

Index 544-001 Crash Cushion Transition Details

ORIGINATION

Date: July 31, 2019

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COMMENTARY

Sheet 1: added callout notes to rigid barrier connection detail to include traffic railing and concrete barrier standards

Sheet 3: NEW SHEET - Thrie Beam retrofit connection detail; Short guardrail extension options from crash cushion to rigid barrier.

COMMENTS AND RESPONSES

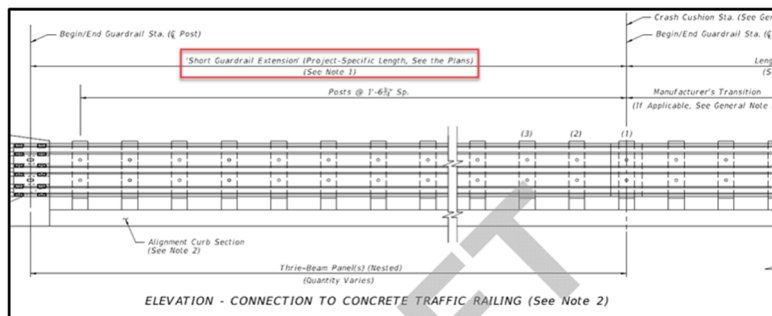
BLACK = Industry Review Comments **RED** = Standard Plans Response

Name: Turnpike

Date: August 8, 2019

COMMENT:

Regarding the new crash cushion details sheet (Sheet 3 of 3 shown below), it is unclear what minimum length of guardrail extension is considered 'short' as described in the index. Consultants and Reviewers will be confused when they would need to use the 1'-6



¾" post spacing called out. The index should call out the minimum number of panels to get the quarter post spacing.

RESPONSE:

Date: August 27, 2019

The callout highlighted references Note 1, which reads, "Short guardrail extensions are typically used where adding length to a barrier system is warranted, but a full Approach Transition Connection to Rigid Barrier will not fit." Additionally, the SPI for 544-001 will explain usage of this short guardrail extension. Basically, any other Standard option for placing guardrail between concrete traffic railing and a crash cushion requires lengthy stiffness transitions, and these transitions to w-beam may not fit projects or may not be practical. This proposed "short guardrail extension" provides a shorter, simplified option.