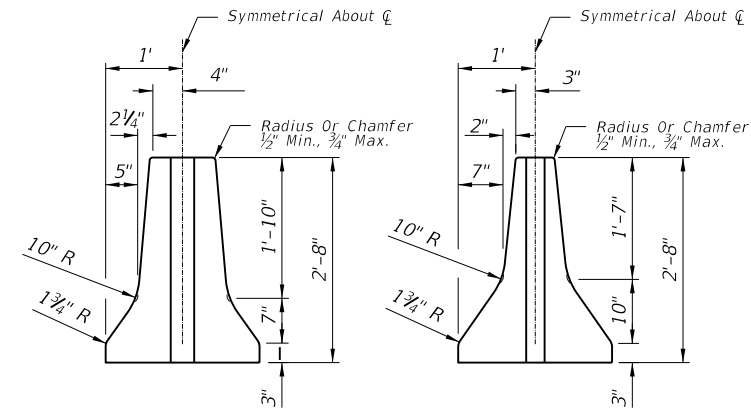


GENERAL NOTES

1. Temporary concrete barrier systems on roadways may be any of the following:
 - a. The FDOT Type K Temporary Concrete Barrier system (Design Standard Index 414). F-Shape Units. For temporary concrete barrier systems on bridges see Design Standard Index No. 414.
 - b. Proprietary temporary concrete barrier systems meeting NCHRP Report 350 Test Level 3 criteria which are included on the Qualified Products List.
2. Barrier units of dissimilar types may be interconnected within a single line barriers using transition units.
3. Alignment, length of need, anchorage and end treatment shall be in accordance with this Index.
4. Temporary concrete barrier units shown herein shall not be used for permanent barrier wall construction regardless of unit length.
5. If the plans specify Barrier (Temporary) (Type K), substitution with other barrier types is not permitted.
6. If the plans specify temporary concrete barrier system, substitution with water filled barriers is not permitted.
7. Where existing pavement is not present, construct an Asphalt Pad using Miscellaneous Asphalt Pavement. Cost of the Asphalt Pad to be included in the cost of the Barrier system.
8. Barrier Delineators are to be mounted on top of temporary concrete barriers that are used as barriers along traveled ways in work zones. Space the Barrier Delineators at 50' centers in alignment transitions, 100' centers on horizontal curves and 200' centers on tangent alignments.
9. Barrier Units used for work zone traffic control and other temporary applications shall be paid for under the contract unit price for Barrier (Temporary), LF.
10. Deflection space shall be clear of any grass, construction debris, stockpiled materials, equipment, and objects.

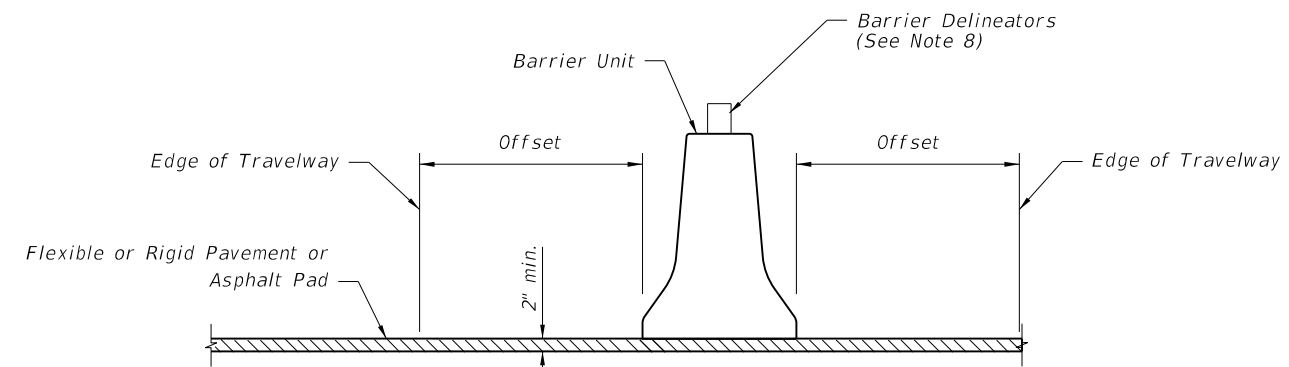


F-SHAPE

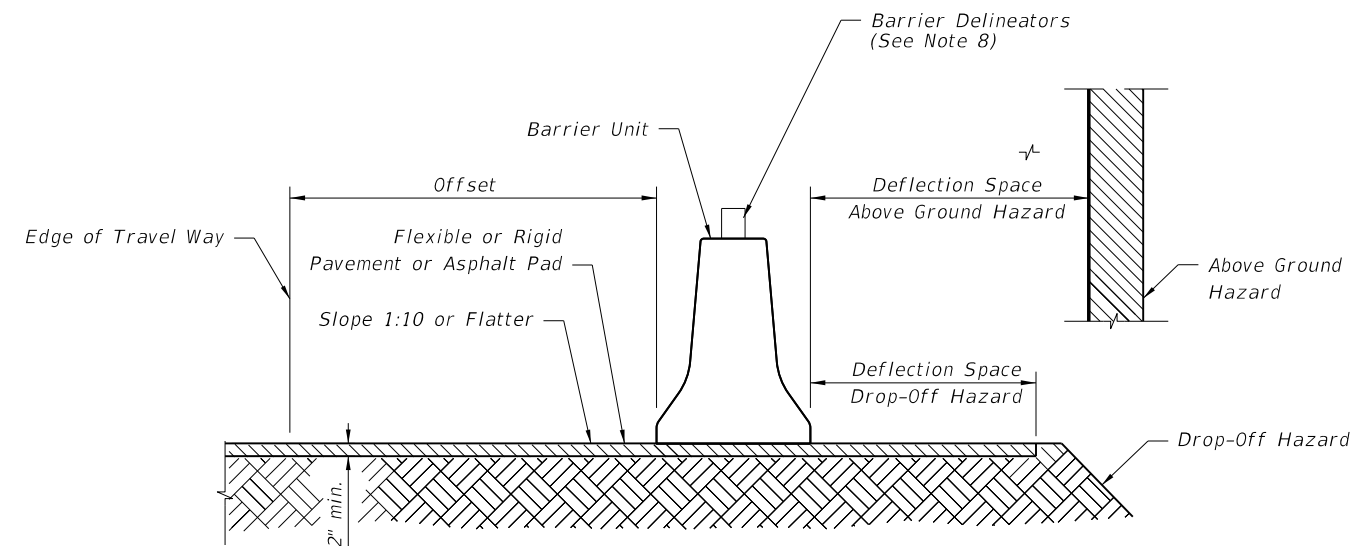
N.J. SHAPE

END VIEWS

REINFORCEMENT AND OTHER UNIT FABRICATION DETAILS NOT SHOWN.
PERMITTED BARRIER UNIT END VIEWS



MEDIAN INSTALLATION



ROADWAY INSTALLATION

OFFSET AND DEFLECTION SPACE REQUIREMENTS

Installation	Shielding	Work Zone Speed	Offset to Travelway	Deflection Space
Left or Right Shoulder	Above Ground Hazards	45 mph or Less	1' min, 2' preferred	2' min.
		50 mph and Greater	2' min, 4' preferred	4' min.
	Drop-Off Hazards	45 mph or Less	1' min, 2' preferred	2' min.
		50 mph and Greater		
		a. Drop-offs 4' or Less and NO traffic below	2' min, 4' preferred	2' min.
b. All drop-off conditions other than 'a'	2' min, 4' preferred	4' min.		
Separating Traffic	Adjacent Opposing Traffic	45 mph or Less	1' min, 2' preferred	1' min., 2' preferred
		50 mph and Greater	2' min, 4' preferred	2' min., 4' preferred

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