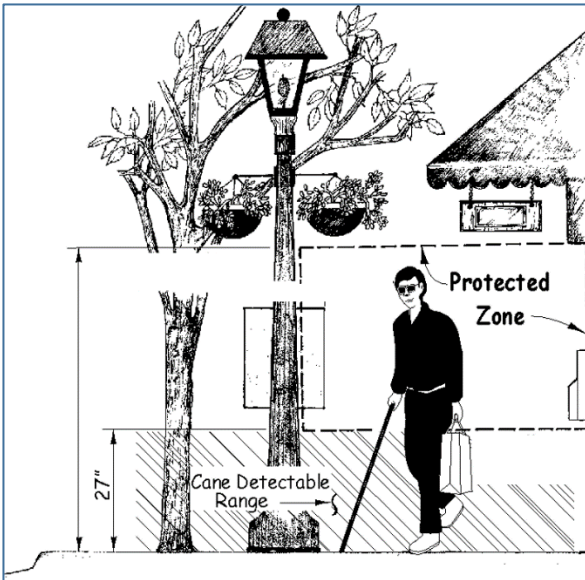


## Signs, Street Furniture, & Landscaping

These may obstruct the walking path or become protruding objects if located within, adjacent to, or over a sidewalk. Signposts, utility poles, other street equipment, or furniture must not obstruct the clear width of the *entire* walking surface within a full height of 7'-0" above the walking surface (FDM 222.2.1.2). Vegetation must be kept at  $\geq$  8'-6" above the walking surface (see MRP). Reduction of the standard clear, unobstructed walking surface width is generally not permitted, except where **no practical alternative exists**. A width reduction to 36" is only allowed at an aboveground fixed utility (AFU) that cannot be relocated. A further reduction to 32" for no more than 24" in length is permitted when there is **no practical alternative available** to avoid the obstruction. (See FDM 222.2.1.1)



*Intent is to remove objects that may protrude into the walking surface, reducing access and possibly causing injury.*



## What is the ADA?

The ADA is a federal civil rights law that requires pedestrian facilities to be accessible to and usable by all users, including persons with disabilities.

## Roadway Features Addressed by the ADA

The basic requirements of the ADA and accessibility issues related to the design, construction, and maintenance of streets and roadways are described in this brochure. It is intended as a quick-reference guide for field personnel on roadway projects to assist with installation of accessible features on public sidewalks.

Pedestrian features that are designed and constructed to meet the accessibility criteria described in this brochure, and applicable references, will be accessible to and usable by persons with disabilities.



For more information, please contact:

### Florida Department of Transportation

ADA/Accessibility Program  
605 Suwannee St., MS40  
Tallahassee, FL 32399-0450  
(850) 414-4295

[www.fdot.gov/Roadway/ADA](http://www.fdot.gov/Roadway/ADA)



## Accessible Roadside Pedestrian Facilities

Design, Construction, & Maintenance  
in Compliance with the  
Americans with Disabilities Act (ADA)

The following accessibility issues on public transportation projects are covered in this brochure:

- Sidewalks
- Street Crossings
- Pedestrian Pushbuttons
- Curb Ramps & Detectable Warnings
- Signs, Street Furniture, & Landscaping

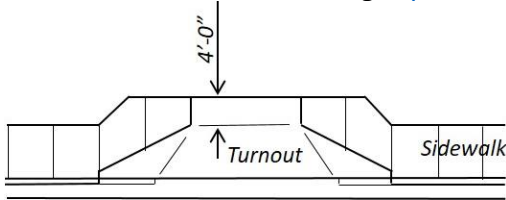


Florida Department of Transportation  
ADA/Accessibility Compliance Program

## Sidewalks

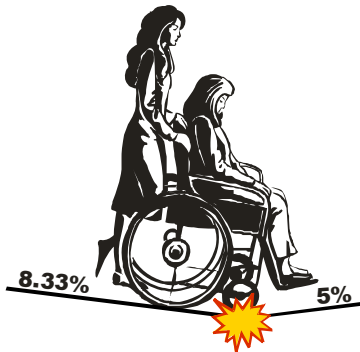
Standard sidewalk width varies, with the minimum width for new construction of **5'-0"** in C1 & C2 context classification. For RRR projects, unaltered sidewalk width  $\geq$  **4'-0"** may be retained within any context classification. (See [FDM](#) Table 222.2.1)

Walkarounds at driveways must be  $\geq$  **4'-0"** wide ([Index](#) 522-003). In all cases, walking surface cross slope cannot exceed **2.0%**. Sidewalk surfaces must be firm, stable, & slip-resistant (see surface **Finishing**, [Spec](#) 522-7).



## Street Crossings

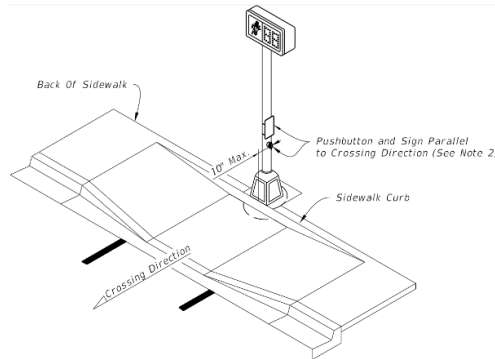
Curb ramps must be wholly contained within crosswalk markings. Crossings must meet the same grade and cross slope criteria as sidewalks ([FDM](#) 222.2.2). Consider providing a 2'-long level space at the counterslope if change in slope from a curb ramp to the street surface exceeds **11.3%** (see below).



Algebraic difference must be no greater than 13.3%; recommend providing 24" level area at grade break where algebraic difference will be  $\geq$  11.3%.

## Pedestrian Pushbuttons

Accessibility of ped buttons is determined by (1) button height and horizontal side reach; (2) level maneuvering clear space; and, (3) button & sign orientation. The ped button must be no more than **42"** above the walking surface with a horizontal reach distance  $\leq$  **10"** from the edge of the clear area ([Index](#) 665-001). A level, clear area (with long dimension centered on the ped button) must have footprint dimensions  $\geq$  **30"x48"** with surface slopes  $\leq$  **2.0%** in all directions ([FDM](#) 222.2.2). Button orientation shall be as shown in the Figure below and as described in Note 2. For Note 3, pushbutton and sign placement must not reduce the clear space of the landing.

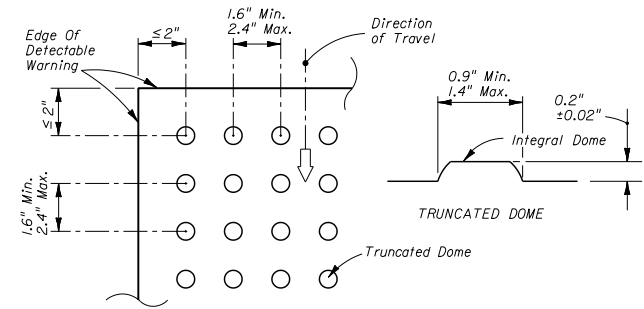


### NOTES:

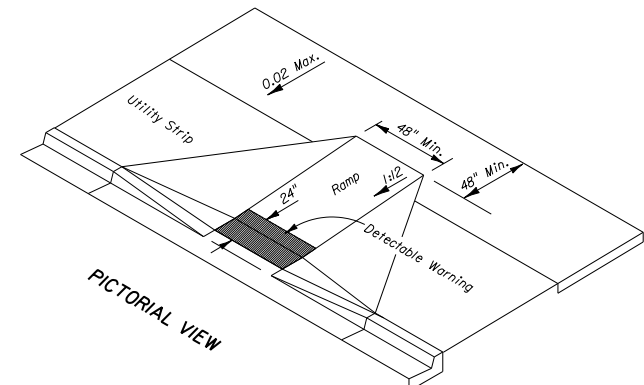
1. Mount Signs above the detectors. See [Index](#) 700-102 for sign details.
2. Install Pushbuttons and Pedestrian Actuation Signs with faces parallel to the crossing direction, see [DETAIL "B"](#).
3. Mount pushbuttons and Signs in accordance with [Specification](#) 665.
4. Pushbutton mounting height shown is taken at the center of the actuation switch.
5. for pedestrian control signal see [index](#) 653-001.
6. For Aluminum Pole and Pedestal supports see [Index](#) 646-001.

## Curb Ramps & Detectable Warnings

Curb ramp running slope must not exceed **1:12 (8.3%)** and cross slope must not exceed **2.0%** ([Index](#) 522-002). A level landing must be located at the top that is at least **4'** deep and at least as wide as the curb ramp. Curb ramps are to be the same width as the sidewalk, where practicable ([FDM](#) 222.2.2). All sidewalk curb ramps must have detectable warnings, of contrasting color (e.g., red, black, or yellow per [Spec](#) 527), that are placed within **5'** of back of curb and extend the full width of the walking surface ([Index](#) 522-002). "Truncated domes" are the only acceptable detectable warning pattern, as shown below (see [FDOT's APL](#)).



### DETECTABLE WARNINGS



<a href="#">MRP</a> - Maintenance Rating Program Handbook	<a href="#">APL</a> - Approved Products List
<a href="#">FDM</a> - <a href="#">FDOT Design Manual</a>	<a href="#">Context Class</a> - transportation context & land use description
<a href="#">Index</a> - Standard Plans	
<a href="#">Spec</a> - Stnd. Specifications	