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DESCRIPTION

SKT Terminal Single Anchor

DRAWING NAME

SKT-SP-S-375-FL

SKT-SP NOTES

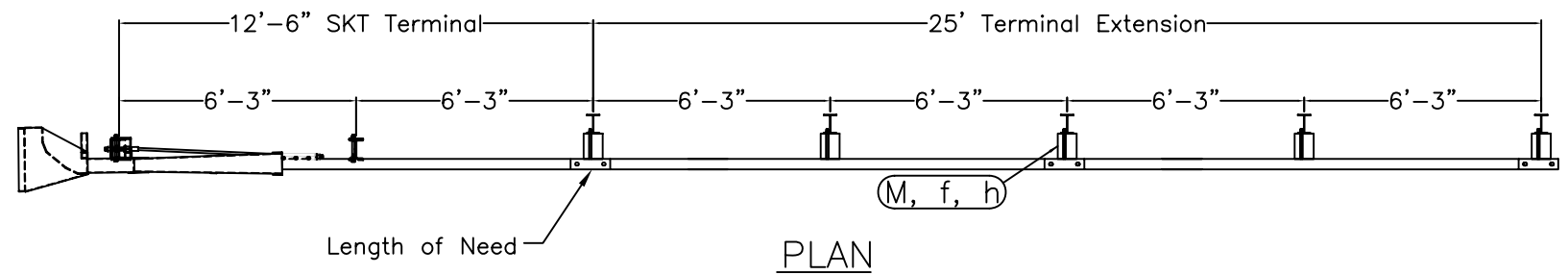
1. The guardrail end anchorage system represented on these QPL drawings is a proprietary design by Road Systems, Inc. and marketed under the trade name SKT-SP. Any infringement on the rights of the designer shall be the sole responsibility of the user.
2. This drawing is sufficient for plan details for the SKT-SP when installed in connection with shoulder guardrail and precludes the requirement for shop drawing submittals unless the plans otherwise call for such submittals. The SKT-SP shall be assembled in accordance with the manufacturer's detailed drawings, procedures and specifications.
3. The SKT-SP is intended for use as an approach end guardrail anchorage for shoulder guardrail located parallel to traffic lanes. The effective length of the SKT-SP is 37.5'. The alignment of the SKT-SP is an extension of the normal guardrail alignment, except when constructed with curb the alignment of the SKT-SP will be flared over the first 25' at a rate of 1:25. If attaching the SKT-SP to a transition or any other non standard W-Beam, an additional 12'-6" of standard W-Beam rail is needed.
4. The SKT-SP can not be used in medians where horizontal clearance requires the use of a backrail.
5. Posts at location Nos. 1 and 2 are special hinged posts (see Vendor detail). The posts at location Nos. 3, 4, 5, 6, and 7 are standard steel W6" x 8.5# (or 9#) posts.
6. For galvanizing requirements of metallic components see Standard Specification Section 967.
7. For details of shoulder widening, miscellaneous asphalt pavement and pay limits see Index 400.
8. If the plans call for the SKT-SP at a specific location, substitution with other end anchorage assemblies will not be permitted unless approved by the Engineer. If the plans call for end anchorage assembly 'parallel' at a specific location, the contractor has the option to construct any FDOT approved parallel assembly. Where a flared end anchorage is called for in the plans, any approved substitution with a parallel end anchor will not be eligible for VECP consideration.
9. The SKT-SP shall be paid for under the contract unit price for Guardrail, End Anchorage Assembly (Parallel), EA and shall be full compensation for furnishing and installing all components in accordance with the plans; the manufacturer's detail drawings, procedures and specifications and this drawing.

STATE OF FLORIDA DEPARTMENT OF TRANSPORTATION

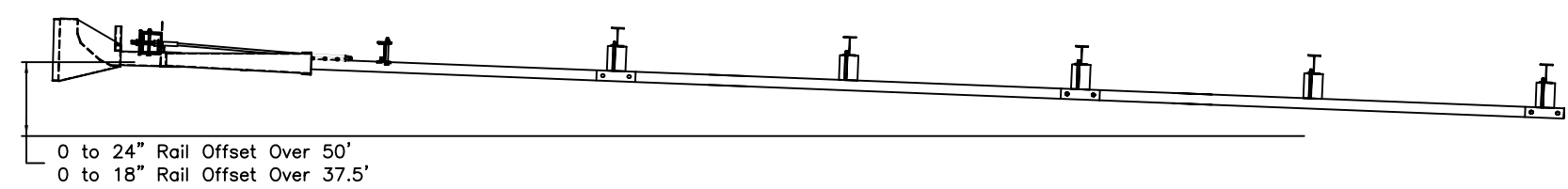
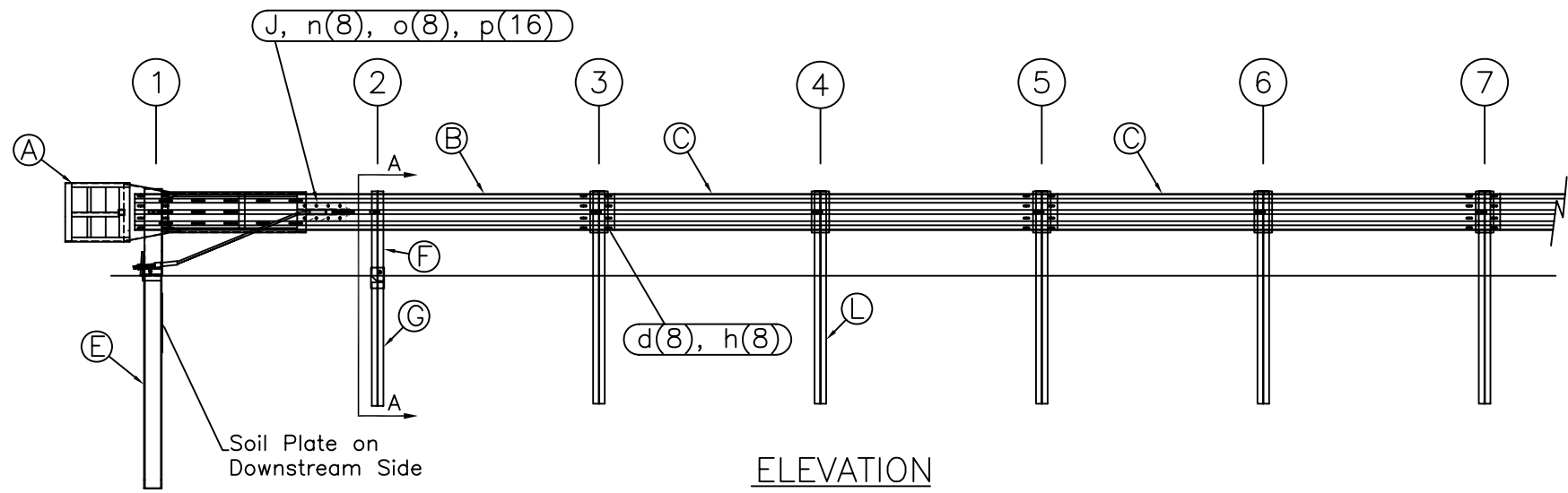
SKT-SP

DATE:
August, 2009

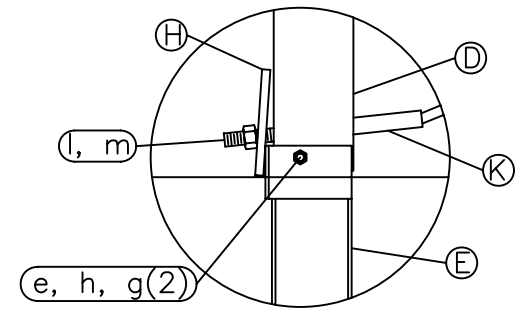
QPL No. S536-0216



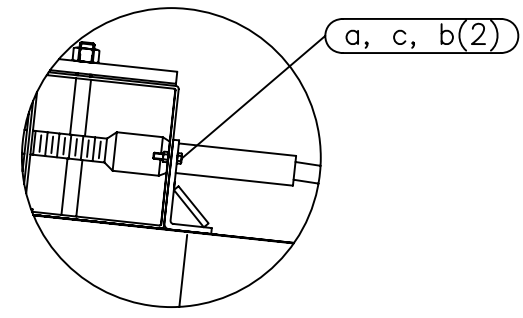
TRAFFIC →



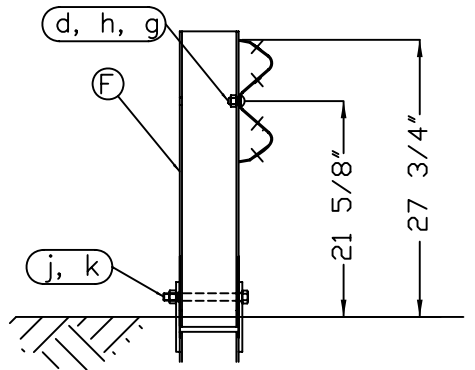
OPTIONAL FLARED INSTALLATION
25:1 maximum flare rate



Post #1 Connection Detail



Impact Head Connection Detail



SECTION A-A
Post #2

ITEM	QTY	BILL OF MATERIALS	ITEM NO.
A	1	IMPACT HEAD	S3000
B	1	W-BEAM GUARDRAIL END SECTION, 12 Ga.	SF1303
C	2	W-BEAM GUARDRAIL END, 12 Ga.	G1203
D	1	FIRST POST TOP (6X6X $\frac{1}{2}$ " Tube)	TPHP1A
E	1	FIRST POST BOTTOM (6' W6X15)	TPHP1B
F	1	SECOND POST ASSEMBLY TOP	HP2A
G	1	SECOND POST ASSEMBLY BOTTOM	HP3B
H	1	BEARING PLATE	E750
J	1	CABLE ANCHOR BOX	S760
K	1	BCT CABLE ANCHOR ASSEMBLY	E770
L	5	W6X9 GUARDRAIL POST	P621
M	5	TIMBER BLOCKOUT	P675
HARDWARE (ALL DIMENSIONS IN INCHES)			
a	2	5/16 x 1 HEX BOLT GRD 5	B5160104A
b	4	5/16 WASHER	W0516
c	2	5/16 HEX NUT	N0516
d	9	5/8 Dia. x 1 1/4 SPLICE BOLT (POST #2)	B580122
e	1	5/8 Dia. x 9 HEX BOLT GRD 5	B580904A
f	5	5/8 Dia. x 10 H.G.R. BOLT	B581002
g	3	5/8 WASHER	W050
h	15	5/8 Dia. H.G.R. NUT	N050
j	1	3/4 Dia. x 8 1/2 HEX BOLT GRD A449	B340854A
k	1	3/4 Dia. HEX NUT	N030
l	2	1 ANCHOR CABLE HEX NUT	N100
m	2	1 ANCHOR CABLE WASHER	W100
n	8	CABLE ANCHOR BOX SHOULDER BOLT	SB58A
o	8	1/2 A325 STRUCTURAL NUT	N055A
p	16	1 1/16 OD x 9/16 ID A325 STR. WASHER	W050A

GENERAL NOTES:

1. All bolts, nuts, cable assemblies, cable anchors and bearing plates shall be galvanized.
2. The lower sections of the Posts 1&2 shall not protrude more than 4 in above the ground (measured along a 5' cord). Site grading may be necessary to meet this requirement.
3. The lower sections of the hinged posts should not be driven with the upper post attached. If the post is placed in a drilled hole, the backfill material must be satisfactorily compacted to prevent settlement.
4. A site evaluation should be considered if there is less than 25' between the outlet side of the terminal and any adjacent driving lane.
5. The breakaway cable assembly must be taut. A locking device (vice grips or channel lock pliers) should be used to prevent the cable from twisting when tightening nuts.
6. See installation manual for detailed installation directions.



SKT Terminal 27-3/4" Top of Rail Single Anchor 7 Post System		Sheet:	1
		Date:	04/30/09
Drawing Name: SKT-SP-S-375-FL		By:	JRR
		Rev:	0
Scale: None			