

 Hollywood, FL

 June 13-14, 2024



# 2024 TRANSPORTATION SYMPOSIUM

## FDOT Freight Mobility Initiatives in Miami-Dade County

Daniel Lameck &  
Armando Moscoso

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& RAIL**



**SAFETY**



**COMMUNITIES**



**WORKFORCE  
DEVELOPMENT**



**RESILIENCY**



**TECHNOLOGY**



**ROBUST SUPPLY CHAIN**

# Agenda

- **Freight Improvement Initiatives**
- **Truck Parking**
- **Federal Funding Opportunities**
- **National Highway Freight Program (NHFP) Overview**
- **NHFP Funding**
- **Project Selection**
- **Q&A/Discussion**



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# Freight Improvement Initiatives

# Freight Industrial Sub-Areas

District 6 identified the following freight-intensive sub-areas:

- Town of Medley
- City of Opa-Locka
- Port of Miami River Corridor
- City of Doral
- City of Miami Gardens
- City of Hialeah
- City of Homestead
- Monroe County



# Freight Sub-Area Studies

- FDOT performed planning level studies that assessed freight accessibility to and from each sub-area and develop strategies to enhance access and improve freight mobility. FDOT has completed all Sub-area Freight Improvement Studies.
- FDOT is currently performing one more additional Freight Study that will be countywide and look at various type of updated improvements we can make to our sub-areas. This will be known as the Miami-Dade Freight Village Countywide Analysis.
- This study will serve the purpose of providing a comprehensive updated Project Bank of freight-specific and freight-oriented projects.



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# Freight Village Countywide Analysis

## Provide

Provide an updated profile of the state of freight and logistics in District Six in terms of transportation assets, demand and usage, and programmed improvements.

## Describe

Describe the policy, regulatory, and funding framework for freight planning.

## Characterize

Characterize history, emerging, and anticipated trends driving freight and logistics activity.

## Examine

Examine various freight modal system components as to their resources, current funding, performance, needs outlook, shortfalls, and options and strategies for addressing future requirements.

## Devise

Devise potential solutions to identified needs, not just for infrastructure projects, but more broadly in terms of strategies and innovations to meet expected demand and mitigate current shortfalls.

## Develop

Develop a project bank of freight infrastructure projects, prioritized based on how effectively each project meets existing and future freight transportation.



# Truck Parking



# THE TRUCK PARKING CHALLENGE

**TRUCK DRIVERS  
RANK PARKING A  
TOP 5 INDUSTRY  
ISSUE SINCE 2015**  
(AMERICAN TRANSPORTATION  
RESEARCH INSTITUTE)

**98%** of truck drivers report problems finding safe parking, costing drivers more than **56 minutes** of drive time. That wasted time is estimated to cost drivers **\$5,500** per year – roughly a **12%** pay cut. (American Transportation Association and Owner Operator Independent Drivers Association)

**58%** of drivers say they have parked in unauthorized places at least **three** times a week. (American Transportation Research Institute)



**\$5.1 Billion Lost**

Finding safe truck parking is a looming issue that can cost the trucking industry an estimated **\$5.1** billion annually.

(Trucker Path)



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# Truck Parking

- FDOT completed a Miami-Dade County Truck Parking supply and demand Master Plan in 2022. The objectives were:
- Evaluated truck parking supply and needs for Miami-Dade County
- Focused on the regional, short-haul truck flows that drive parking demand in South Florida.
- Performed an inventory of existing legal and illegal truck parking supply within the County
- Forecasted truck parking demand for 2045
- Coordinated with stakeholders to get support for parcel development



# Truck Parking

## Challenges

- Increased use of industrial lands for development of distribution centers –less remaining for truck parking
- Increasing Land values limit truck parking development
- Overflow of truck parking from busy industrial facilities results in unauthorized parking

## Opportunities

- County designated lands outside UDB
- Shared use truck parking
- Real-time Truck Parking Availability Systems



# Truck Parking

- District Six will continue addressing the goals of the Master Plan
  - Site Feasibility Analysis is a core goal
  - Analysis will identify feasible brownfields, industrial, and vacant parcels that could be developed as truck parking facilities
- The Study was completed in Spring 2024
  - Implementation of Truck Parking site scheduled to in FY 28



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# Golden Glades Truck Travel Center

- 135 truck parking spaces, fueling stations, travel center, truck repair facility, truck wash, and scale, Truck Parking Availability System (TPAS), security features, and Intelligent Transportation Systems (ITS)
- Scheduled to start construction after completion of Golden Glades Interchange Project



## The Wedge Parcel:

- Potential 316 total truck parking spots
- 30 EV truck parking.
- Scheduled to start PD&E study 2028



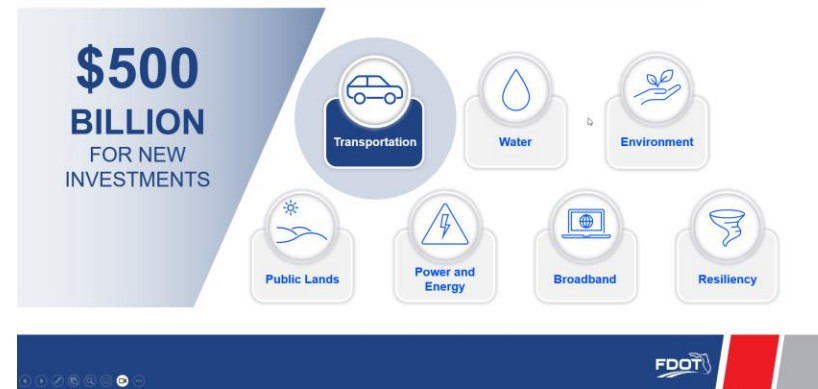
A red semi-truck is shown driving on a road towards the right. The background features a sunset sky with a yellow and orange glow over a dark horizon. In the foreground, a large, stylized gear graphic is partially visible, with a red circular area on its left side. The text "Federal Funding Opportunities" is written in white, bold, sans-serif font within this red area.

# Federal Funding Opportunities

# Federal Funding Opportunities

- The Bipartisan Infrastructure Law (BIL) includes
  - \$973 billion in funding over five years from FFY 2022 through FFY 2026,
  - \$550 billion for new investments for all modes of transportation
- Most federal transportation grants are broken into planning and capital grants
  - Planning grants may not have a minimum funding amount and may not require a match
  - FDOT encourages partners to apply for planning grants and capital grants.
  - FDOT generally will only apply for capital grants

## BIL Investments





# Freight Federal Funding Opportunities

- **\$1.8 billion for the National Infrastructure Project Assistance (Mega) program**
- **\$3.1 billion for the Infrastructure for Rebuilding America (INFRA) program**
- **\$675 million for the Rural Surface Transportation Grant (Rural) program**



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# National Highway Freight Program Overview



# History

**In December 2015, the Fixing America's Surface Transportation Act (FAST Act) was signed into law**

- The FAST Act established the new National Highway Freight Program (NHFP)



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# Federal Policy

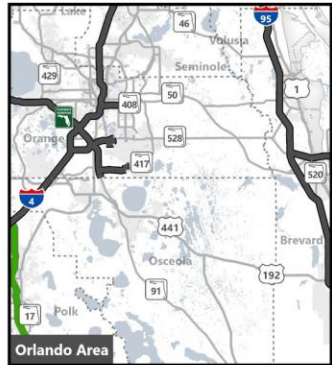
- Federal Requirements for NHFP
  - *Establishment of the National Highway Freight Network (NHFN)*
    - The Primary Highway Freight System (PHFS)
    - Critical Rural Freight Corridors (CRFC)
    - Critical Urban Freight Corridors (CUFC)

**FAST**  
**ACT**



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# Statewide Network

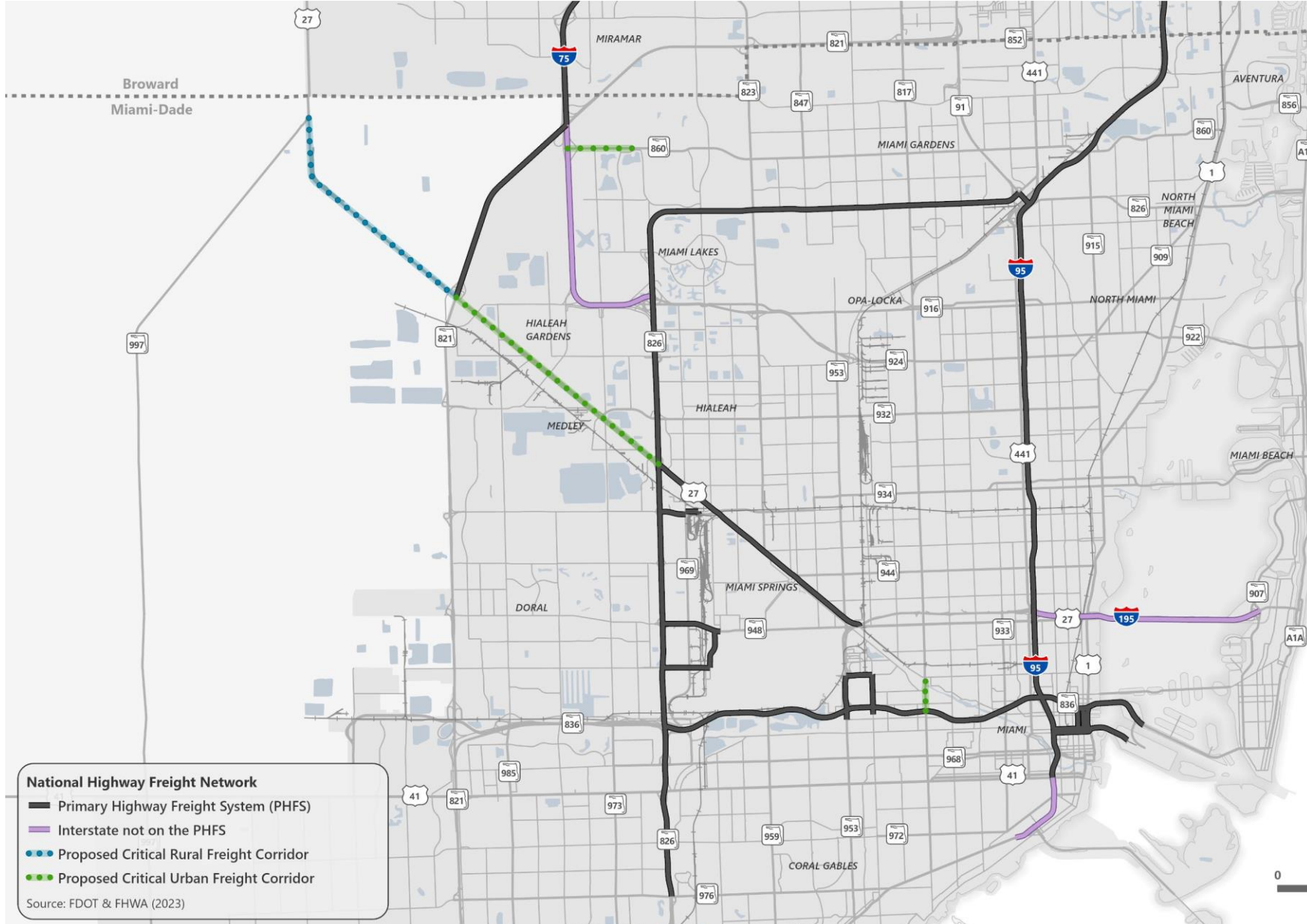


- National Highway Freight Network**
- Primary Highway Freight System (PHFS)
  - Interstate not on the PHFS
  - Critical Rural Freight Corridor (265.60 mi)
  - Critical Urban Freight Corridor (121.70 mi)
  - Proposed Critical Rural Freight Corridor (43.34 mi)
  - Proposed Critical Urban Freight Corridor (15.54 mi)

Source: FDOT & FHWA (2023)



# FDOT District 6 Network



**National Highway Freight Network**  
— Primary Highway Freight System (PHFS)  
— Interstate not on the PHFS  
••• Proposed Critical Rural Freight Corridor  
••• Proposed Critical Urban Freight Corridor  
Source: FDOT & FHWA (2023)



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# Requirements for Projects



The project must be on the NHFN

[Link to ArcGIS network map](#)



- The project must be in the state's Freight Mobility and Trade Plan (FMTP)
- The project must be an eligible project type

# Eligible Project Types

1. Development phase and preconstruction activities.
2. Construction, reconstruction, rehabilitation, and operational improvements directly relating to improving system performance.
3. Intelligent transportation systems and other technology to improve the flow of freight.
4. Efforts to reduce the environmental impacts of freight movement.
5. Environmental and community mitigation for freight movement.
6. Railway-highway grade separation.
7. Geometric improvements to interchanges and ramps.
8. Truck-only lanes.
9. Climbing and runaway truck lanes.
10. Adding or widening of shoulders.
11. Truck parking facilities eligible for funding under section 1401 (Jason's Law) of MAP-21.
12. Real-time traffic, truck parking, roadway condition, and multimodal transportation information systems.
13. Electronic screening and credentialing systems for vehicles.
14. Track signal optimization, including synchronized and adaptive signals.
15. Work zone management and information systems.
16. Highway ramp metering.
17. Electronic cargo and border security technologies that improve truck freight movement.
18. Intelligent transportation systems that would increase truck freight efficiencies inside the boundaries of intermodal facilities.
19. Additional road capacity to address highway freight bottlenecks.
20. Physical separation of passenger vehicles from commercial motor freight.
21. Enhancement of the resiliency of critical highway infrastructure to improve the flow of freight.
22. A highway or bridge project to improve the flow of freight on the NHFN.
23. Any other surface transportation project to improve the flow of freight into and out of an eligible intermodal freight facility.
24. Diesel retrofit or alternative fuel projects.
25. Conducting analyses and data collection related to the NHFP



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# NHFP Funding



# FDOT Statewide Totals by Year

- Expected Statewide funding amount per FY is \$70M
  - *Totals finalized in yearly approval process*

Fiscal Year	Estimated NHFP Funding* **	Estimated NHFP Project Cost ***	Estimated Cumulative Spending
2016	\$53,926,568	\$-	
2017	\$51,581,934	\$38,711,324	\$38,711,324
2018	\$56,271,202	\$78,429,007	\$117,140,331
2019	\$63,305,102	\$39,417,950	\$156,558,281
2020	\$70,339,002	\$62,388,661	\$218,946,942
2021	\$69,530,857	\$18,791,915	\$237,738,857
2022	\$65,707,643	\$66,540,501	\$304,279,358
2023	\$67,021,795	\$92,425,049	\$396,704,407
2024	\$68,362,231	\$67,000,335	\$463,704,742
2025	\$69,729,476	\$53,361,508	\$517,066,250
2026	\$70,000,000	\$62,611,063	\$579,677,313
2027	\$70,000,000	\$61,162,124	\$640,839,437
2028	\$70,000,000	\$60,229,759	\$701,069,196
2029	\$70,000,000	\$53,726,602	\$754,795,798
<b>Total</b>	<b>\$949,424,734</b>	<b>\$754,795,798</b>	



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# FDOT District 6 Totals By Year

- Expected Statewide funding amount per FY is \$70M
  - *Totals finalized in yearly approval process*

Fiscal Year	Estimated NHFP Funding
2019	\$ 849,478
2020	\$ 500,000
2021	\$ 1,924,665
2022	\$ 464,598
2023	\$ 23,014,024
2024	\$ 8,283,193
2025	\$ 12,590,388
2026	\$ 704,970
2027	\$ 5,720,000
2028	\$ 12,983,000
<b>Total</b>	<b>\$ 67,034,316</b>



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# District 6 Totals by type

- Total funding for projects through 2028 is \$67M
- 13 Projects funded through 2028
  - *Variety of project types*

Work Mix	Total Cost
ADD LANES & RECONSTRUCT	\$ 1,968,000.00
ADD LANES & REHABILITATE PVMNT	\$ 595,000.00
BRIDGE - PAINTING	\$ 704,970.00
BRIDGE-REPAIR/REHABILITATION	\$ 1,040,388.00
MODAL SYSTEMS PLANNING	\$ 2,645,908.00
OTHER ITS	\$ 4,930,000.00
PD&E/EMO STUDY	\$ 53,800,572.00
PTO STUDIES	\$ 1,349,478.00
Grand Total	\$ 67,034,316.00



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# Project Selection Process



# FDOT NATIONAL HIGHWAY FREIGHT PROGRAM PROCESS

START

**STEP 2:**  
Project Prioritization  
**DATE:**  
June

**STEP 4:**  
Project Programming  
**DATE:**  
September

**STEP 6:**  
Estimate Updates  
**DATE:**  
~Dec, March & June

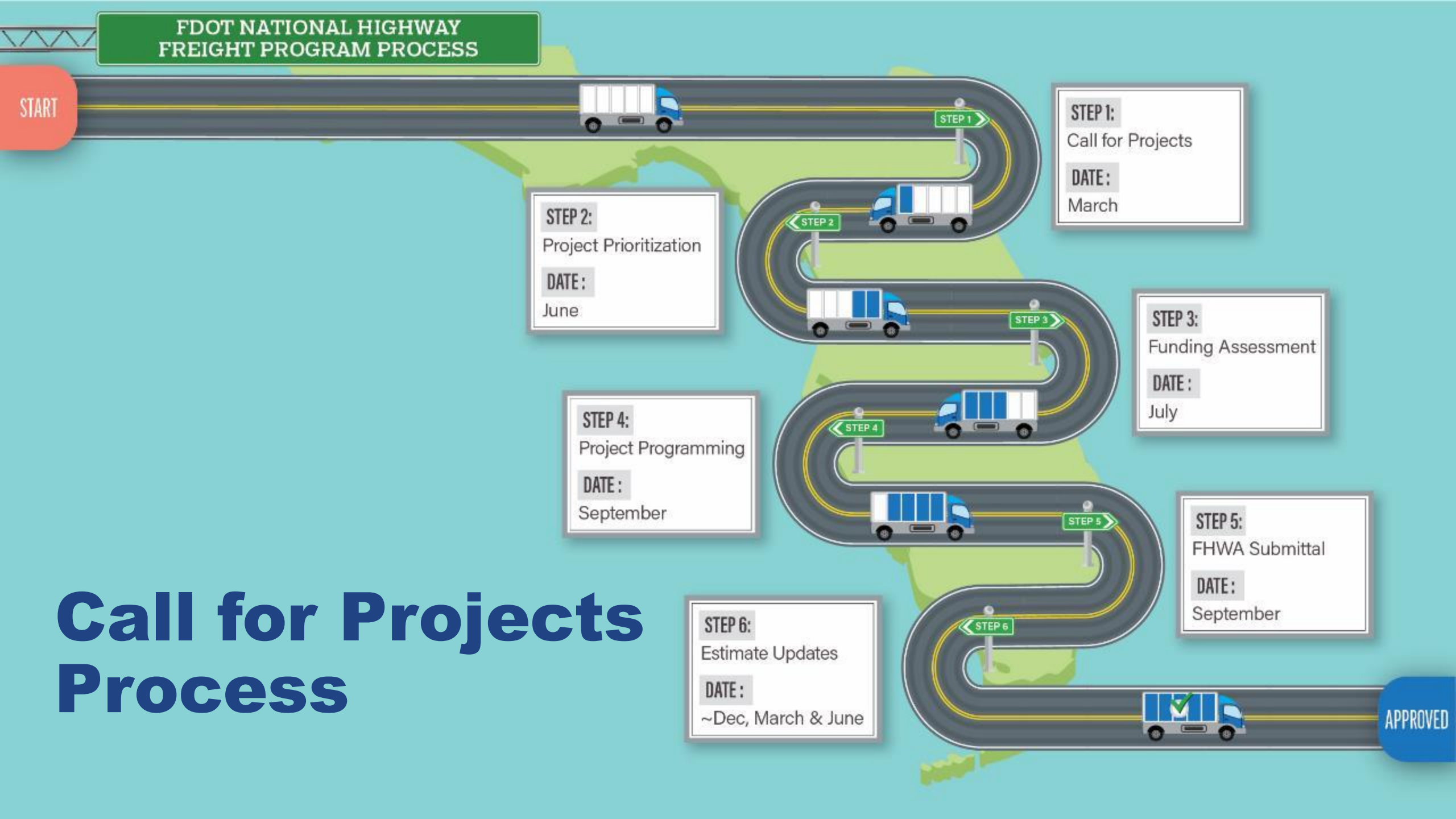
**STEP 1:**  
Call for Projects  
**DATE:**  
March

**STEP 3:**  
Funding Assessment  
**DATE:**  
July

**STEP 5:**  
FHWA Submittal  
**DATE:**  
September

# Call for Projects Process

APPROVED



# Scoring and Ranking

- The 2024 FMTP has two parts to the prioritization scoring process
  - *The quantitative portion is based on geographical/locational factors,*
  - *The qualitative portion weighs a project's ability to affect FRO priorities*
- The quantitative score is a GIS-based analysis run in Central Office
- The qualitative score is done as the Districts fill out the excel template for project information
  - *The template scores the projects' qualitative criteria in real time. Each "Yes" response requires additional details in the "Project Details" box to receive points*



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# Quantitative

The quantitative score is tied to freight performance measures:

- Truck Injuries/Truck VMT) \*1000
- (Truck Fatalities/Truck VMT) \*1000
- Crime Index
- Roadways within 100 Year Flood Zones
- Presence of Structurally Deficient Bridges
- Presence of Poor Pavement Condition Segments
- Roadways with Top Bottlenecks
- Truck AADT
- Vicinity to Hubs
- Roadways within Freight Intensive Areas
- Labor Force Size (Ratio of labor force by county population relative to average statewide ratio)
- County GRP Level (Relative to the average county GRP level in FL)
- Transportation and Warehousing Industry Share of Total Employment
- County Population Density (Relative to the average county-level population density in FL)
- On Designated Alternative Fuels Corridors
- Number of Alternative Fueling Stations within 1 Mile of Roadway



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# Qualitative

The qualitative score weighs a project's ability to effect FRO priorities:

- Does this project implement safety or security enhancements?
- Does this project improve the state's data gathering efforts?
- Does this project address the environmental or economic resiliency of the freight system?
- Does this project optimize the functionality and efficiency of the freight system?
- Does this project preserve the existing State Highway System?
- Does this project address Truck Parking?
- Does this project create a Grade separation?
- Is this a tech driven and/or TSM&O project?
- Does this project improve multimodal freight connectivity?
- Does this project use public/private partnerships?
- Does this project capitalize on emerging freight trends?
- Is this project on the MPOAC freight project list?
- Does this project promote the use of LNG/CNG/electric vehicles?



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# What to do if your project is not on the NHFN ?

- **Baseline Requirement**

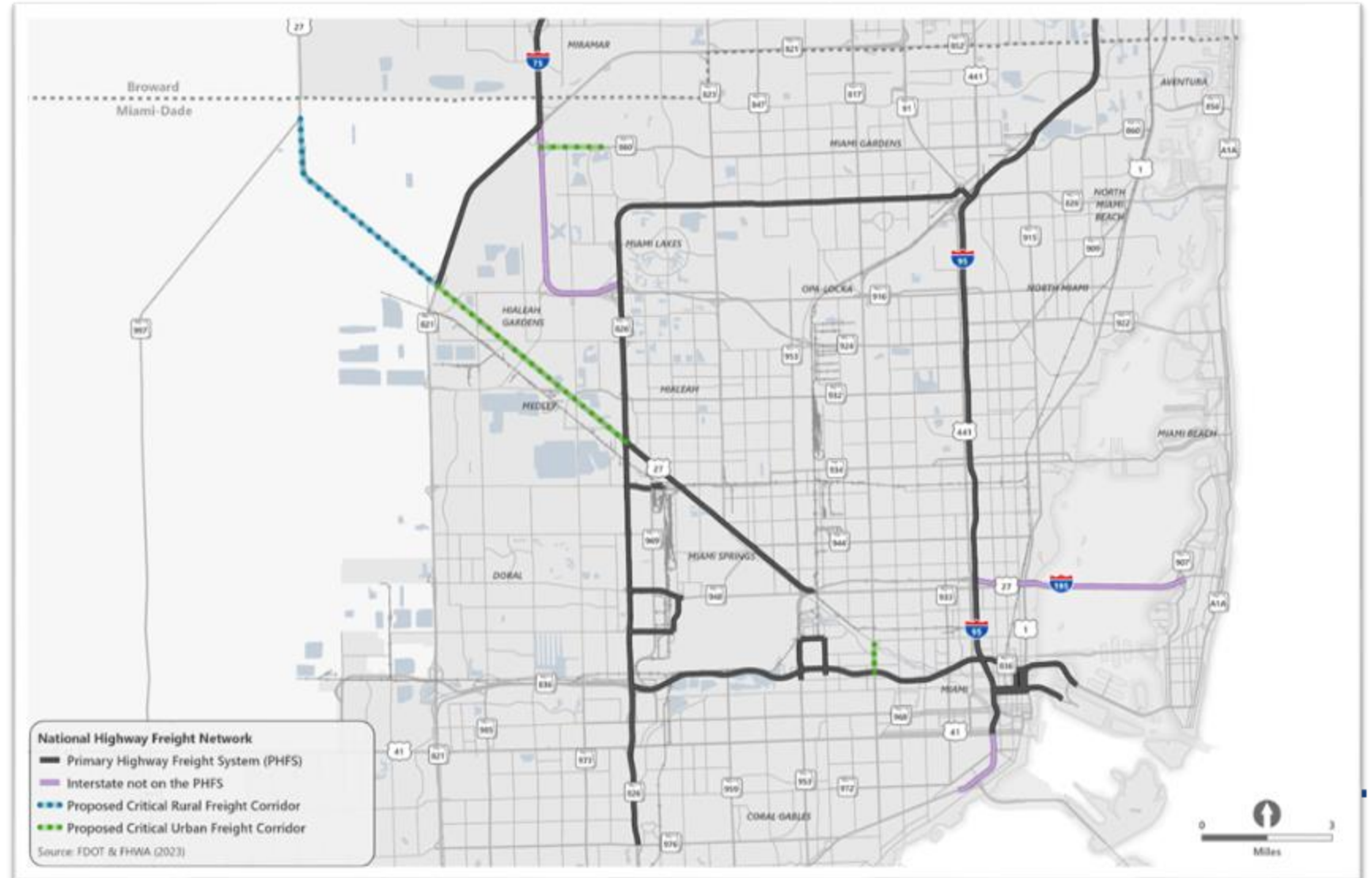
- Only real requirement is: “Is the segment determined by the State to be vital to improving the efficient movement of freight of importance to the economy of the State”
- Does your Off-Network project meet this requirement?
  - *If so, we can redesignate the CUFC-CRFC*

- **Redesignating CUFC-CRFC**

- By redesignating the CUFC-CRFC sections, off network projects can be brought into the network
- How to know if Project will score well
  - We have created a dashboard that shows scores of roadways near NHFN
    - *Potential NHFN Critical Freight Corridors (arcgis.com)*

# Network Designation

- FDOT District 6 is one of the first Districts to submit projects off the active network to have it updated.



# 2024 FLORIDA HURRICANE GUIDE

## SAFETY MESSAGE

- Hurricane season began on June 1<sup>st</sup>
- Be sure to have a plan for you and your family's safety
- Create a disaster supply kit and keep it ready
  - Ensure that all supplies are effective and not expired
- Know the safe areas within your home and understand which flood zones you are in
  - Understand the evacuation routes and plan for their use
- And of course, stay safe

Make a Plan  
Build a Disaster Supply Kit  
Know Your Home, Know Your Zone  
Stay Safe Before, During, & After a Storm

**VISIT [FLORIDADISASTER.ORG](https://www.floridadisaster.org)**





# Questions?



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