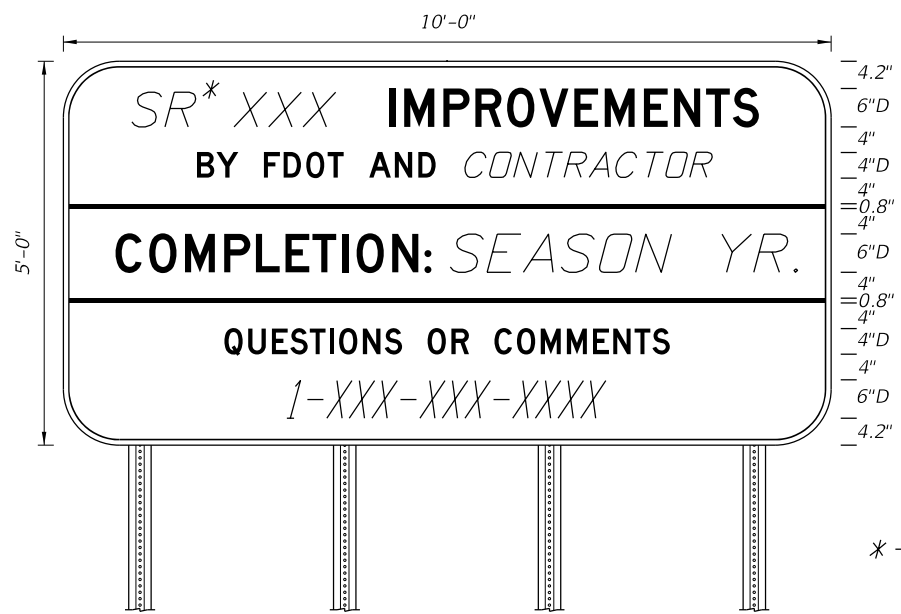
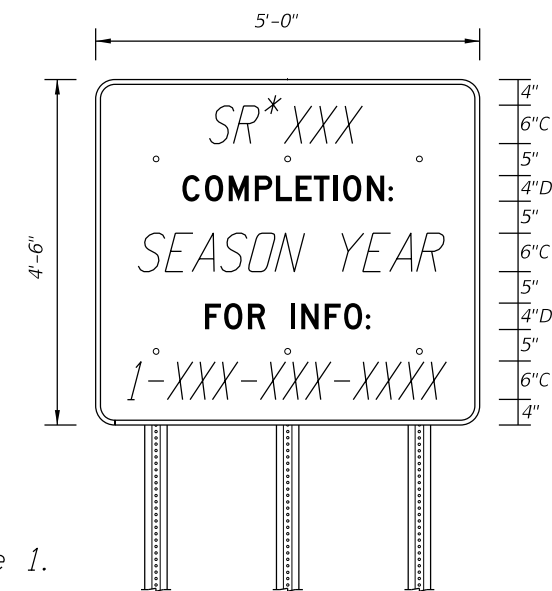


4 POST SIGN SUPPORT MOUNTING DETAIL



BORDER 10'-0" x 5'-0"  
 R=8" 8" Radii  
 TH=0.25" 4" and 6" series D Legend  
 IN=0.75" Blue Background  
 White Legend and Border

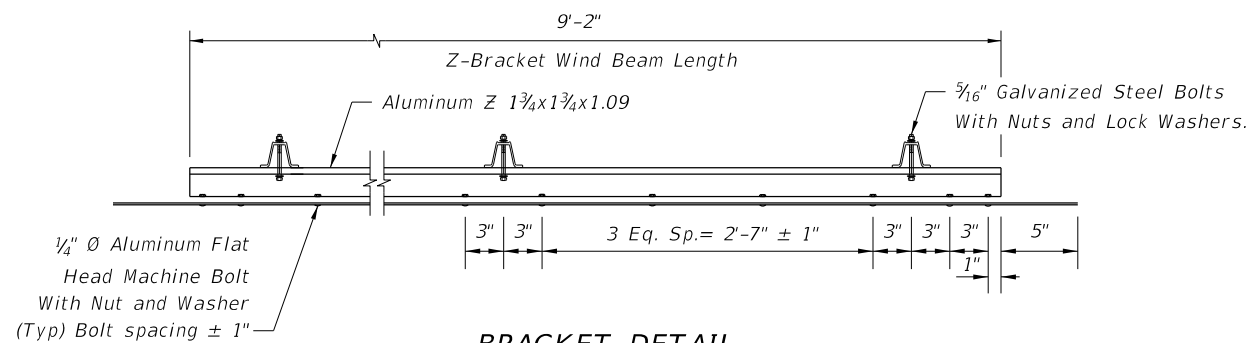
PROJECT INFORMATION SIGN DETAIL  
 50 MPH OR GREATER  
 Use SIGN ATTACHMENT DETAIL  
 (WITH Z-BRACKET).



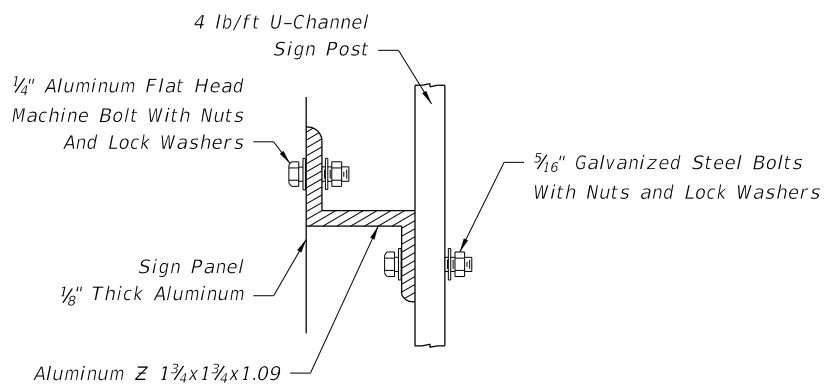
BORDER 5'-0" x 4'-6"  
 R=3" 3" Radii  
 TH=0.25" 4" series D Legend and  
 IN=0.75" 6" series C Legend  
 Blue Background  
 White Legend and Border

PROJECT INFORMATION SIGN DETAIL  
 45 MPH OR LESS  
 Use SIGN ATTACHMENT DETAIL  
 (WITHOUT Z-BRACKET)  
 On Sheet 5.

\*-See Note 1.



BRACKET DETAIL



SIGN ATTACHMENT DETAIL  
 (WITH Z-BRACKET)

PROJECT INFORMATION SIGN NOTES:

1. Road designation should be the most common designation (i.e. I-Interstate, SR-State Road or US.)
2. Italic text on signs indicate variable information specific to the project.
3. See Sheet 5 for Typical Foundation Details and Post and Foundations Table.

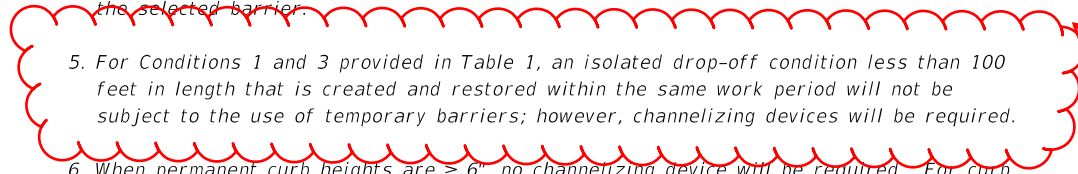
PROJECT INFORMATION SIGN

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**DROP-OFF CONDITION NOTES**

1. These conditions and treatments can be applied only in work areas that fall within a properly signed work zone.
2. When drop-offs occur within the clear zone due to construction or maintenance activities, protection devices are required (See Table 1). A drop-off is defined as a drop in elevation, parallel to the adjacent travel lanes, greater than 3" with slope (A:B) steeper than 1:4. In superelevated sections, the algebraic difference in slopes should not exceed 0.25 (See Drop-off Condition Detail).
3. Drop-offs may be mitigated by placement of slopes with optional base material per Specifications Section 285. Slopes shallower than 1:4 may be required to avoid algebraic difference in slopes greater than 0.25. Include the cost for the placement and removal of the material in Maintenance of Traffic, LSD. Use of this treatment in lieu of a temporary barrier is not eligible for CSIP consideration. Conduct daily inspections for deficiencies related to erosion, excessive slopes, rutting or other adverse conditions. Repair any deficiencies immediately.
4. For Setback Distance, refer to the Index or Approved Products List (APL) drawing of the selected barrier.
5. For Conditions 1 and 3 provided in Table 1, an isolated drop-off condition less than 100 feet in length that is created and restored within the same work period will not be subject to the use of temporary barriers; however, channelizing devices will be required.
6. When permanent curb heights are  $\geq 6"$ , no channelizing device will be required. For curb heights  $< 6"$ , see Table 1.

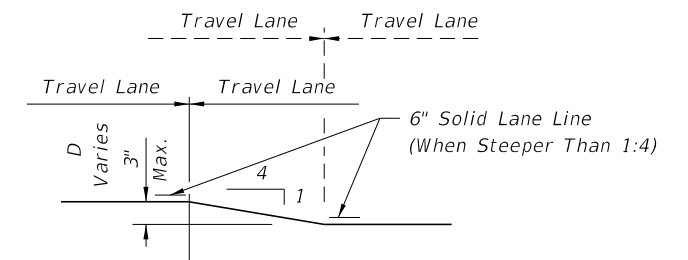


**Reverted to Previous Version:** For Conditions 1 and 3 provided in Table 1, any drop-off condition that is created and restored within the same work period will not be subject to the use of temporary barriers; however, channelizing devices will be required.

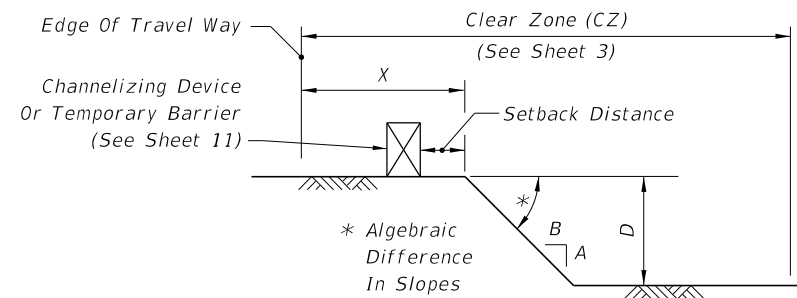
**TRAVEL LANE TREATMENT FOR MILLING OR RESURFACING NOTES**

1. This treatment applies to resurfacing or milling operations between adjacent travel lanes.
2. Whenever there is a difference in elevation between adjacent travel lanes, the W8-11 sign with "UNEVEN LANES" is required at intervals of 1/2 mile maximum.
3. If D is 1 1/2" or less, no treatment is required.
4. Treatment allowed only when D is 3" or less.

If slope is steeper than 1:4 (not to be steeper than 1:1), the R4-1 and W8-11 signs shall be used as a supplement to the W8-11; this condition shall never exceed 3 miles in length.



**TRAVEL LANE TREATMENT FOR MILLING OR RESURFACING DETAIL**



**DROP-OFF CONDITION DETAIL**

**Table 1  
Drop-off Protection Requirements**

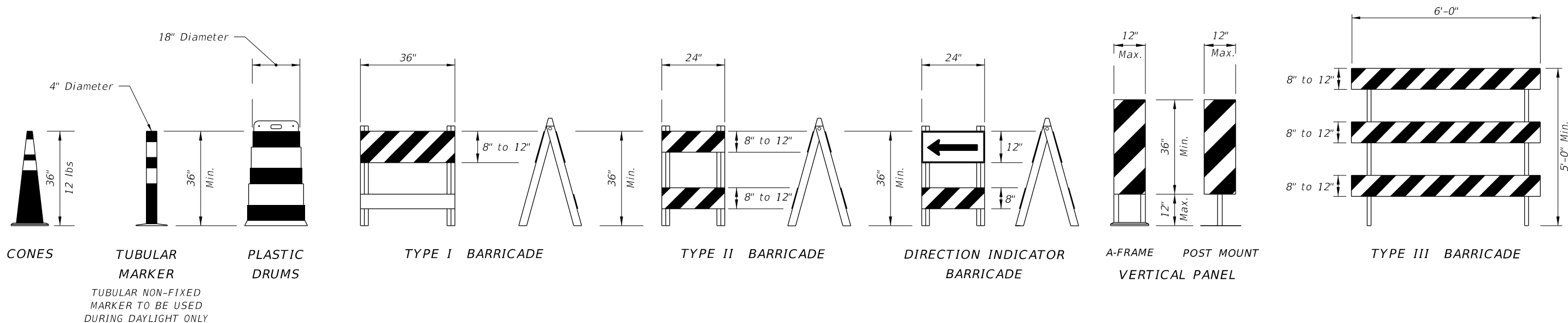
Condition	X (ft)	D (in.)	Device Required
1	0-12	> 3	Temporary Barrier
2	> 12-CZ	> 3 to $\leq 5$	Channelizing Device
3	0-CZ	> 5	Temporary Barrier
4	Removal of Bridge or Retaining Wall Barrier		Temporary Barrier
5	Removal of portions of Bridge Deck		Temporary Barrier

**PEDESTRIAN WAY DROP-OFF CONDITION NOTES**

1. A pedestrian way drop-off is defined as:
  - a. a drop in elevation greater than 10" that is closer than 2' from the edge of the pedestrian way
  - b. a slope steeper than 1:2 that begins closer than 2' from the edge of the pedestrian way when the total drop-off is greater than 60"
2. Protect any drop-off adjacent to a pedestrian way with pedestrian longitudinal channelizing devices, temporary barrier wall, or approved handrail.

**DROP-OFFS IN WORK ZONES**

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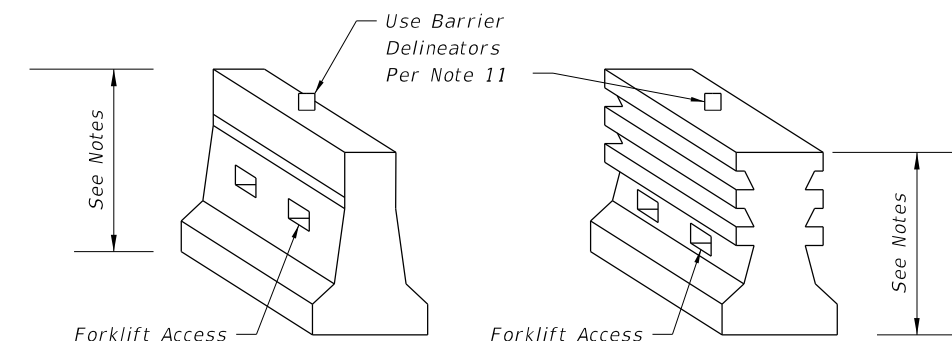
**CHANNELIZING DEVICES**

**CHANNELIZING DEVICE NOTES:**

- The details shown on this sheet are for the following purposes:
  - For ease of identification and
  - To provide information that supplements or supersedes that provided by the MUTCD.
- The Type III Barricade shall have a unit length of 6'-0" only. When barricades of greater lengths are required those lengths shall be in multiples of the 6'-0" unit.
- No sign panel should be mounted on any channelizing device unless the channelizing device/sign combination was found to be crashworthy and the sign panel is mounted in accordance with the vendor drawing for the channelizing device shown on the Approved Products List (APL).
- Ballast shall not be placed on top rails or any striped rails or higher than 13" above the driving surface.
- The direction indicator barricade may be used in tapers and transitions where specific directional guidance to drivers is necessary. If used, direction indicator barricades shall be used in series to direct the driver through the transition and into the intended travel lane.
- The splicing of sheeting is not permitted on either channelizing devices or MOT signs.
- For rails less than 3'-0" long, 4" stripes shall be used.
- Cones shall:
  - Be used only in active work zones where workers are present.
  - ~~Not exceed 2 miles in length of use at any one time.~~
  - Be reflectorized as per the MUTCD with Department-approved reflective collars when used at night.
- Vehicular longitudinal channelizing devices shall not exceed 36" in height. For vehicular longitudinal channelizing devices (LCDs) less than 32" in height, the LCD shall be supplemented with approved fixed (surface mounted) channelizing devices (tubular markers, vertical panels, etc.) along the run of the LCD, at the ends, at 50' centers on tangents, and 25' centers on radii. The cost of the fixed supplemented channelizing devices shall be included in the cost of the LCD. LCDs less than 32" in height shall not be used for speeds greater than 45 mph.

10. For pedestrian longitudinal channelizing devices, the device shall have a minimum of 8" continuous detectable edging above the walkway. A gap not exceeding a height of 2" is allowed to facilitate drainage. The top surface of the device shall be a minimum height of 32" and have a 1/8" or less difference in any plane at all connection points between the devices to facilitate hand trailing. The bottom and the top surface of the device shall be in the same vertical plane. If pedestrian drop-off protection is required, the device shall have a footprint or offset of at least 2', otherwise the device must be at least 42" in height above the walkway and be anchored or ballasted to withstand a 200 lb lateral point load at the top of the device.

11. For Barrier Delineators, see Specification 102. Place on top of unit so that retroreflective sheeting faces vehicular traffic. Color must match adjacent longitudinal pavement marking.

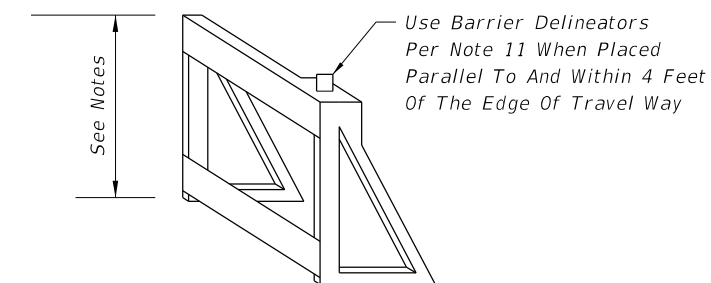


**TEMPORARY BARRIER NOTES:**

1. Where a barrier is specified, any of the types below may be used in accordance with the applicable Index:

Index	Description
102-100	Temporary Barrier
102-120	Low Profile Barrier
536-001	Guardrail

2. Trailer Mounted Barriers may be used to provide positive protection for workers within the work areas. APL drawings may be used as a guide to develop project specific Temporary Traffic Control Plans that are signed and sealed by the Contractor's Engineer.



**LONGITUDINAL CHANNELIZING DEVICE**

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