

L<sub>2</sub> Index No. 301 \*

\* Add 70' For Each Additional WB-40 Expected In Storage

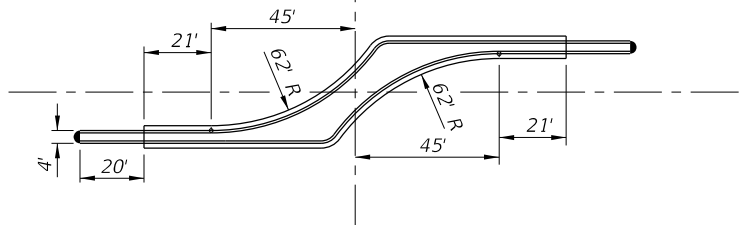
NOTE: Return configurations for each quadrant must be analyzed independently to assure adequate return pavement for semi-trailer inside tracking and for 4' minimum clearance between trucks making opposing movement. The depicted design only applies where roads and streets intersect at 90° to the mainline and have centerlines common with the opposing road or street. Swept paths are by AutoTURN 4.0 for the AASHTO 2001 SU and WB-40 tractor-semitrailer.

RETURNS:

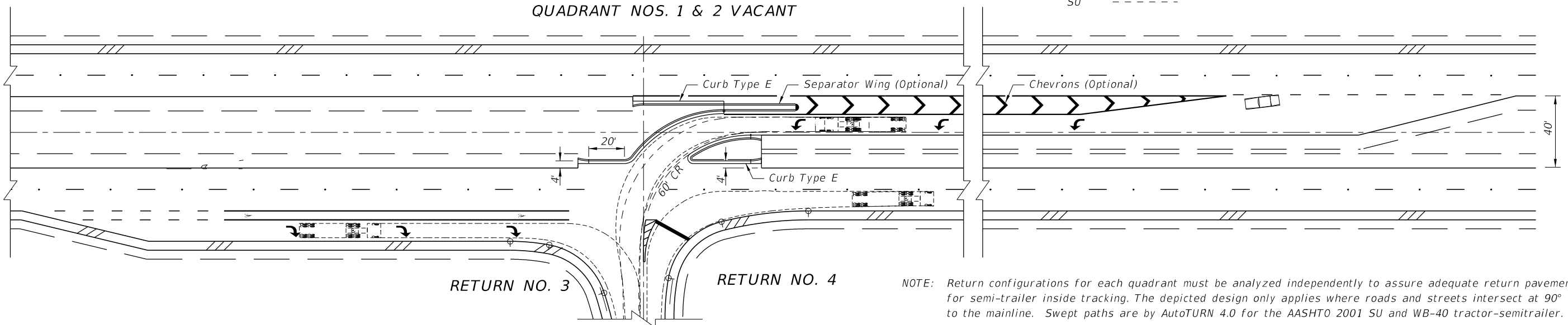
Returns Depicted:  
 Three Centered Compound Curves For All Returns Depicted:  
 120'-40'-200' Radii; 2' And 8' Offsets  
 Simple Curve With Tapers Not Shown:  
 40' Radius; 1:15 And 1:8 Tapers With  
 2' And 8' Offsets Tested (Practical Fit)

SWEPT PATH LEGEND:

WB 40 -----  
 SU - - - - -



QUADRANT NOS. 1 & 2 VACANT

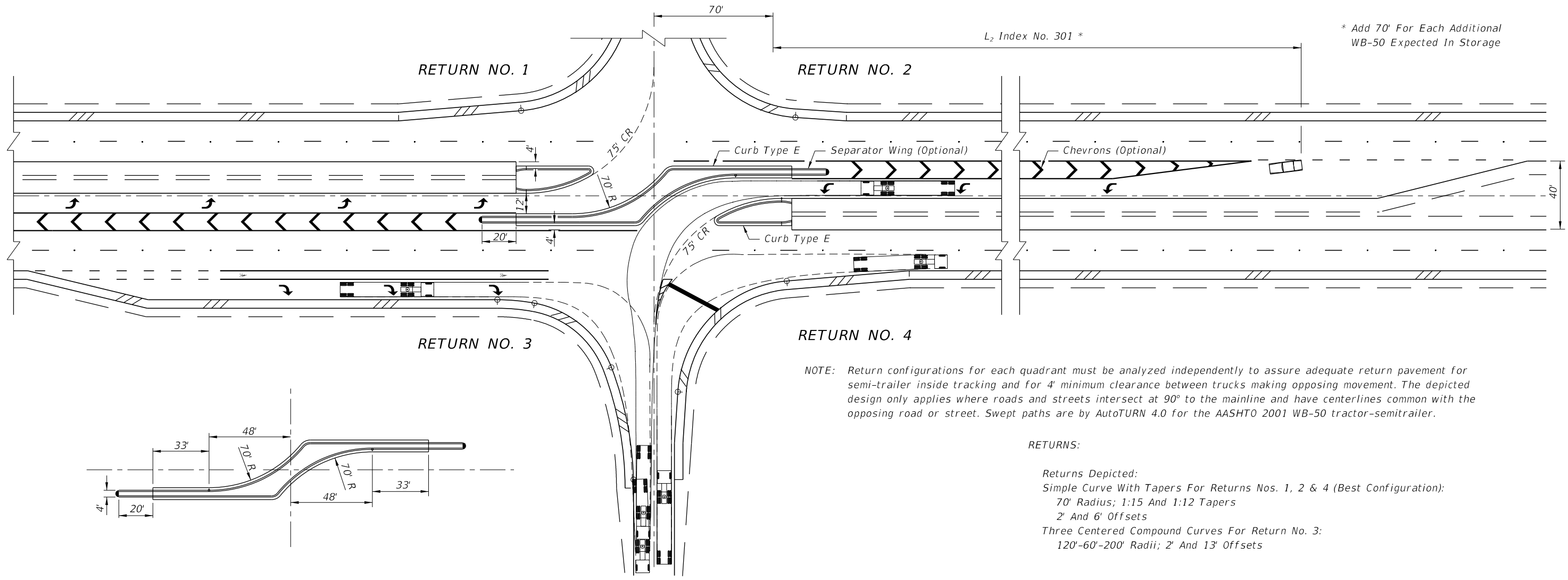


NOTE: Return configurations for each quadrant must be analyzed independently to assure adequate return pavement for semi-trailer inside tracking. The depicted design only applies where roads and streets intersect at 90° to the mainline. Swept paths are by AutoTURN 4.0 for the AASHTO 2001 SU and WB-40 tractor-semitrailer.

40' MEDIAN • 4-LANE DIVIDED • PARALLEL TURN BAY • 2001 AASHTO SU & WB-40

12/31/2015 9:24:42 AM

LAST REVISION 07/01/04	REVISION	DESCRIPTION:	 FY 2016-17 DESIGN STANDARDS	DIRECTIONAL MEDIAN OPENINGS	INDEX NO. 527	SHEET NO. 1 of 3
---------------------------	----------	--------------	------------------------------------	-----------------------------	------------------	---------------------



\* Add 70' For Each Additional WB-50 Expected In Storage

L<sub>2</sub> Index No. 301 \*

RETURN NO. 1

RETURN NO. 2

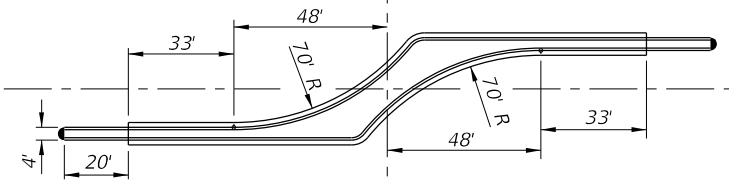
RETURN NO. 3

RETURN NO. 4

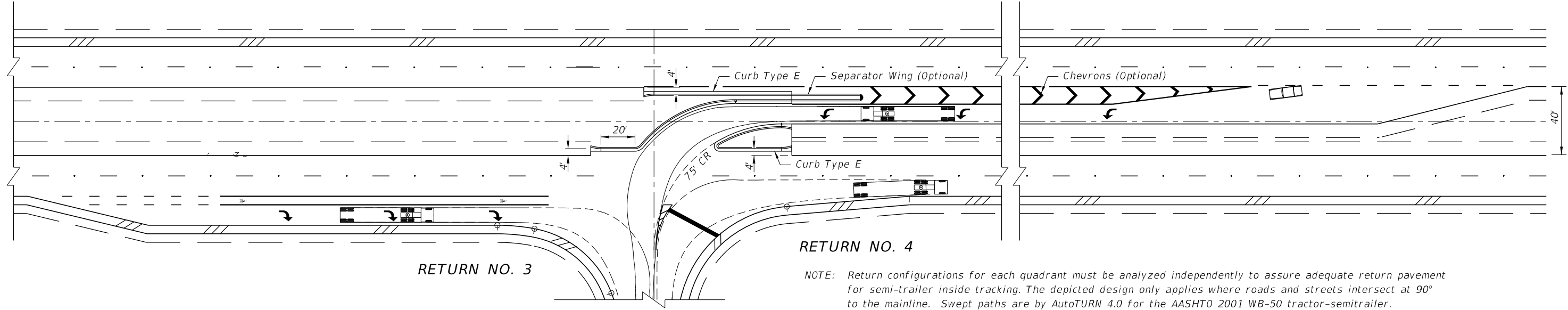
NOTE: Return configurations for each quadrant must be analyzed independently to assure adequate return pavement for semi-trailer inside tracking and for 4' minimum clearance between trucks making opposing movement. The depicted design only applies where roads and streets intersect at 90° to the mainline and have centerlines common with the opposing road or street. Swept paths are by AutoTURN 4.0 for the AASHTO 2001 WB-50 tractor-semitrailer.

RETURNS:

- Returns Depicted:
- Simple Curve With Tapers For Returns Nos. 1, 2 & 4 (Best Configuration):  
70' Radius; 1:15 And 1:12 Tapers  
2' And 6' Offsets
- Three Centered Compound Curves For Return No. 3:  
120'-60'-200' Radii; 2' And 13' Offsets



QUADRANT NOS. 1 & 2 VACANT

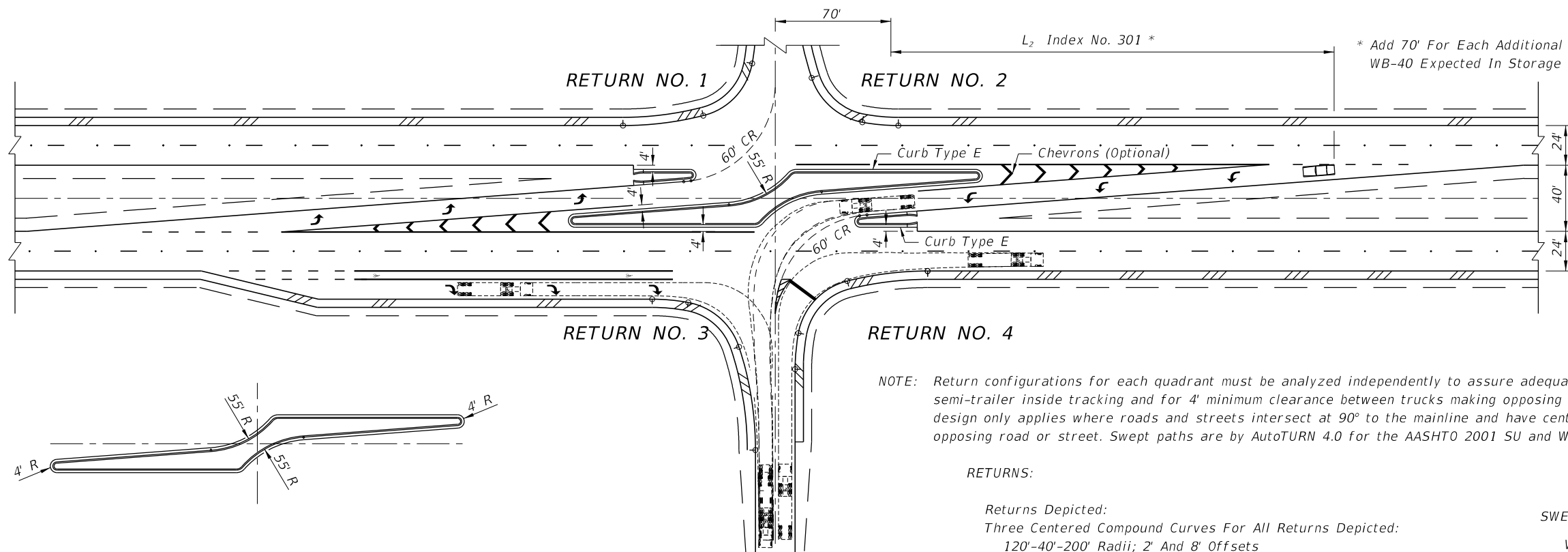


NOTE: Return configurations for each quadrant must be analyzed independently to assure adequate return pavement for semi-trailer inside tracking. The depicted design only applies where roads and streets intersect at 90° to the mainline. Swept paths are by AutoTURN 4.0 for the AASHTO 2001 WB-50 tractor-semitrailer.

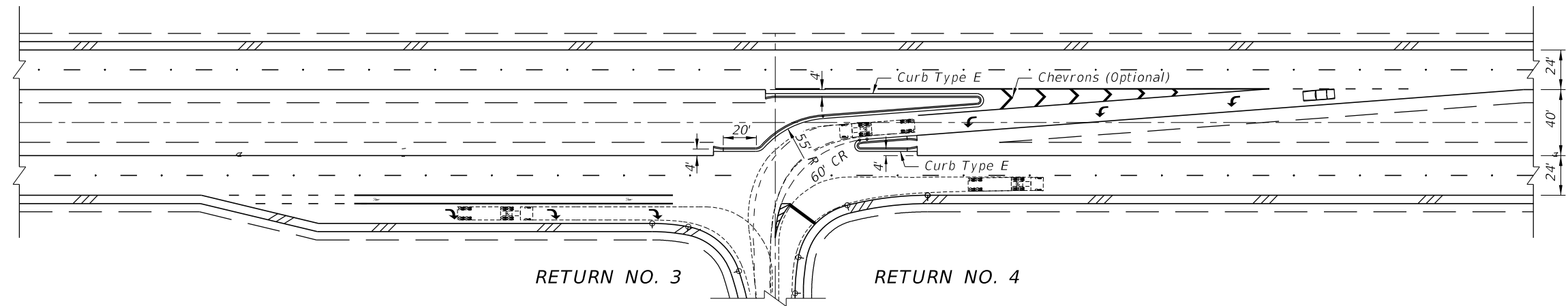
40' MEDIAN 4-LANE DIVIDED • PARALLEL TURN BAY • 2001 AASHTO WB-50

12/31/2015 9:24:43 AM

LAST REVISION 07/01/04	REVISION	DESCRIPTION:	 FY 2016-17 DESIGN STANDARDS	DIRECTIONAL MEDIAN OPENINGS	INDEX NO. 527	SHEET NO. 2 of 3
---------------------------	----------	--------------	------------------------------------	-----------------------------	------------------	---------------------




QUADRANT NOS. 1 & 2 VACANT



40' MEDIAN • 4-LANE DIVIDED • TAPERED TURN BAY • 2001 AASHTO SU & WB-40

12/31/2015 9:24:44 AM

LAST REVISION 07/01/04	REVISION	DESCRIPTION:	 FY 2016-17 DESIGN STANDARDS	DIRECTIONAL MEDIAN OPENINGS	INDEX NO. 527	SHEET NO. 3 of 3
---------------------------	----------	--------------	--	-----------------------------	------------------	---------------------