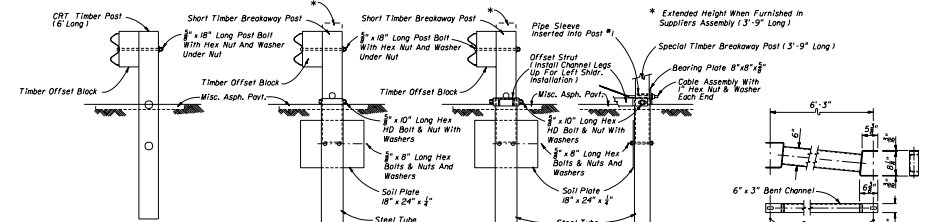


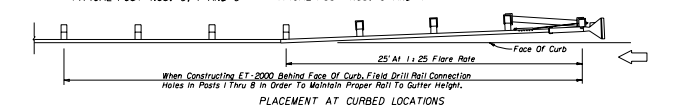
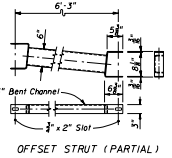
ET-2000 NOTES

- The guardrail end anchorage system represented on this standard drawing is a proprietary design by Syra, Inc. and marketed under the trade name ET-2000. Any infringement on the rights of the designer shall be the sole responsibility of the user.
- This standard drawing is produced by the Florida Department of Transportation solely for the use by the Department and its assignees. This standard drawing provides the general graphics and information necessary to field identify component parts of the ET-2000 and their incorporation into a whole system.
- This standard drawing is sufficient for plan details for the ET-2000 when installed in connection with shoulder guardrail and precludes the requirement for shop drawing submittals unless the plans otherwise call for such submittals. The ET-2000 shall be assembled in accordance with the manufacturer's detailed drawings, procedures and specifications.
- The ET-2000 is intended for use as an approach end guardrail anchorage for shoulder guardrail located parallel to travel or auxiliary lanes. The effective length of the ET-2000 is 50' including two 25' W-beam spans of guardrail. The effective length is outside of any standard guardrail, guardrail transitions or other special treatments. The ET-2000 alignment is an extension of the normal guardrail alignment, except when constructed with curb alignment of the ET-2000 will be flared over the first 25' of a rate of 1:25.
- The ET-2000 can not be used in medians where horizontal clearance requires the use of a beacon.
- Posts at location Nos. 1, 2, 3 and 4 must be timber breakaway posts with steel foundation tubes. The breakaway posts of location Nos. 5, 6, 7 and 8 may be constructed as shown in Section CC or may utilize timber breakaway posts with steel foundation tubes as shown in Section BB.
- See the General Notes for galvanizing requirements of metallic component.
- If the plans call for the ET-2000 of a specific location, substitutions with other end anchorage assemblies will not be permitted unless approved by the Engineer. If the plans call for an anchorage assembly parallel or a specific location the contractor has the option to construct any FDOT approved parallel assembly. Where a flared end anchorage is called for in the plans, any approved substitution with a parallel end anchorage will not be eligible for VEDC consideration.
- The ET-2000 shall be paid for under the contract unit price for Guardrail, End Anchorage Assembly (Parallel), EA and shall be full compensation for furnishing and installing all components in accordance with the plans, the manufacturer's detail drawings, procedures and specifications and this index.

Standard Guardrail, Special Transition Or Other Special Treatment
 Do Not Attach Rail To Block At Post No. 5 And Roll To Post At Post No. 1



Notes: Post No. 5 is similar to Section CC except rail is not attached to post and block.
 SECTION CC TYPICAL POST NOS. 6, 7 AND 8
 SECTION BB TYPICAL POST NOS. 3 AND 4
 SECTION AA POST NO. 2
 PARTIAL VIEW POST NO. 1



DESIGN NOTES

- A special site evaluation should be considered prior to using the ET-2000 where there is less than 25' clear area on the extrusion side (back side) of the ET-2000.
- The ET-2000 is suitable for all design speeds.

END ANCHORAGE ASSEMBLY TYPE ET-2000

STATE OF FLORIDA DEPARTMENT OF TRANSPORTATION
 ROAD DESIGN

Revised	Date	Approved By
DESIGNED BY	07/95	STATE HIGHWAY DESIGN DIVISION
DRAWN BY	08/95	STATE HIGHWAY DESIGN DIVISION
CHECKED BY	08/95	00
		25 OF 31
		400