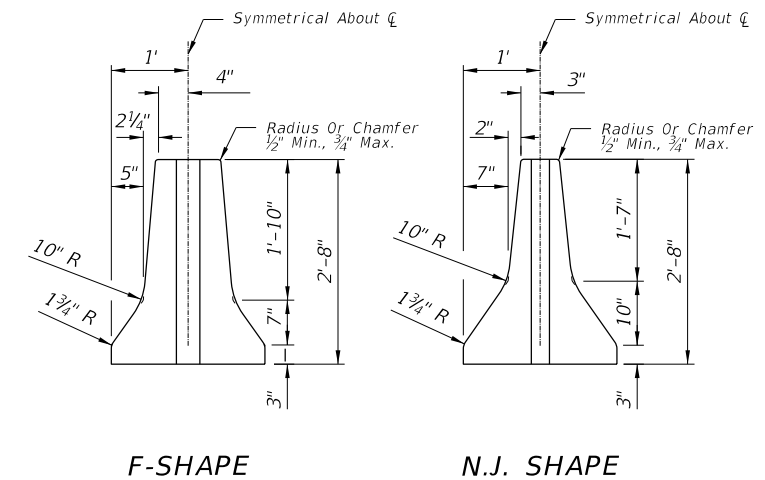


GENERAL NOTES

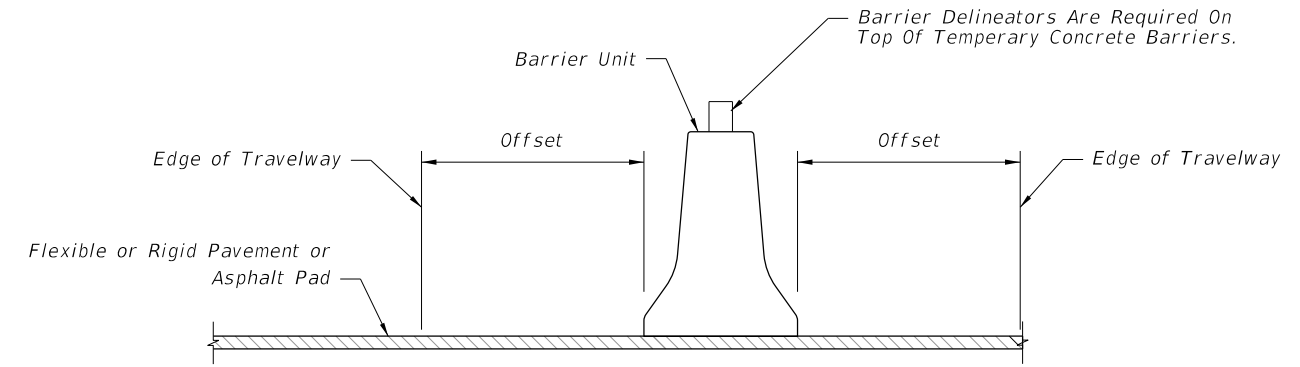
1. Temporary concrete barrier systems on roadways may be any of the following:
 - a. The FDOT Type K Temporary Concrete Barrier system (Design Standard Index 414). F-Shape Units. For temporary concrete barrier systems on bridges see Design Standard Index No. 414.
 - b. Proprietary temporary concrete barrier systems meeting NCHRP Report 350 Test Level 3 criteria which are included on the Approved Products List.
2. Barrier units of dissimilar types may be interconnected within a single line barriers using transition units.
3. Alignment, length of need, anchorage and end treatment shall be in accordance with this Index.
4. Temporary concrete barrier units shown herein shall not be used for permanent barrier construction regardless of unit length.
5. If the plans specify Barrier (Temporary) (Type K), substitution with other barrier types is not permitted.
6. If the plans specify temporary concrete barrier ~~with~~ **CHANGED TO: 50'** water filled barriers is not permitted.
7. Where existing flexible pavement is not present, construct a minimum 2" thick temporary Asphalt Pad using Miscellaneous Asphalt Pavement in accordance with Specification Section 339 with the exception that the use of a pre-emergent herbicide is not required. No separate payment will be made for the Asphalt Pad.
8. Barrier Delineators meeting the requirements of Specifications Section 993 are to be mounted on top of temporary concrete barriers that are used as barriers along traveled ways in work zones. The barrier delineators are to be spaced at ~~30'~~ **CHANGED TO: 100'** centers in transitions, and ~~80'~~ on all other locations. Color must match adjacent longitudinal pavement marking.
9. Barrier units used for work zone traffic control and other temporary applications shall be paid for under the contract unit price for Barrier (Temporary), LF.
10. Deflect ~~any~~ **ADDED: alignment** any grass, construction debris, stockpiled material or other objects.
11. Placing alternate temporary barrier systems with heights greater than 32 inches within the work zone may obstruct the clear sight distance at intersections and driveways. Prior to placing these barrier systems, the contractor shall submit a Certification Statement that the clear sight distance meets the requirements of Index 546, signed and sealed by a Florida Professional Engineer.
12. Minimum temporary concrete barriers installed per run shall be 16 units.



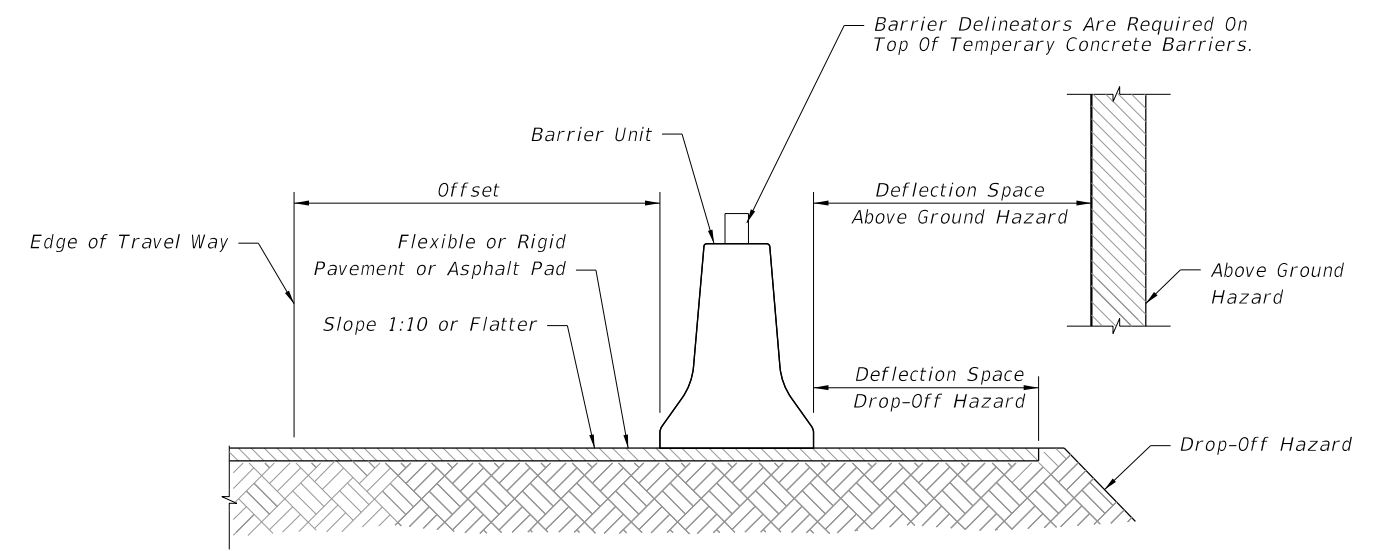
F-SHAPE N.J. SHAPE

END VIEWS

REINFORCEMENT AND OTHER UNIT FABRICATION DETAILS NOT SHOWN.
PERMITTED BARRIER UNIT END VIEWS



MEDIAN INSTALLATION



ROADWAY INSTALLATION

OFFSET AND DEFLECTION SPACE REQUIREMENTS				
Installation	Shielding	Work Zone Speed	Offset to Travelway	Deflection Space
Left or Right Shoulder	Above Ground Hazards	45 mph or Less	1' min, 2' preferred	2' min.
		50 mph and Greater	2' min, 4' preferred	4' min.
	Drop-Off Hazards	45 mph or Less	1' min, 2' preferred	2' min.
		50 mph and Greater		
		a. Drop-offs 4' or Less and NO traffic below	2' min, 4' preferred	2' min.
b. All drop-off conditions other than 'a'	2' min, 4' preferred	4' min.		
Separating Traffic	Adjacent Opposing Traffic	45 mph or Less	1' min, 2' preferred	1' min., 2' preferred
		50 mph and Greater	2' min, 4' preferred	2' min., 4' preferred

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