

White-Red reflective Pavement Markers Shall Begin at the Transition.

Maintain Full Ramp Width (15' Typical)

6" White Edge Line

Refer To Detail A

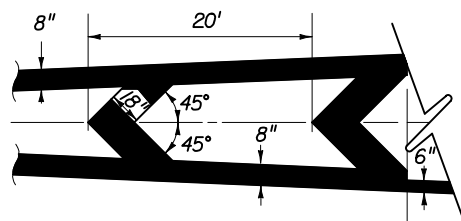
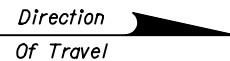
White-Red Reflective Markers Every 40'.

Yellow-Red Reflective Markers Every 40'.

Shoulder Line 6" Yellow Edge Line

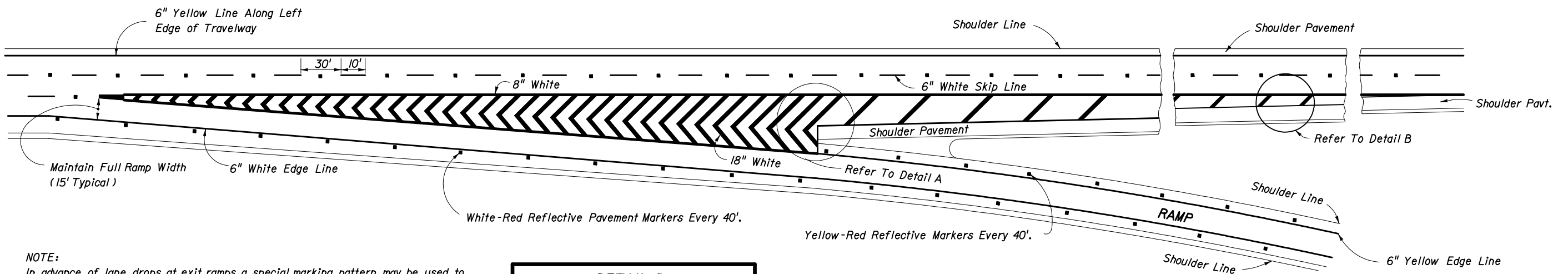
DETAIL A

For RPM Location Refer To Index 17352.



NOTE:
Reflective pavement markers are installed adjacent to the edge line.

**NORMAL TAPERED EXIT
(TWO THRU LANES)**



6" Yellow Line Along Left Edge of Travelway

Shoulder Line

Shoulder Pavement

Shoulder Pavt.

Refer To Detail B

Maintain Full Ramp Width (15' Typical)

6" White Edge Line

White-Red Reflective Pavement Markers Every 40'.

Yellow-Red Reflective Markers Every 40'.

Refer To Detail A

Shoulder Line

RAMP

Shoulder Line

6" Yellow Edge Line

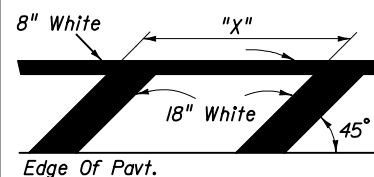
NOTE:

In advance of lane drops at exit ramps a special marking pattern may be used to distinguish the lane drop situation from a normal exiting ramp or auxiliary lane. A typical special marking for lane drops consist of 8" wide by 3' long white stripes separated by 12' gaps. If used, this special marking should begin 1/2 mile in advance of the theoretical gore point. Where last minute lane changes may cause conflicts, an 8" wide solid white channelizing line may be extended 300' upstream from the theoretical gore. (M.U.T.C.D. Section 3B-11).

DETAIL B

"S" MPH	30	35	40	45	50	55
"X" Ft.	20	20	40	40	60	60

Passenger Car, Daytime, Posted Speeds Or 85th Percentile (Use Higher Value)

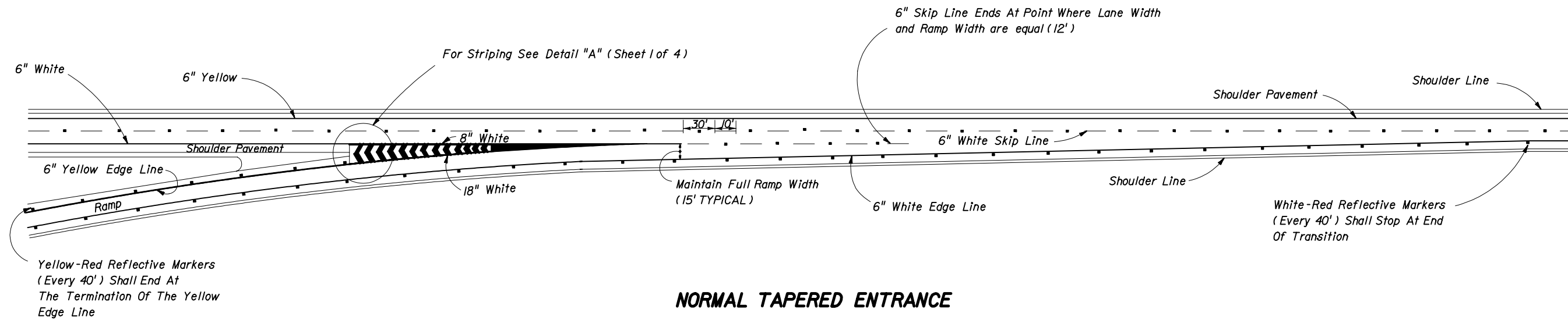


**NORMAL TAPERED EXIT ONLY
(TWO THRU LANES - THREE APPROACH LANES)**

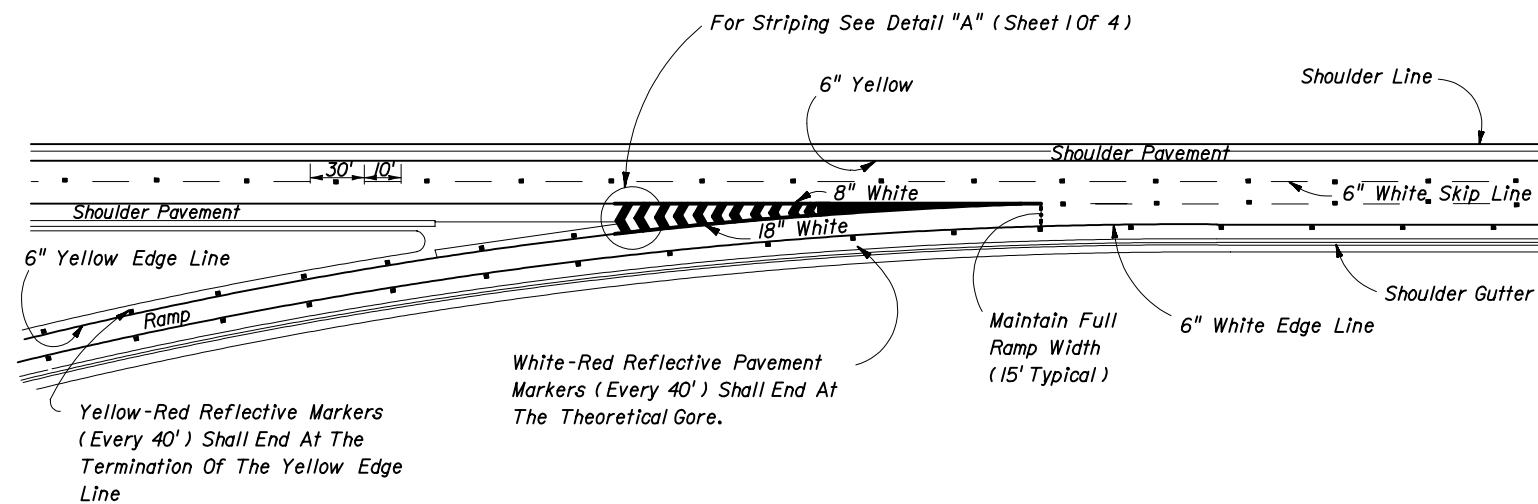
STATE OF FLORIDA DEPARTMENT OF TRANSPORTATION

INTERCHANGE MARKINGS

Names	Dates	Approved By		
Designed By	9-73	Clark A. Scott State Traffic Standards Engineer		
Drawn By		Revision	Sheet No.	Index No.
Checked By	9-73	04	1 of 4	17345




NORMAL TAPERED ENTRANCE

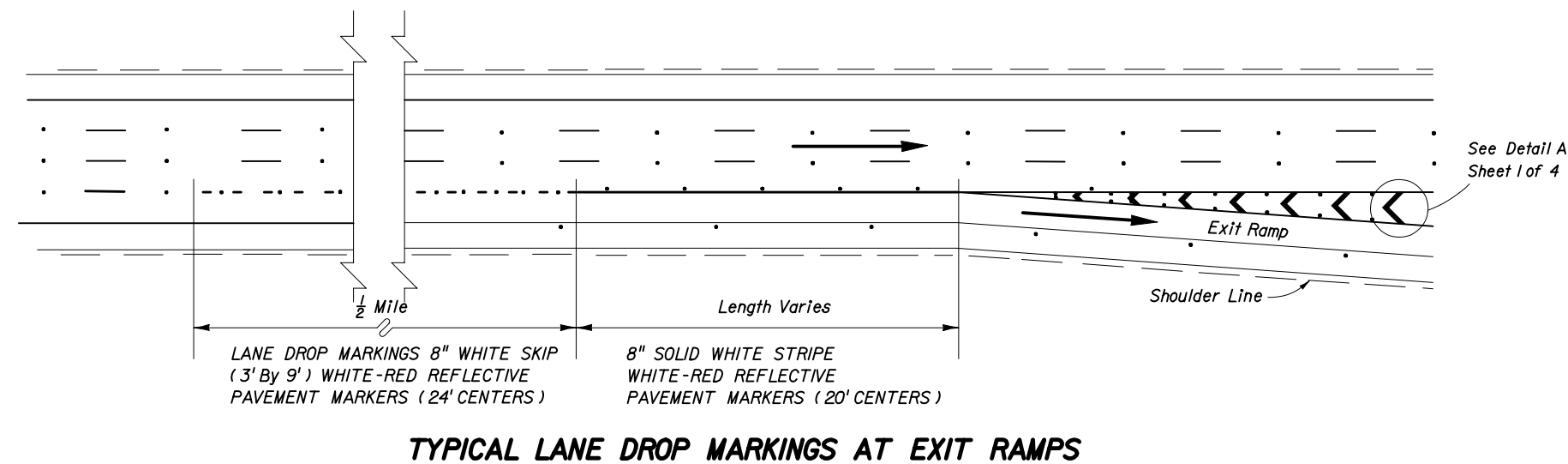
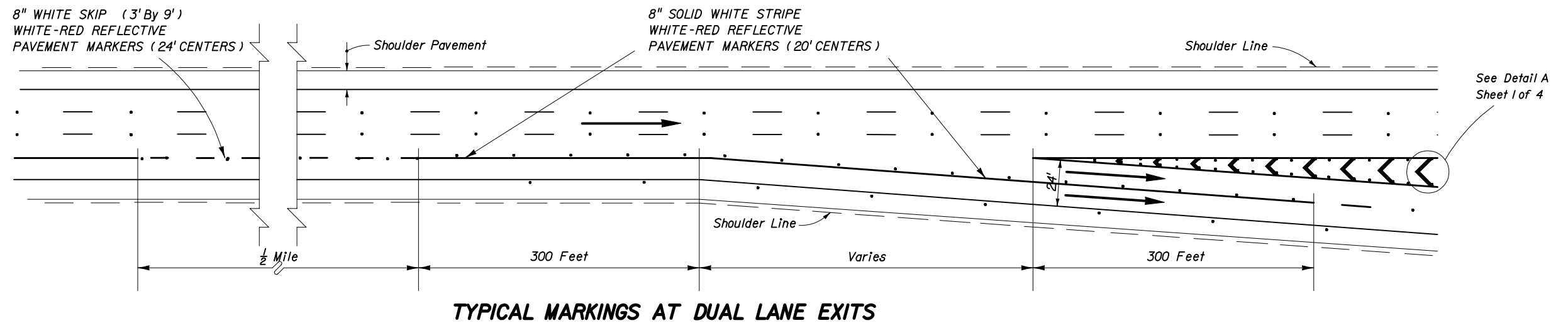
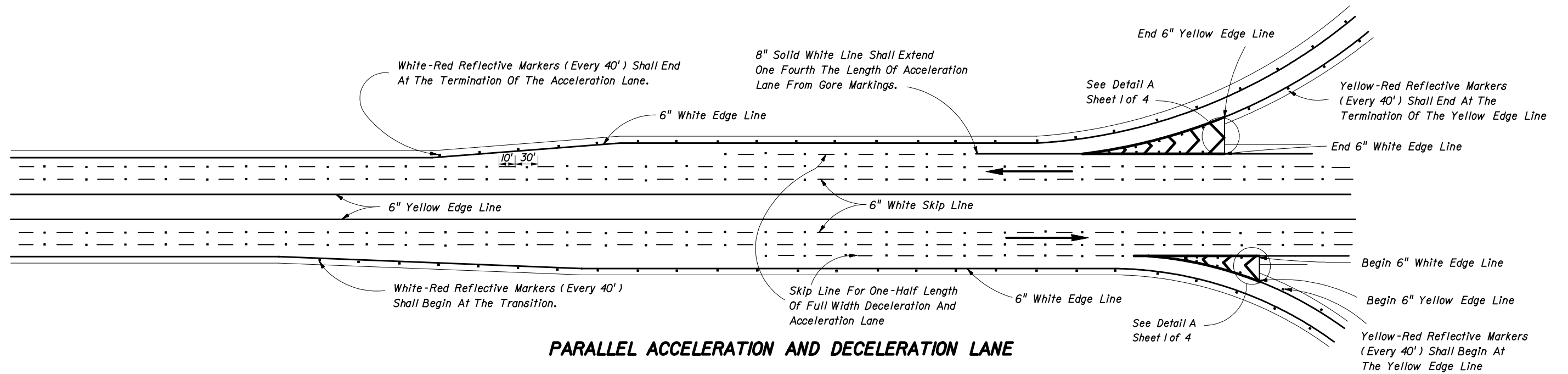


NORMAL TAPERED ENTRANCE WITH ADDED LANE

STATE OF FLORIDA DEPARTMENT OF TRANSPORTATION

INTERCHANGE MARKINGS

Names	Dates	Approved By		
Designed By	7-73	 State Traffic Standards Engineer		
Drawn By				
Checked By	7-73	Revision	Sheet No.	Index No.
		04	2 of 4	17345

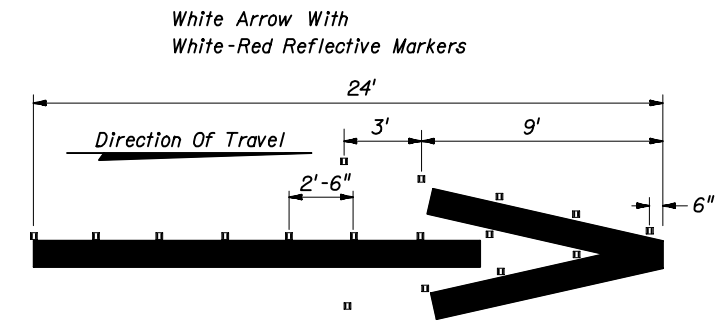
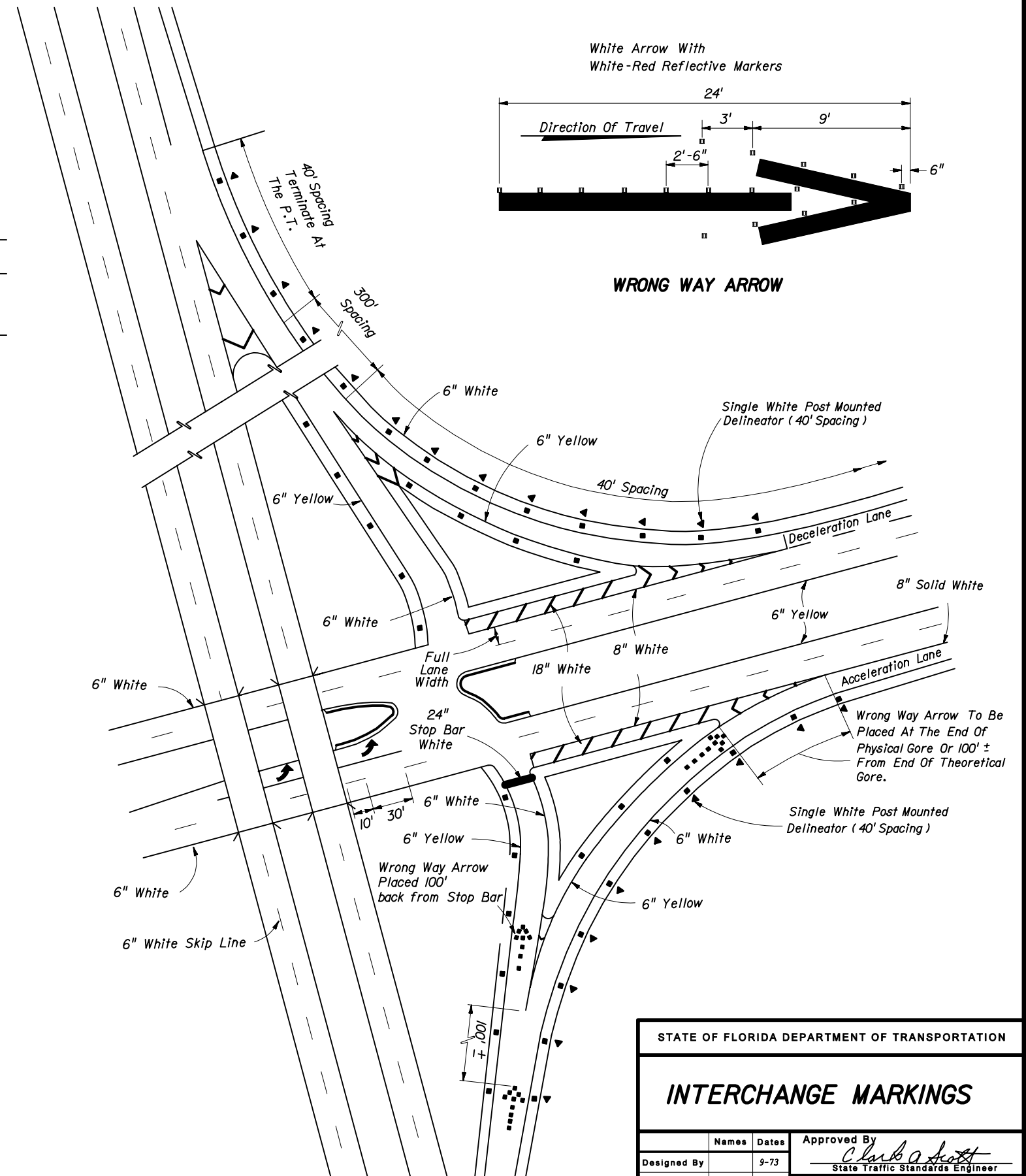
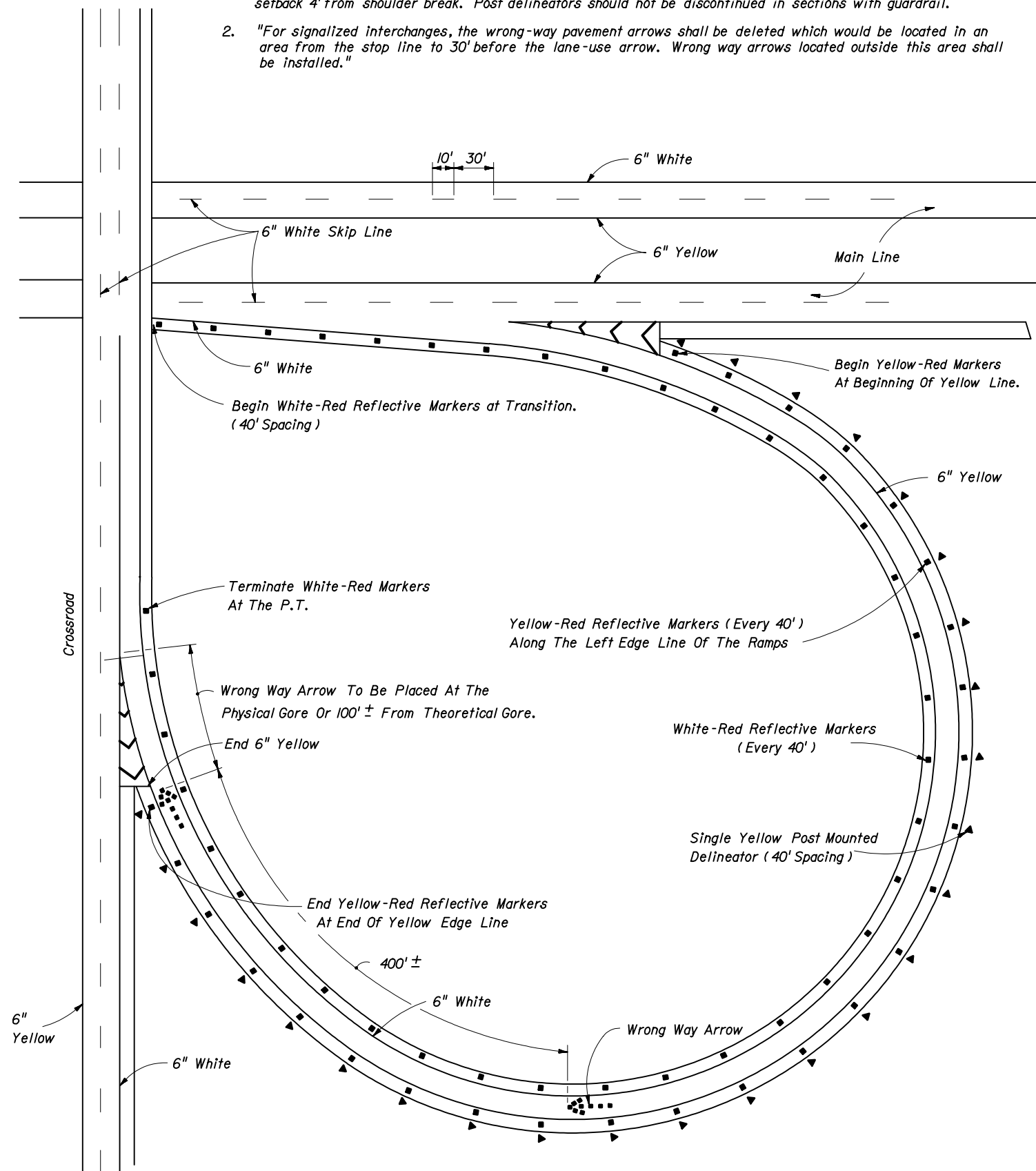


→ Note: Arrows indicate direction of travel and are not shown for pavement marking.

STATE OF FLORIDA DEPARTMENT OF TRANSPORTATION				
INTERCHANGE MARKINGS				
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Checked By		9-73	Revision	Sheet No. Index No.
			04	3 of 4 17345

Notes:

1. Post delineators spaced at 40' begin at the P.C. and end at the P.T. of the entrance and terminus of ramps. The spacing on the ramp section between the entrance and terminus shall be 300'. All delineators are to be setback 4' from shoulder break. Post delineators should not be discontinued in sections with guardrail.
2. "For signalized interchanges, the wrong-way pavement arrows shall be deleted which would be located in an area from the stop line to 30' before the lane-use arrow. Wrong way arrows located outside this area shall be installed."



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			Revision	Sheet No.
			04	4 of 4
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