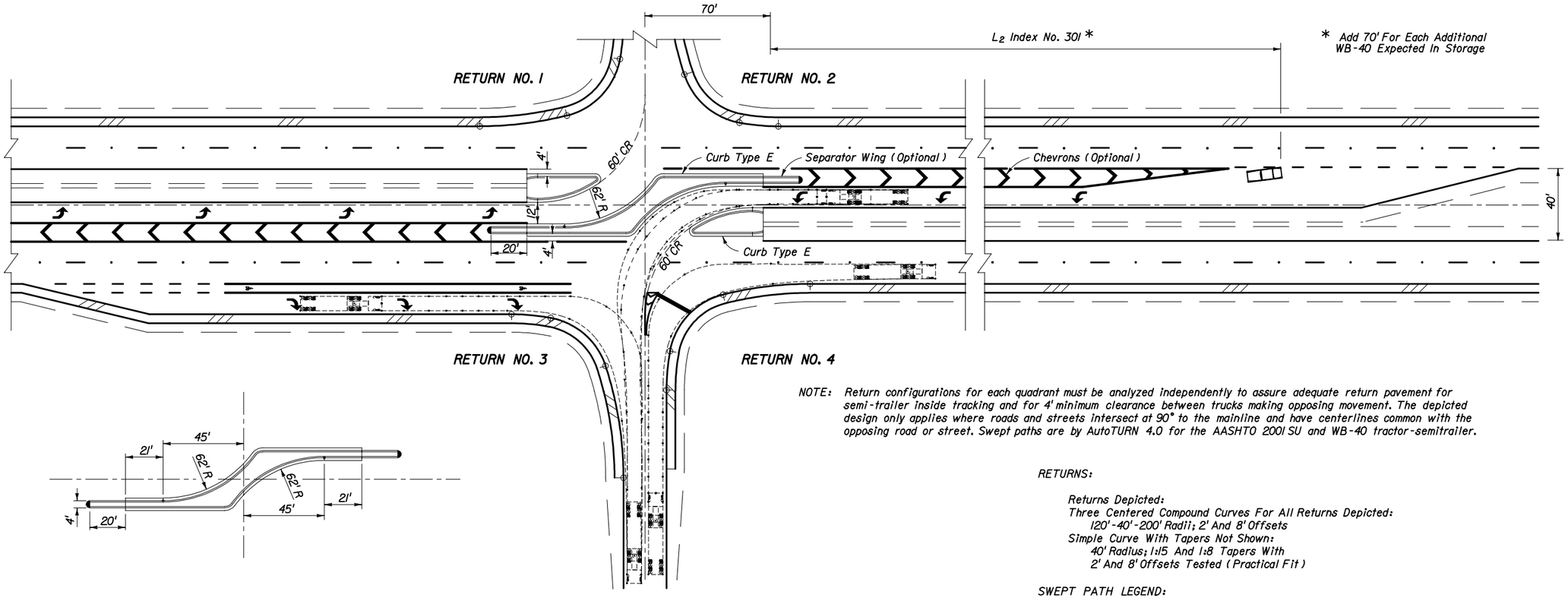


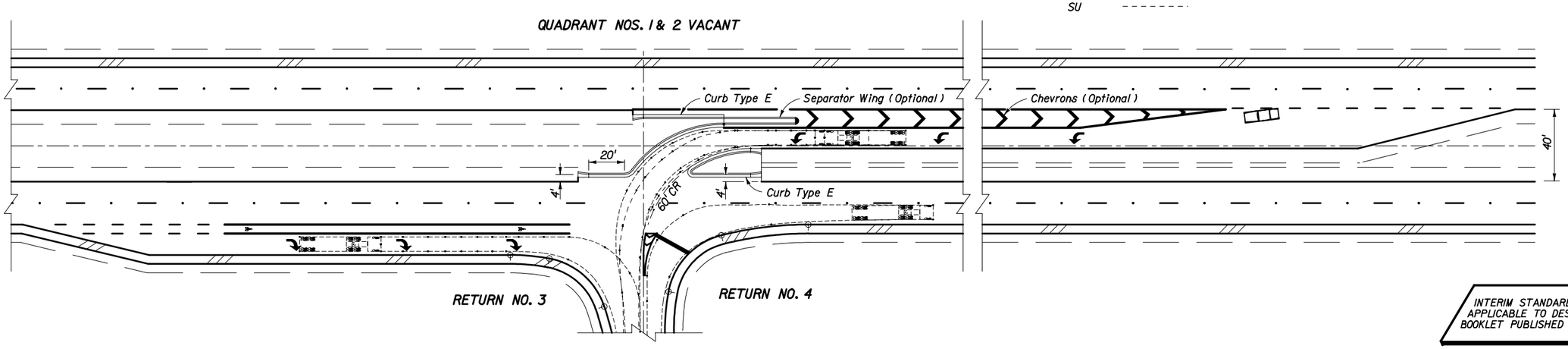

NOTE: Return configurations for each quadrant must be analyzed independently to assure adequate return pavement for semi-trailer inside tracking and for 4' minimum clearance between trucks making opposing movement. The depicted design only applies where roads and streets intersect at 90° to the mainline and have centerlines common with the opposing road or street. Swept paths are by AutoTURN 4.0 for the AASHTO 2001 SU and WB-40 tractor-semitrailer.

RETURNS:

Returns Depicted:
Three Centered Compound Curves For All Returns Depicted:
120'-40'-200' Radii; 2' And 8' Offsets
Simple Curve With Tapers Not Shown:
40' Radius; 1:15 And 1:8 Tapers With
2' And 8' Offsets Tested (Practical Fit)

SWEPT PATH LEGEND:

WB 40 ----------
SU - - - - - - - -




NOTE: Return configurations for each quadrant must be analyzed independently to assure adequate return pavement for semi-trailer inside tracking. The depicted design only applies where roads and streets intersect at 90° to the mainline. Swept paths are by AutoTURN 4.0 for the AASHTO 2001 SU and WB-40 tractor-semitrailer.

INTERIM STANDARD IN ENGLISH UNITS APPLICABLE TO DESIGN STANDARDS BOOKLET PUBLISHED IN ENGLISH UNITS.

# 40' MEDIAN • 4-LANE DIVIDED • PARALLEL TURN BAY • 2001 AASHTO SU & WB-40 (WB-12)

Date: 01-07-03

STATE OF FLORIDA DEPARTMENT OF TRANSPORTATION

## DIRECTIONAL MEDIAN OPENINGS

INTERIM STANDARD

THIS INDEX IS A SUPPLEMENT TO INDEX NO. 526 OF THE DESIGN STANDARDS, BOOKLET DATED JANUARY 2002.

APPROVED BY Roadway Design Engineer

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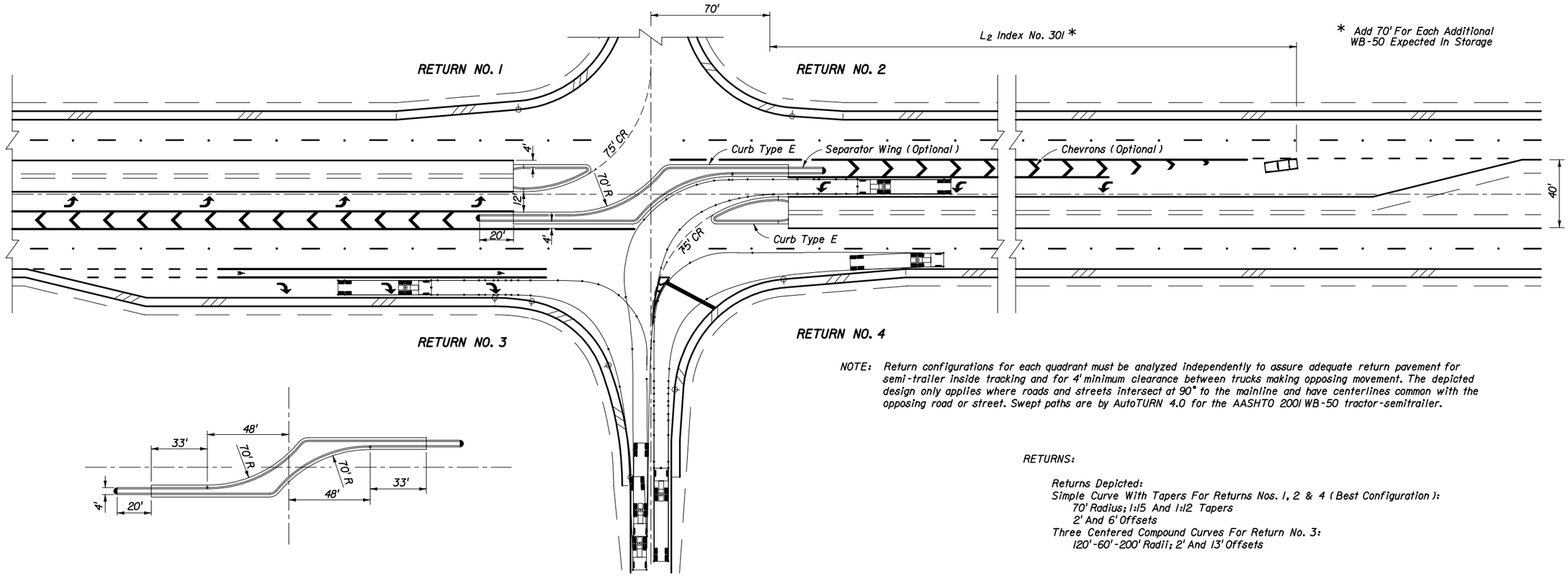
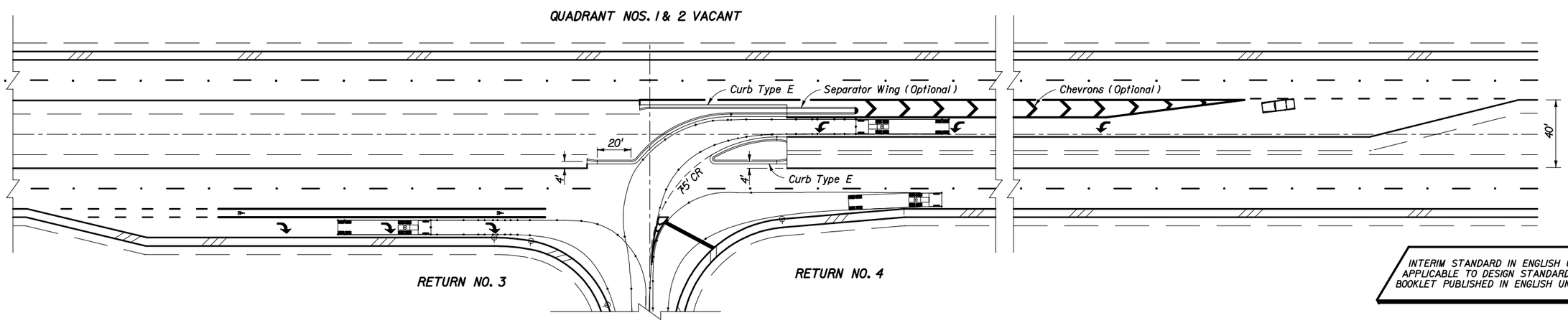



NOTE: Return configurations for each quadrant must be analyzed independently to assure adequate return pavement for semi-trailer inside tracking and for 4' minimum clearance between trucks making opposing movement. The depicted design only applies where roads and streets intersect at 90° to the mainline and have centerlines common with the opposing road or street. Swept paths are by AutoTURN 4.0 for the AASHTO 2001 WB-50 tractor-semitrailer.

RETURNS:

Returns Depicted:
Simple Curve With Tapers For Returns Nos. 1, 2 & 4 (Best Configuration):
70' Radius; 1:15 And 1:12 Tapers
2' And 6' Offsets
Three Centered Compound Curves For Return No. 3:
120'-60'-200' Radii; 2' And 13' Offsets




INTERIM STANDARD IN ENGLISH UNITS APPLICABLE TO DESIGN STANDARDS BOOKLET PUBLISHED IN ENGLISH UNITS.

NOTE: Return configurations for each quadrant must be analyzed independently to assure adequate return pavement for semi-trailer inside tracking. The depicted design only applies where roads and streets intersect at 90° to the mainline. Swept paths are by AutoTURN 4.0 for the AASHTO 2001 WB-50 tractor-semitrailer.

# 40' MEDIAN • 4-LANE DIVIDED • PARALLEL TURN BAY • 2001 AASHTO WB-50 (WB-15)

Date: 01-07-03

STATE OF FLORIDA DEPARTMENT OF TRANSPORTATION

## DIRECTIONAL MEDIAN OPENINGS

INTERIM STANDARD

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APPROVED BY Roadway Design Engineer

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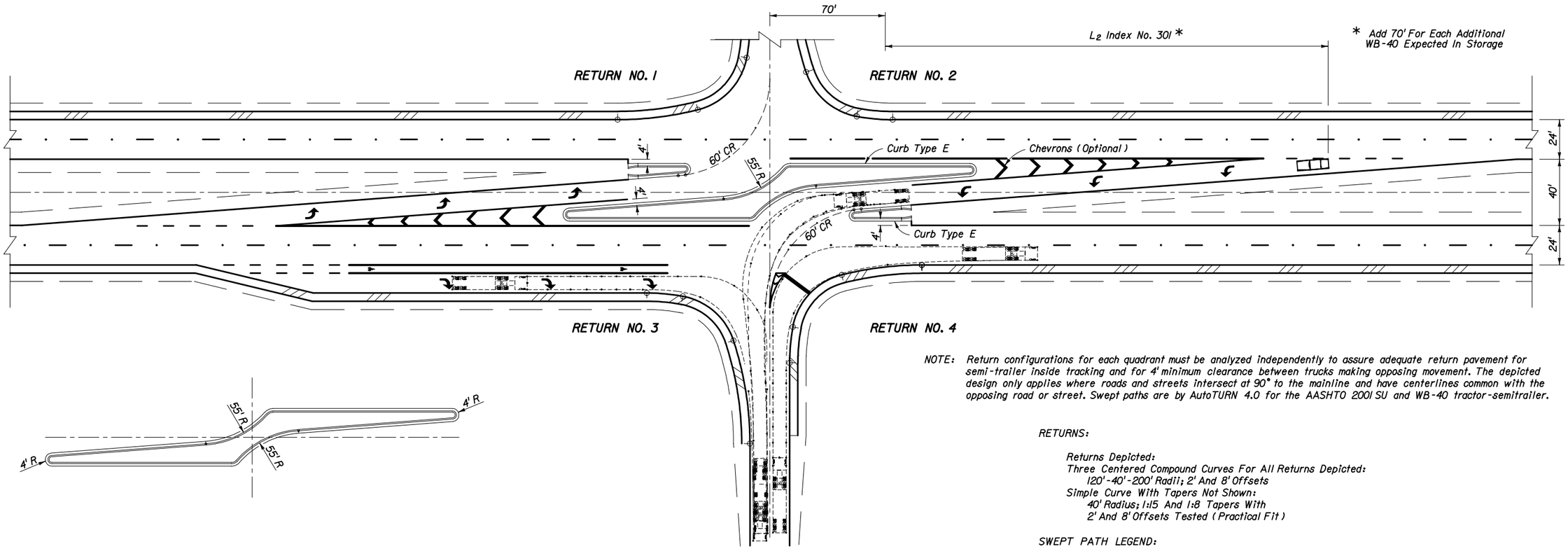






NOTE: Return configurations for each quadrant must be analyzed independently to assure adequate return pavement for semi-trailer inside tracking and for 4' minimum clearance between trucks making opposing movement. The depicted design only applies where roads and streets intersect at 90° to the mainline and have centerlines common with the opposing road or street. Swept paths are by AutoTURN 4.0 for the AASHTO 2001 SU and WB-40 tractor-semitrailer.

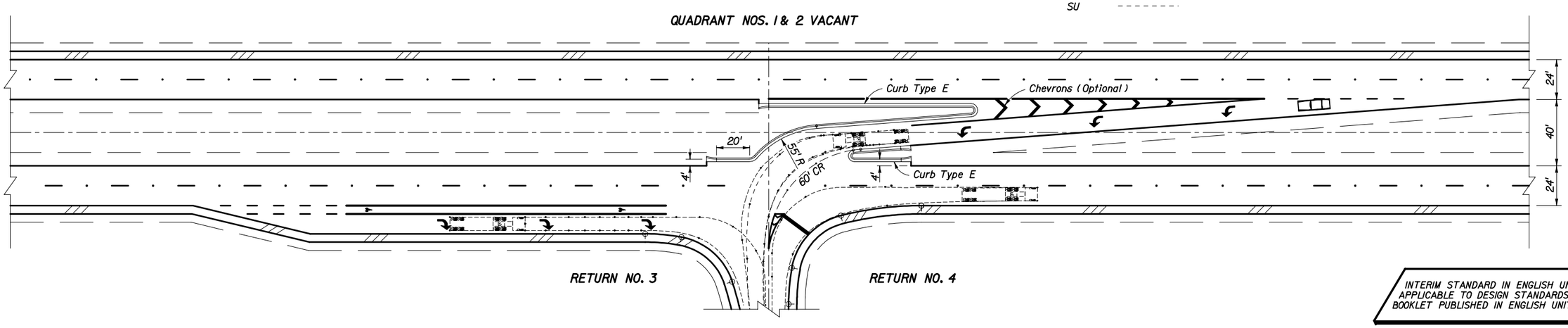
RETURNS:

Returns Depicted:
Three Centered Compound Curves For All Returns Depicted:
120'-40'-200' Radii; 2' And 8' Offsets
Simple Curve With Tapers Not Shown:
40' Radius; 1:15 And 1:8 Tapers With
2' And 8' Offsets Tested (Practical Fit)

SWEPT PATH LEGEND:

WB 40 ----------
SU - - - - - - - -

QUADRANT NOS. 1 & 2 VACANT




INTERIM STANDARD IN ENGLISH UNITS APPLICABLE TO DESIGN STANDARDS BOOKLET PUBLISHED IN ENGLISH UNITS.

NOTE: Return configurations for each quadrant must be analyzed independently to assure adequate return pavement for semi-trailer inside tracking. The depicted design only applies where roads and streets intersect at 90° to the mainline. Swept paths are by AutoTURN 4.0 for the AASHTO 2001 SU and WB-40 tractor-semitrailer.

# 40' MEDIAN • 4-LANE DIVIDED • TAPERED TURN BAY • 2001 AASHTO SU & WB-40 (WB-12)

Date: 01-07-03

STATE OF FLORIDA DEPARTMENT OF TRANSPORTATION

## DIRECTIONAL MEDIAN OPENINGS

INTERIM STANDARD

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APPROVED BY
Roadway Design Engineer

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