

PARTIAL ELEVATION OF INSIDE FACE OF RAILING (Existing Traffic Railing not shown for clarity)

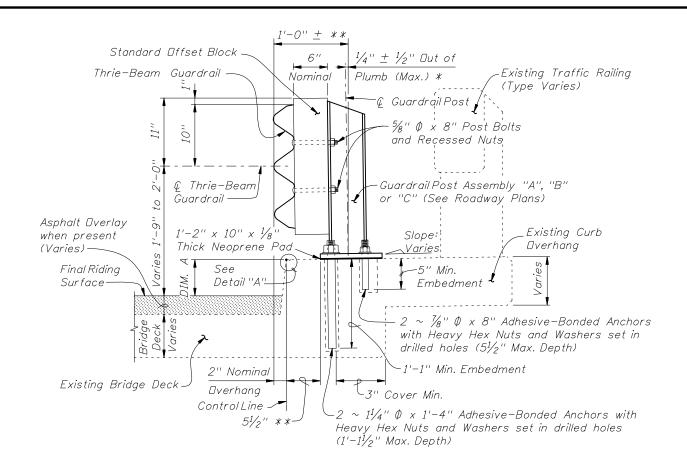
==== TYPICAL TREATMENT OF RAILING ALONG BRIDGE ======

NOTES:

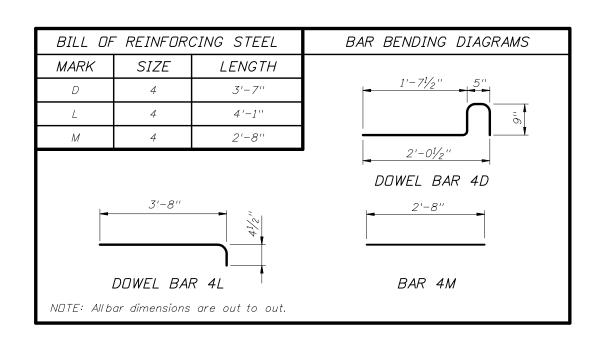
- 1. On approach end provide Index No. 402 (as shown) or other site specific treatment, see Roadway Plans. For treatment of trailing end see Roadway Plans.
- 2. Actual joint dimension and orientation vary. For Intermediate Deck Joints use the Modified Post Spacing at Intermediate Deck Joints Detail, Index No. 470, Sheet 2, as required.
- 3. Areas where existing structure has been removed shall match adjoining areas and shall be finished flat by grouting or grinding as required. Exposed existing reinforcing steel shall be burned off 1" below existing concrete and grouted over.

CROSS REFERENCES:
For Section A-A see Sheet 2.
For Traffic Railing Notes and Details
see Index No. 470.





SECTION A-A TYPICAL SECTION THRU RAILING ON BRIDGE DECK





Match Front Face of

Asphalt Overlay

when present

Final Riding

Approach

Slab Varies

Schemes 3 & 4 '- Overhang Varies

Schemes 5 & 6 - 2" Nominal Overhang

ControlLine (Schemes 5 & 6) ~

ControlLine (Projected from

Bridge) (Schemes 3 & 4) —

Surface

(Varies)

Thrie-Beam Guardrail along Bridge

Offset Block(s) as required -

Thrie-Beam

Guardrail ·

¢ Thrie−Beam

 $1'-2'' \times 10'' \times \frac{1}{8}$

See

Thick Neoprene Pad

Detail ''A'

Existing

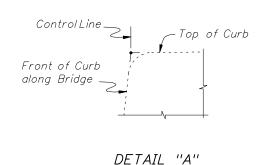
Approach

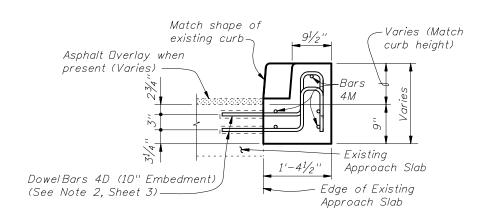
Slab

Varies 51/2" **

Guardrail ·

^{**} Offset may vary ± 1" for Adhesive-Bonded Anchors to clear existing curb reinforcing and provide minimum edge clearance. Offset shall be consistent along length of bridge.





VIEW C-C

CROSS REFERENCES:

Varies **

 $(1'-0'' \pm Min.)$

 $\frac{1}{4}$ " $\pm \frac{1}{2}$ " Dut of

-Ç Guardrail Post¦

5%'' ∅ Post Bolts (length varies)

-Guardrail Post ˈAssembly ''A'', ''B''

Existing Curb Overhang

or "C" (See Roadway Plans)

and Recessed Nuts

Varies

Embedment

-5" Min

∽3" Cover Min.

SECTION B-B

TYPICAL SECTION THRU RAILING ALONG APPROACH SLAB (SCHEMES 5 AND 6 SHOWN, SCHEMES 3 AND 4 SIMILAR)

Depth respectively)

Plumb (Max.) *

-Existing Wing Post

Existing Wing

~ 7/8" \$\Phi x 8" Adhesive-Bonded Anchors

 $\sim 1^{1}/_{4}$ " $\emptyset \times 1'-4$ " (1'-1" Min. Embed. Schemes 3 & 5)

or $2 \sim 1^{1}/_{4}$ " $\emptyset \times 8$ " (5" Min. Embed. Schemes 4 & 6)

Adhesive-Bonded Anchors with Heavy Hex Nuts and

Washers set in drilled holes $(1'-1^{1/2})''$ or $5^{1/2}$ '' Max.

with Heavy Hex Nuts and Washers set in

drilled holes $(5\frac{1}{2}"$ Max. Depth)

(Type Varies)

For location of Section A-A see Sheet 1, 3 & 4.

For location of Section B-B see Sheet 4.

For location of View C-C see Sheet 3.

For application of Dim. A see Post Dimension Table on Index 470, Sheet 3.

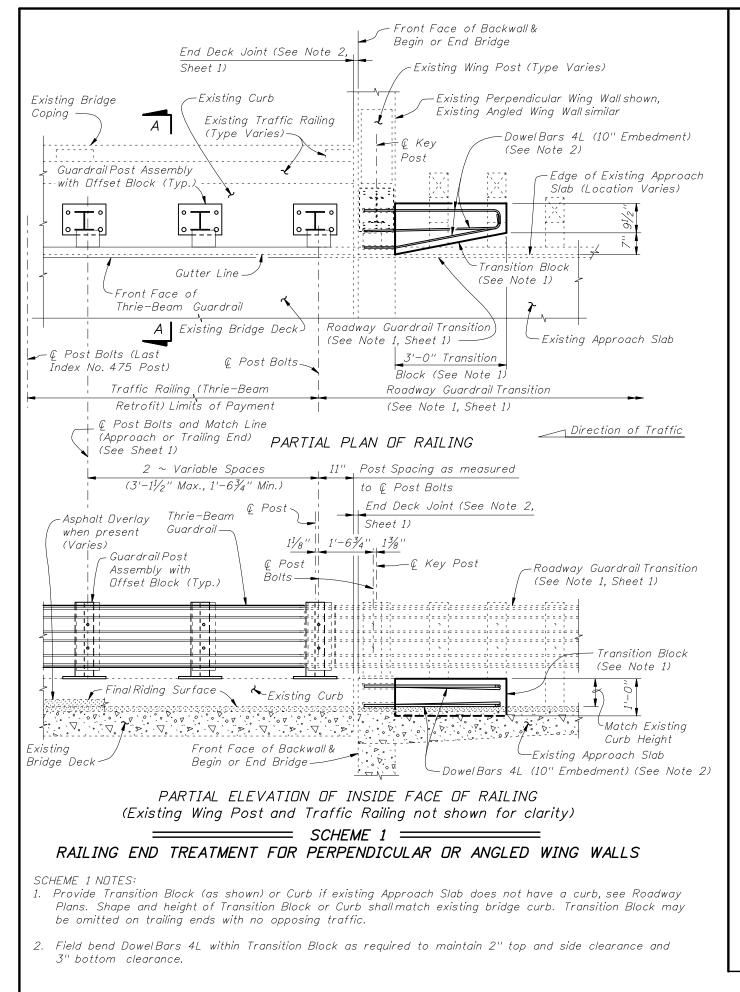


2010 FDOT Design Standards

TRAFFIC RAILING - (THRIE-BEAM RETROFIT)
WIDE CURB TYPE 1

Last Sheet No. 07/01/08 2 of 4

Index No. 475



Begin or End Bridge. Plans for Details of Sidewalk replacement. Existing Wing Post (Type Varies) -End Deck Joint (See Existing Flared Wing Wall Note 2, Sheet 1) shown, Existing Parallel Existing Bridge Wing Wall similar Existing Traffic Railing Coping Approach Slab Curb to (Type Varies)remain (Parallel Curb shown, Flared Curb similar) Guardrail Post Assembly with Offset Block (Typ.) 5 Bars 4M Double Dowel Bars 4D (10' Edae of Front Face of Thrie Embedment) (See Note 2) Existing -Beam Guardrail Gutter Line Approach Slab Existing Approach (Location -Existing Slab Varies) 2 sp. @ 1'-3' Bridge Deck Q Key @ Post Bolts _ Post © Post Bolts (Last -Roadway Guardrail Transition 3'-0" Īndex No. 475 Post) (See Note 1, Sheet 1) Transition Block Traffic Railing (Thrie-Beam Roadway Guardrail Transition (See Note 1) Retrofit) Limits of Payment (See Note 1, Sheet 1) → Direction of Traffic Post Bolts and Match Line (Approach or Trailing End) PARTIAL PLAN OF RAILING (See Sheet 1) 11" Post Spacing as measured 2 ~ Variable Spaces $(3'-1\frac{1}{2}'')$ Max., $1'-6\frac{3}{4}''$ Min.) to & Post Bolts € Post-End Deck Joint (See Note 2, Thrie-Beam Sheet 1) Guardrail Post Guardrail-1'-63/4'' Assembly with Offset Block (Typ.) -@ Key Post Post -Asphalt Overlay when Roadway Guardrail Transition present (Varies) (See Note 1, Sheet 1) Transition Block (See Note 1) Varies (Match Curb Height)~ -Bars 4M Existing Curb
Double Dowel Bars 4D (10" Embedment) (See Note 2) Existing Approach Slab Existing Front Face of Backwall& Bridge Deck Begin or End Bridge-PARTIAL ELEVATION OF INSIDE FACE OF RAILING (Existing Wing Post and Traffic Railing not shown for clarity) _______ SCHEME 2 == RAILING END TREATMENT FOR PARALLEL OR FLARED CURBS WITH DETACHED SIDEWALKS OR INTEGRAL SIDEWALKS LESS THAN 6" THICK SCHEME 2 NOTES: 1. Provide Transition Block (as shown) or Curb if existing Approach Slab Curb does not extend to end of Approach Slab. Shape and height of Transition Block or Curb shall match existing bridge curb. Transition Block may be omitted on trailing ends with no opposing traffic and on bridges with flared Approach Slab Curbs. 2. Field bend or tilt DowelBars 4D and Bars 4M within Transition Block as required to maintain 2" top and side clearance and 3" bottom clearance 2010 FDOT Design Standards Sheet No. 01/01/08 3 of 4

Front Face of Backwall &

Existing Detached Sidewalk or Integral Sidewalk

less than 6" thick to be removed. See Roadway

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1ndex No. 475

