

**Origination Form**  
Proposed Revisions to a Standard Plans Index

<b>Originator:</b>	Turley, Joshua	<b>Index Number:</b>	521-428
<b>Date:</b>	4/29/2025	<b>Sheet Number(s):</b>	Sheet 4
<b>E-mail:</b>	Joshua.Turley@dot.state.fl.us	<b>Index Title:</b>	TRAFFIC RAILING - (42" SINGLE-SLOPE)

**Summary of the changes:**

Sheet 4: Added a grout plug option as an alternate detail.

**Commentary/Background:**

Contractors wanted a more constructible and economical option that would still perform the intended function of preventing water to flow out between the open joints.

Other Affected Documents/Offices	Person Contacted	Affected (Yes/No)
Other Standard Plans		No
FDOT Design Manual		No
Standard Specifications		No
Basis of Estimates Manual		No
Approved Product List		No
Construction Office		No
Maintenance Office		No

**Implementation**

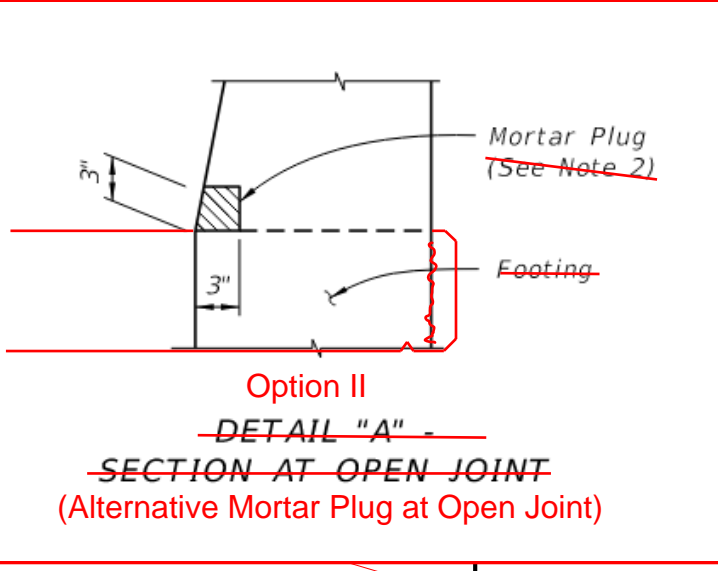
["FY-Standard Plans (Next Release)"]

CONVENTIONAL REINFORCING STEEL BENDING DIAGRAM

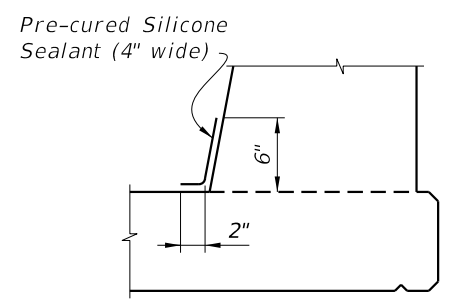
BILL OF REINFORCING STEEL		
MARK	SIZE	LENGTH
P	5	7'-0"
S1	6	As Req'd.
S2	5	As Req'd.
T1 & T2	6	10'-0"
V	5	5'-9"

ROADWAY CROSS-SLOPE	LOW GUTT
	ØB
0% to 2%	101°
2% to 6%	98°
6% to 10%	95°

ØA and ØB shall be 90° if Contractor to place Railing perpendicular to

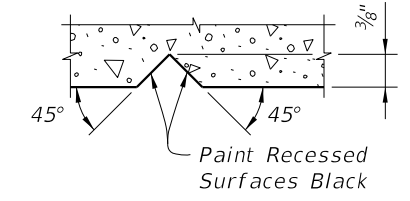


**Option II**  
~~DETAIL "A" - SECTION AT OPEN JOINT~~  
**(Alternative Mortar Plug at Open Joint)**



~~DETAIL "C" - SECTION AT INTERMEDIATE OPEN JOINT~~  
**Option I**

- INTERMEDIATE JOINT SEAL NOTES:
- At Intermediate Open Joints, seal the lower 6" portion of the open joint with Pre-cured Silicone Sealant in accordance with Specification Section 932.
  - Apply sealant prior to any Class V finish coating and remove all curing compound and loose material from the surface prior to application of bonding agent.
  - The cost of the Pre-cured Silicone Sealant shall be included in the Contract Unit Price for the Traffic Railing.
  - As an alternative option, a mortar plug may be used to seal the joint as shown in the mortar plug detail and in accordance with Specification Section 400.



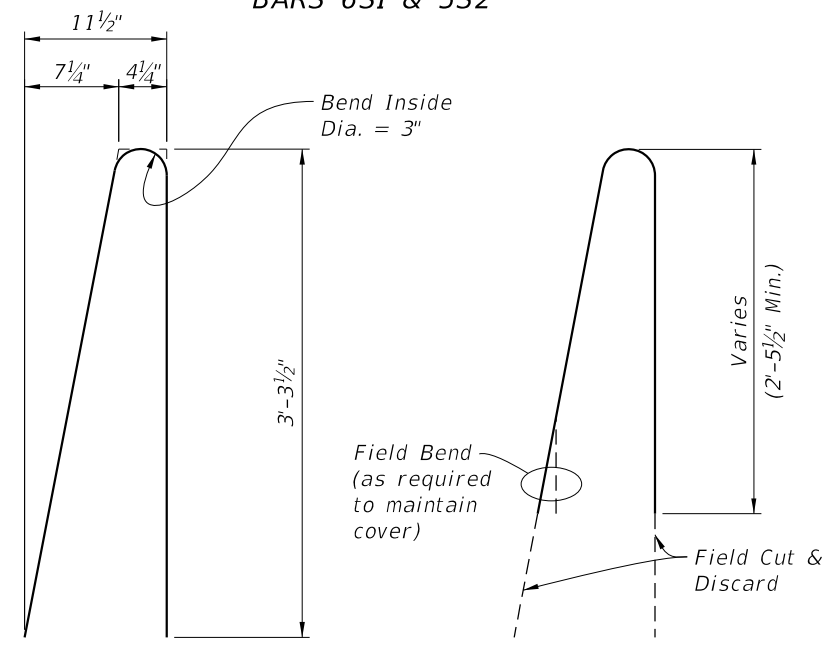
**SECTION THRU RECESSED "V" GROOVE TO FORM INSCRIBED LETTERS AND FIGURES**

ESTIMATED TRAFFIC RAILING QUANTITIES		
ITEM	UNIT	QUANTITY
Concrete	CY/LF	0.143
Reinforcing Steel	LB/LF	39.34

Note:  
The estimated railing quantities are based on a 2% deck cross slope; railing on low side of deck.

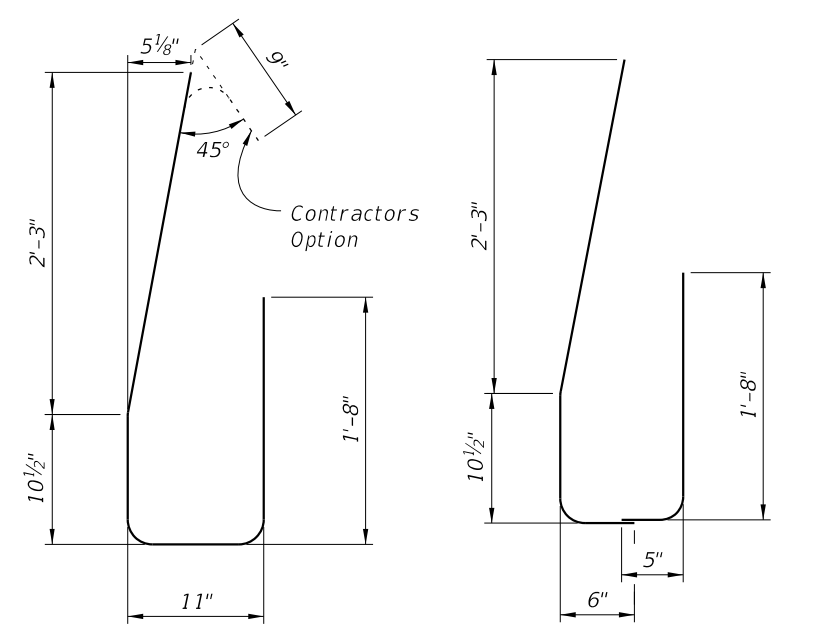
Length as Required

**BARS 6S1 & 5S2**



**STIRRUP BAR 5P**  
**TRANSITION STIRRUP BAR 5P**  
To Be Field Cut (10 of each required per Railing End Transition)

**TRANSITION BARS 6T1 & 6T2**  
(2~Bars 6T1 & 3~Bars 6T2 required per Railing End Transition)



**STIRRUP BAR 5V**  
**END STIRRUP BAR 5V**  
To Be Field Cut and Lapped

- REINFORCING STEEL NOTES:
- All bar dimensions in the bending diagrams are out to out.
  - All reinforcing steel at the open joints shall have a 2" minimum cover.
  - Bars 6S1 may be continuous or spliced at the construction joints. Lap splices for Bars 6S1 and 5S2 shall be a minimum of 3'-0" and 2'-2", respectively.
  - The Contractor may utilize deformed WWR when approved by the Engineer. WWR must meet the requirements of Specification Section 931.

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LAST REVISION 11/01/24 11/01/25	REVISION	DESCRIPTION:
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**FDOT**  
FY ~~2025-26~~  
STANDARD PLANS

TRAFFIC RAILING - (42" SINGLE-SLOPE)

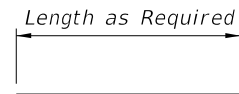
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CONVENTIONAL REINFORCING STEEL BENDING DIAGRAMS

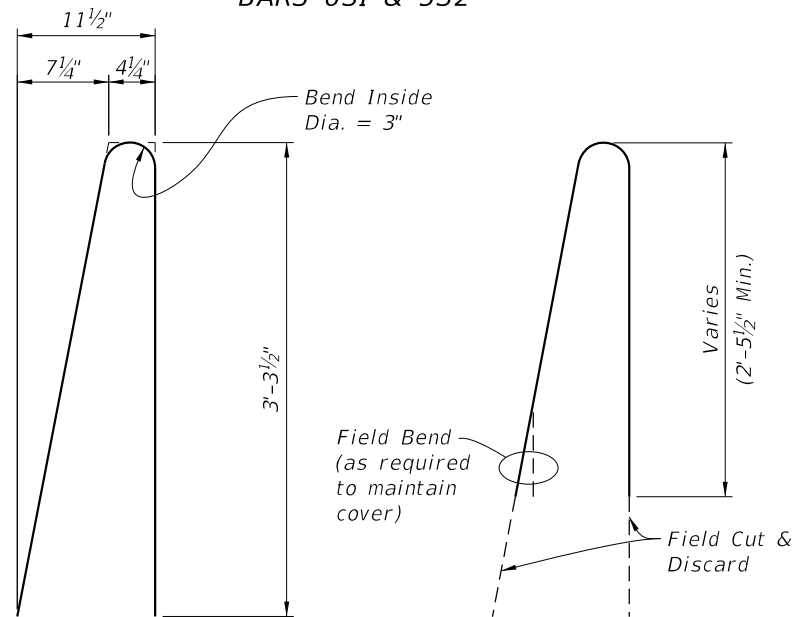
BILL OF REINFORCING STEEL		
MARK	SIZE	LENGTH
P	5	7'-0"
S1	6	As Req'd.
S2	5	As Req'd.
T1 & T2	6	10'-0"
V	5	5'-9"

ROADWAY CROSS-SLOPE	LOW GUTTER $\emptyset B$	HIGH GUTTER $\emptyset B$
0% to 2%	101°	101°
2% to 6%	98°	104°
6% to 10%	95°	107°

$\emptyset A$  and  $\emptyset B$  shall be 90° if Contractor elects to place Railing perpendicular to the Deck.

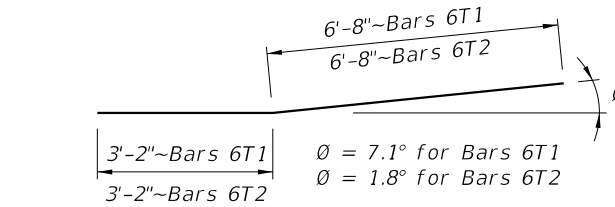


BARS 6S1 & 5S2

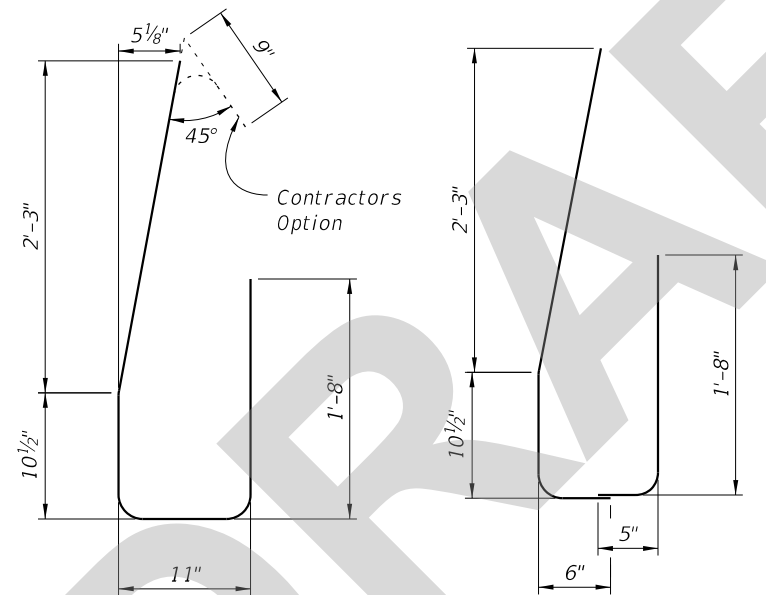


STIRRUP BAR 5P

TRANSITION STIRRUP BAR 5P  
To Be Field Cut (10 of each required per Railing End Transition)



TRANSITION BARS 6T1 & 6T2  
(2~Bars 6T1 & 3~Bars 6T2 required per Railing End Transition)



STIRRUP BAR 5V

END STIRRUP BAR 5V  
To Be Field Cut and Lapped

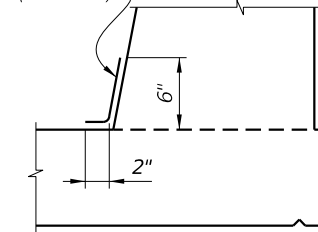
REINFORCING STEEL NOTES:

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- All reinforcing steel at the open joints shall have a 2" minimum cover.
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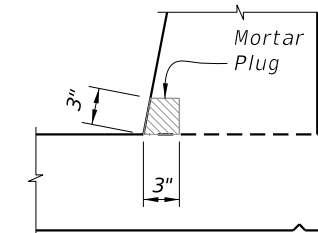
INTERMEDIATE JOINT SEAL NOTES:

- At Intermediate Open Joints, seal the lower 6" portion of the open joint with Pre-cured Silicone Sealant in accordance with Specification Section 932.
- Apply sealant prior to any Class V finish coating and remove all curing compound and loose material from the surface prior to application of bonding agent.
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- As an alternative option, a mortar plug may be used to seal the joint as shown in the mortar plug detail and in accordance with Specification Section 400.

Pre-cured Silicone Sealant (4" wide)

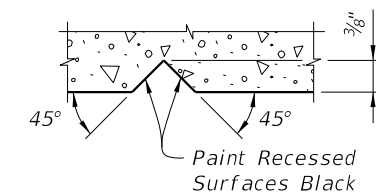


OPTION I



OPTION II  
(Alternative Mortar Plug at Open Joint)

DETAIL "C"



SECTION THRU RECESSED  
'V' GROOVE TO FORM INSCRIBED  
LETTERS AND FIGURES

ESTIMATED TRAFFIC RAILING QUANTITIES		
ITEM	UNIT	QUANTITY
Concrete	CY/LF	0.143
Reinforcing Steel	LB/LF	39.34

Note:

The estimated railing quantities are based on a 2% deck cross slope; railing on low side of deck.

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