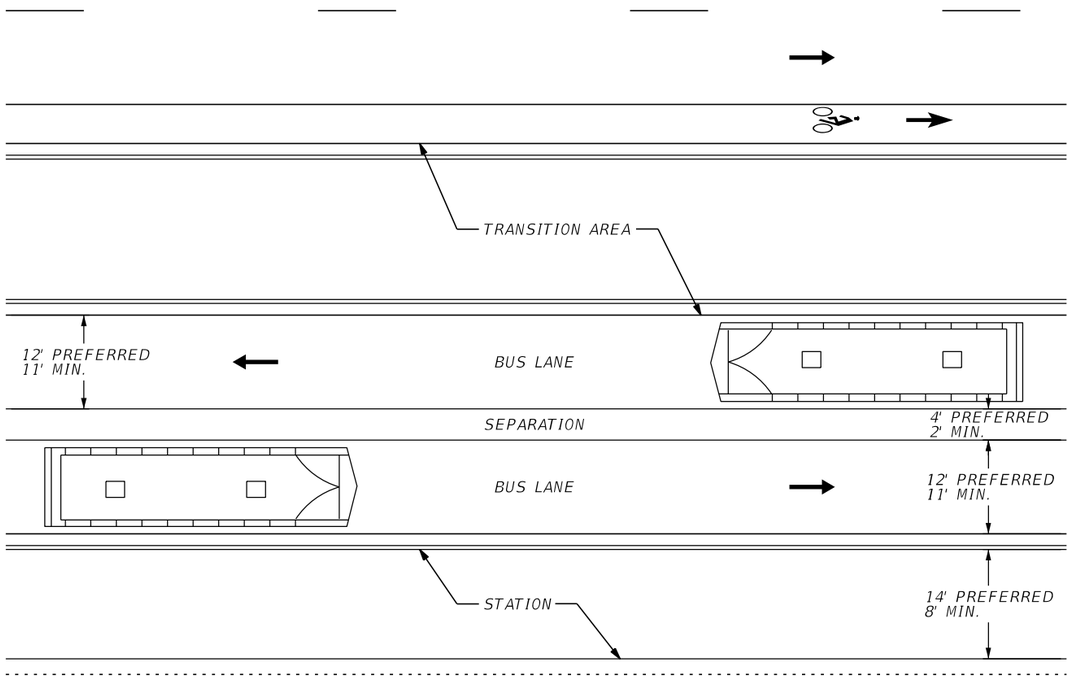


TWO WAY BUSWAY: MIDBLOCK, TWO-WAY STREET
* REQUIRES VEHICLES WITH LEFT SIDE DOORS



REVERSIBLE ONE-LANE MEDIAN BUSWAY MIDBLOCK, TWO-WAY STREET

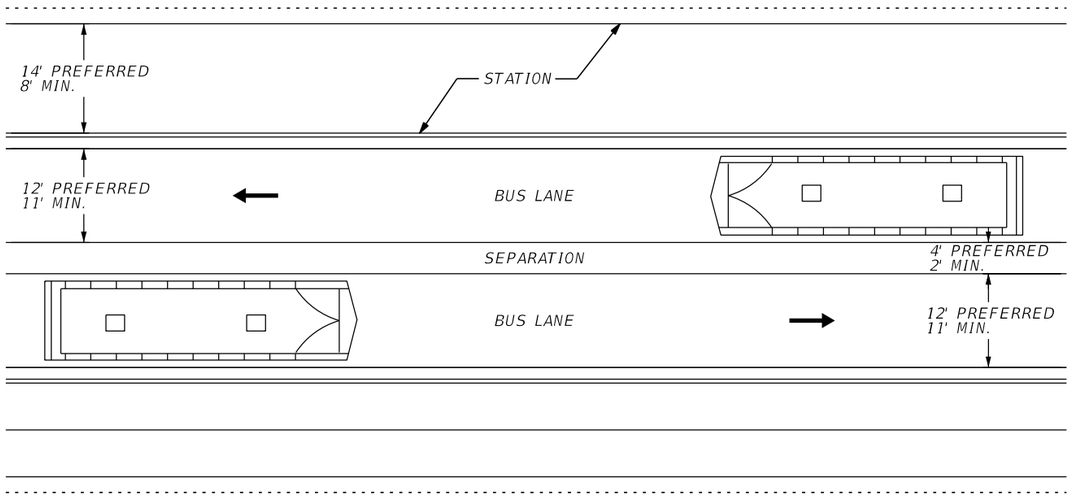
$TIMES
$DATES



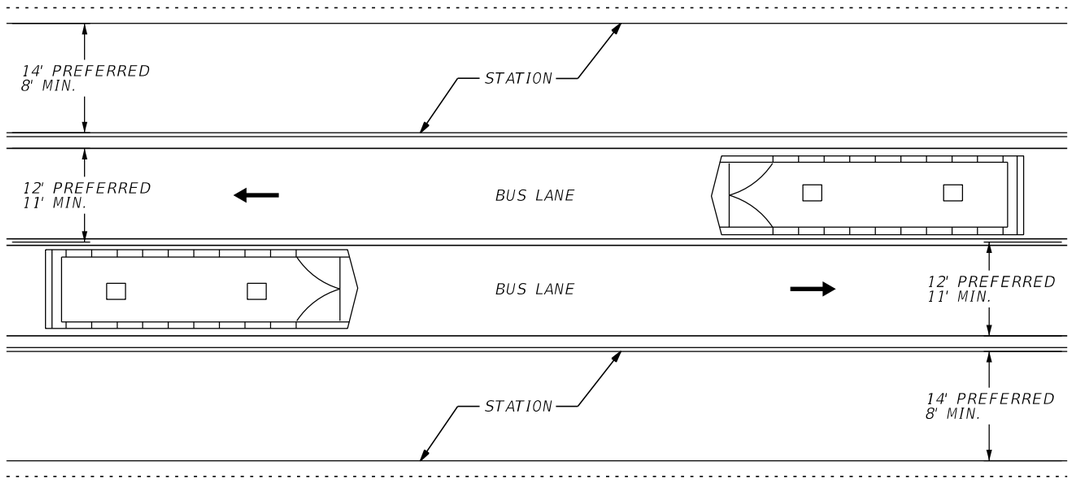
EXCLUSIVE BUSWAY IN ROADWAY RIGHT-OF-WAY

GENERAL NOTES

1. CURB TYPE IS SHOWN ONLY AS AN ILLUSTRATION.
2. FOR PAVEMENT MARKINGS SEE INDEX 17346 AND 17347.
3. FOR SHELTER AND SHELTER PAD DETAILS, REFER TO SHEET 6.



EXCLUSIVE BUSWAY

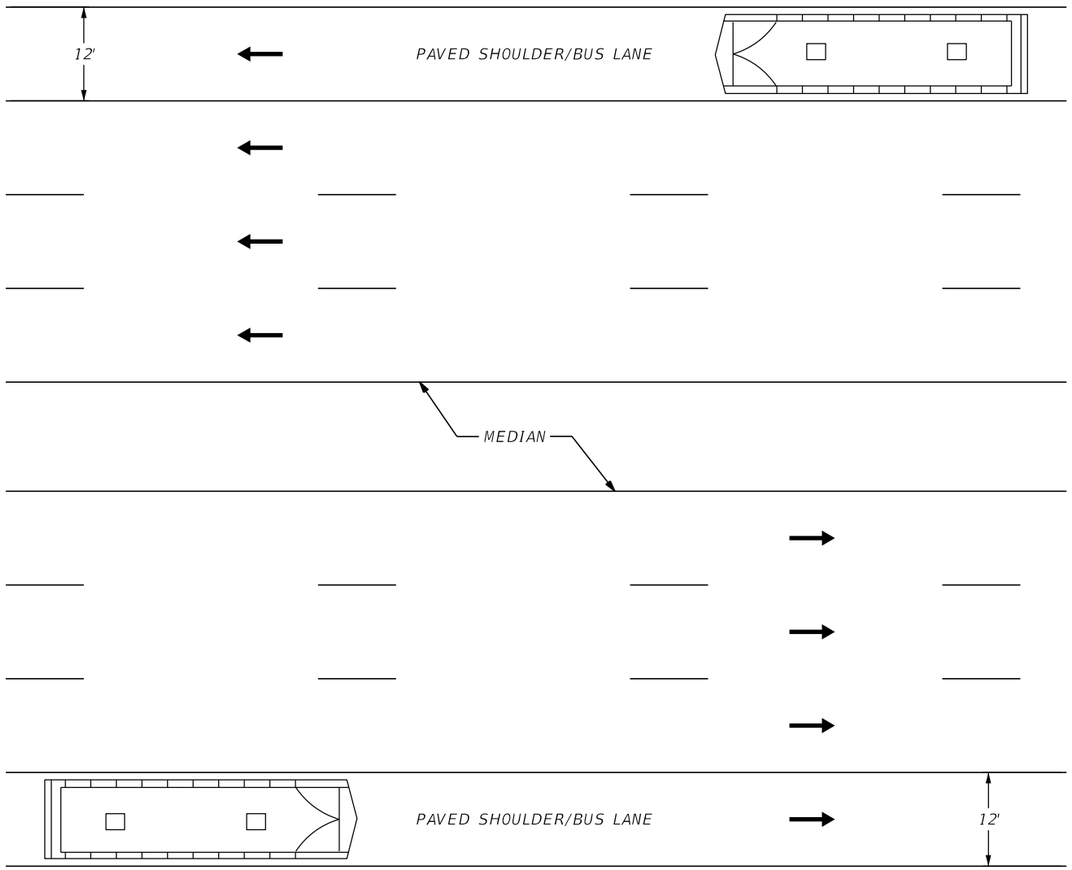


EXCLUSIVE BUS STREET

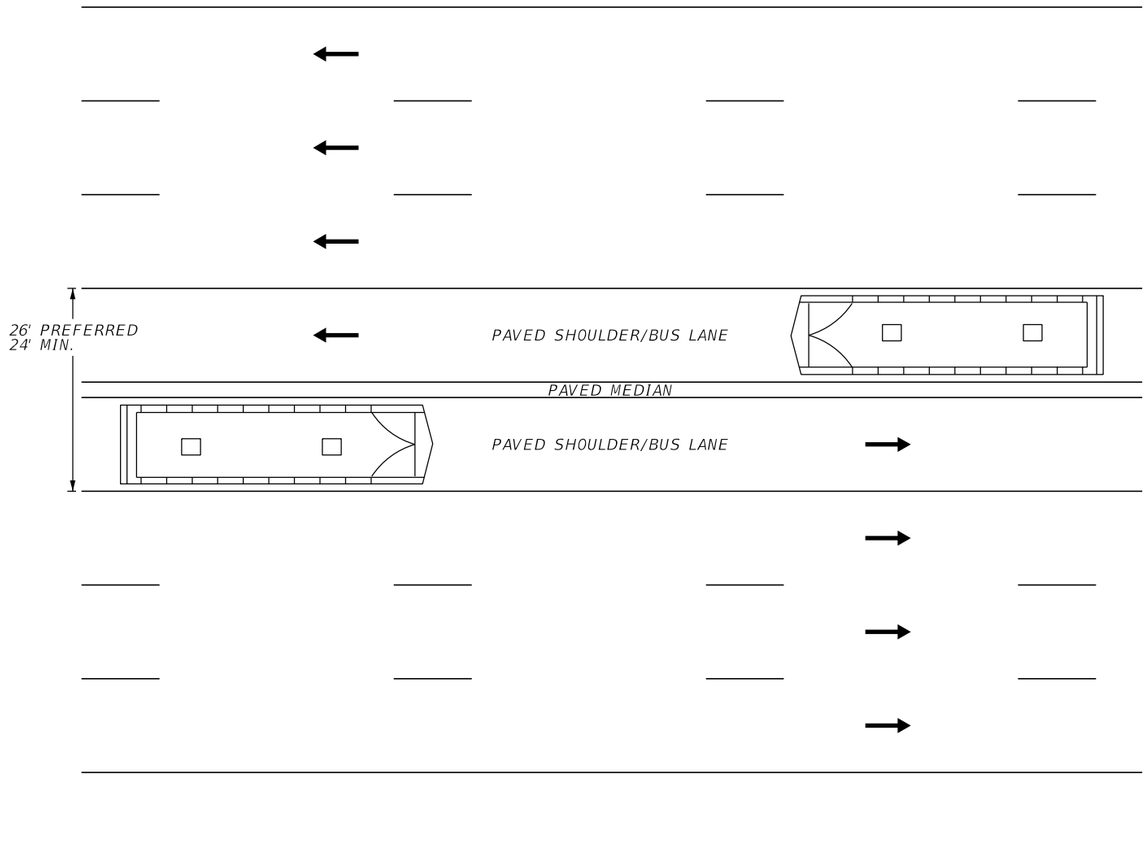
$TIMES
$DATES

GENERAL NOTES

1. CURB TYPE IS SHOWN ONLY AS AN ILLUSTRATION.
2. FOR PAVEMENT MARKINGS SEE INDEX 17346 AND 17347.
3. FOR SHELTER AND SHELTER PAD DETAILS, REFER TO SHEET 6.



BUS-ON-SHOULDER (BOS) OPERATIONS UN-INTERRUPTED FLOW
HIGHWAY, RIGHT HAND SHOULDER



BUS-ON-SHOULDER (BOS) OPERATIONS UN-INTERRUPTED FLOW
HIGHWAY, LEFT HAND SHOULDER

$TIMES
$DATES