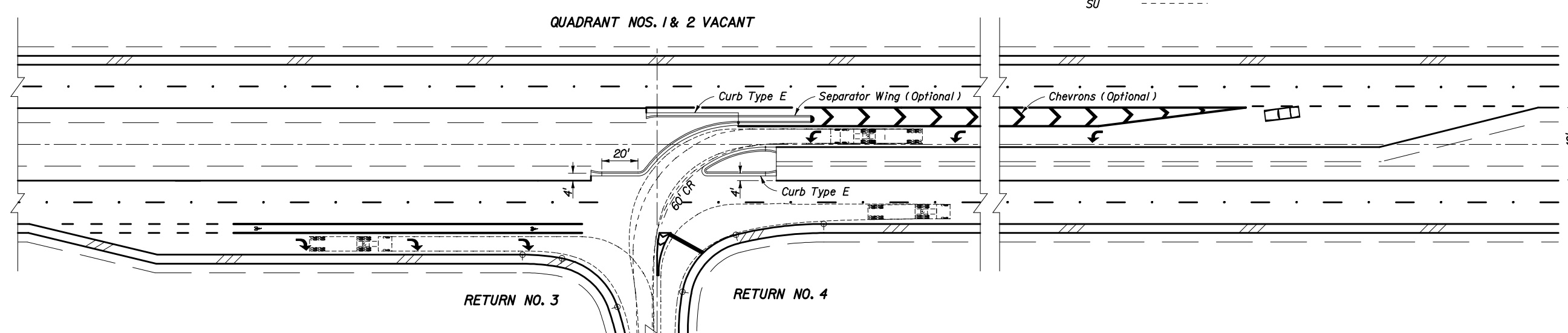


NOTE: Return configurations for each quadrant must be analyzed independently to assure adequate return pavement for semi-trailer inside tracking and for 4' minimum clearance between trucks making opposing movement. The depicted design only applies where roads and streets intersect at 90° to the mainline and have centerlines common with the opposing road or street. Swept paths are by AutoTURN 4.0 for the AASHTO 2001 SU and WB-40 tractor-semi-trailer.

RETURNS:
 Returns Depicted:
 Three Centered Compound Curves For All Returns Depicted:
 120'-40'-200' Radii; 2' And 8' Offsets
 Simple Curve With Tapers Not Shown:
 40' Radius; 1:15 And 1:8 Tapers With
 2' And 8' Offsets Tested (Practical Fit)

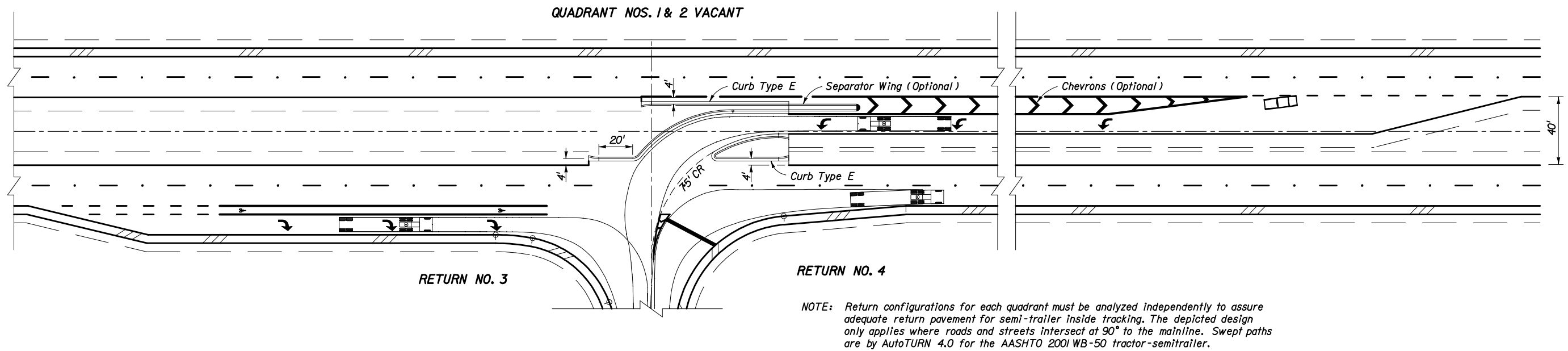
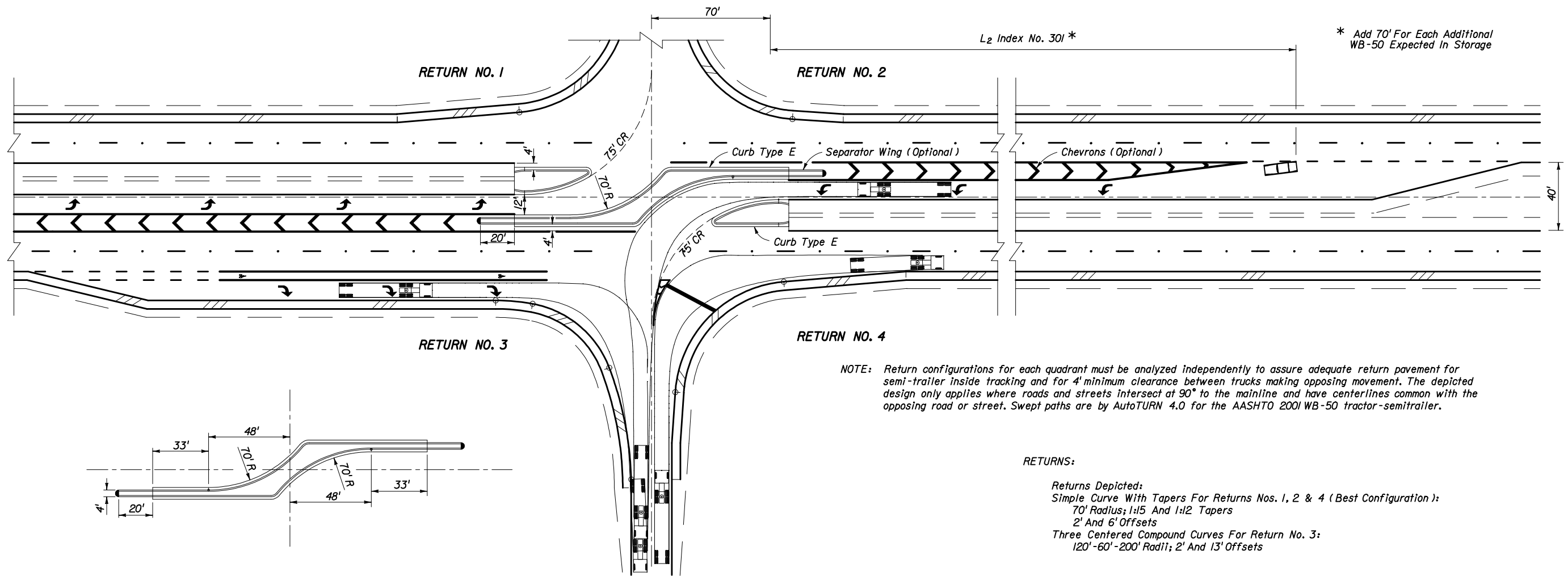
SWEPT PATH LEGEND:
 WB 40 -----
 SU - - - - -



NOTE: Return configurations for each quadrant must be analyzed independently to assure adequate return pavement for semi-trailer inside tracking. The depicted design only applies where roads and streets intersect at 90° to the mainline. Swept paths are by AutoTURN 4.0 for the AASHTO 2001 SU and WB-40 tractor-semi-trailer.

40' MEDIAN • 4-LANE DIVIDED • PARALLEL TURN BAY • 2001 AASHTO SU & WB-40 (WB-12)





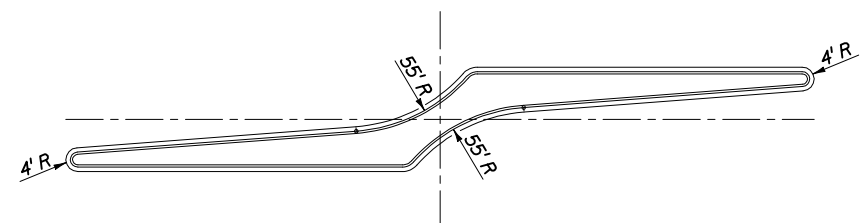
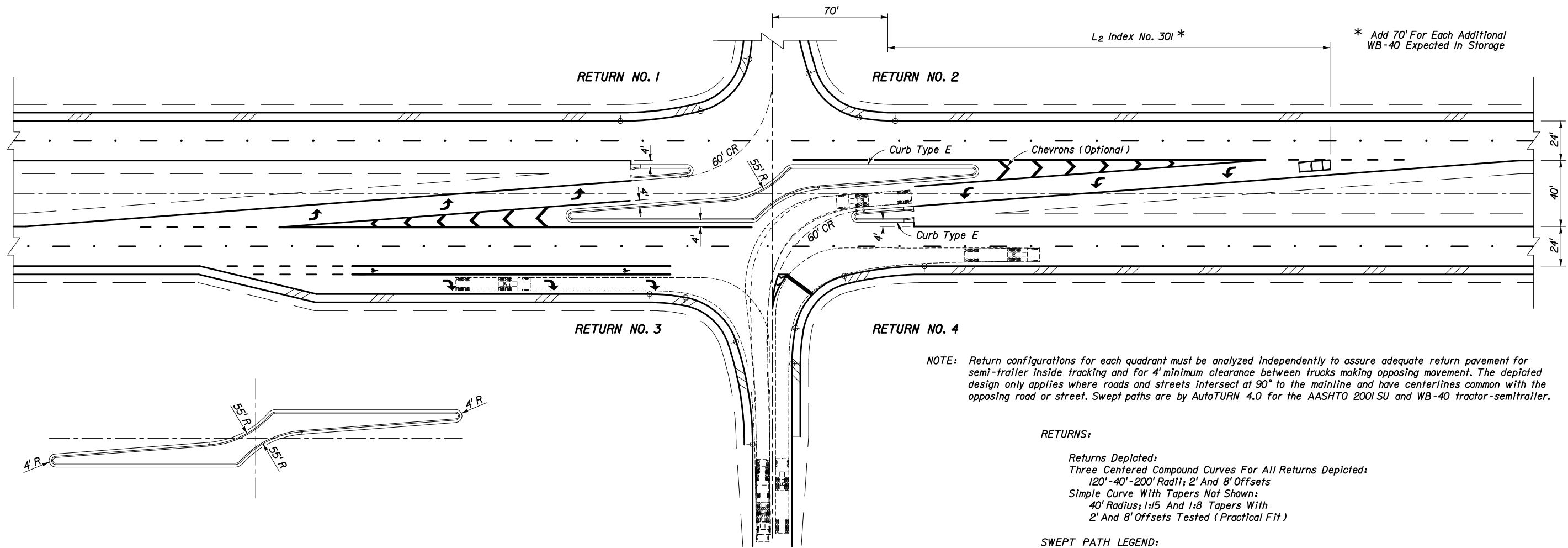
40' MEDIAN • 4-LANE DIVIDED • PARALLEL TURN BAY • 2001 AASHTO WB-50 (WB-15)



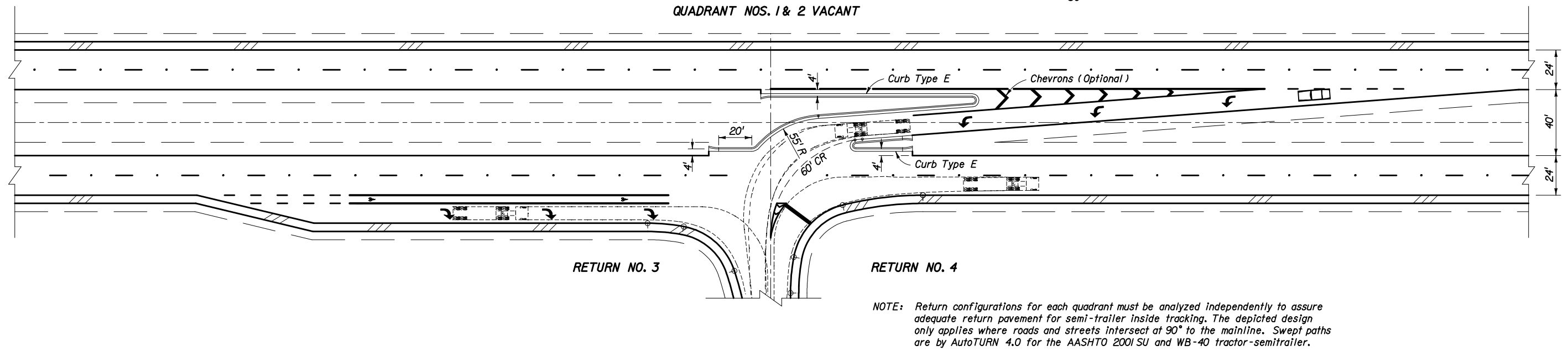
2006 FDOT Design Standards

DIRECTIONAL MEDIAN OPENINGS

Last Revision 04	Sheet No. 2 of 3
Index No. 527	



QUADRANT NOS. 1 & 2 VACANT



40' MEDIAN • 4-LANE DIVIDED • TAPERED TURN BAY • 2001 AASHTO SU & WB-40 (WB-12)



2006 FDOT Design Standards

DIRECTIONAL MEDIAN OPENINGS

Last Revision
04

Sheet No.
3 of 3

Index No.
527