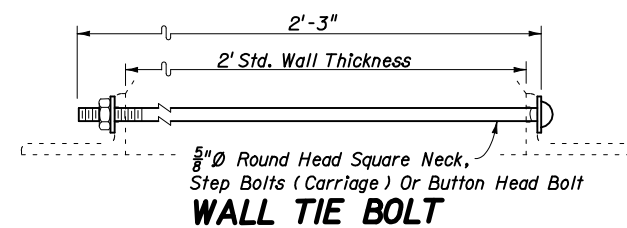
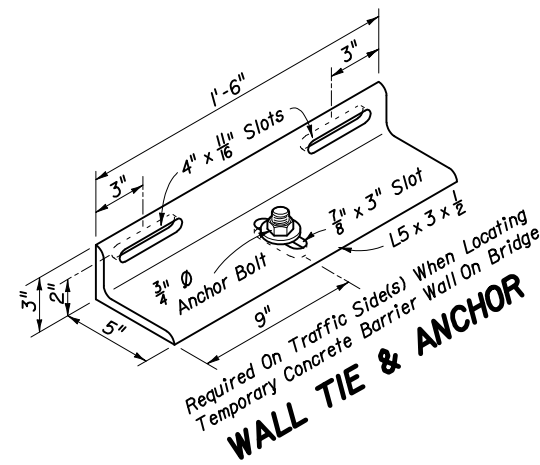


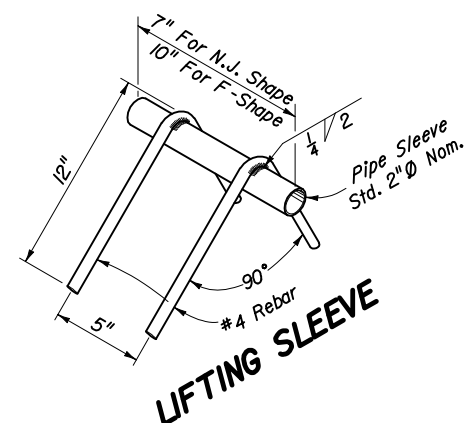
WALL TIE ANCHOR PIPE



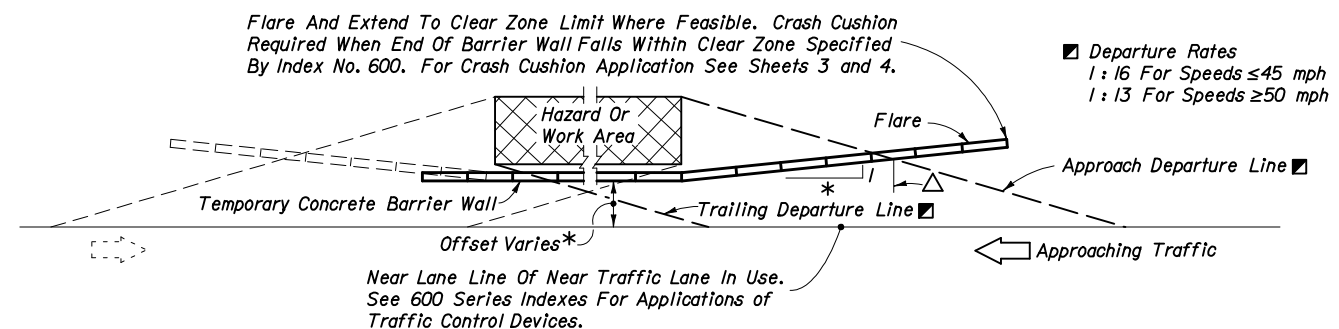
WALL TIE BOLT



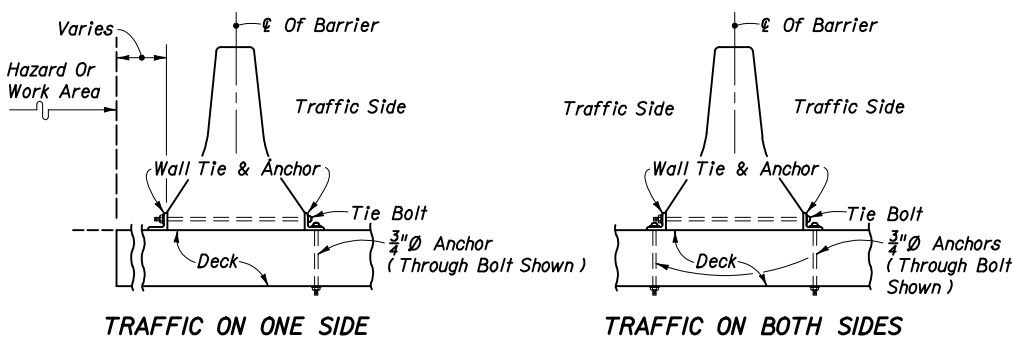
WALL TIE & ANCHOR



LIFTING SLEEVE

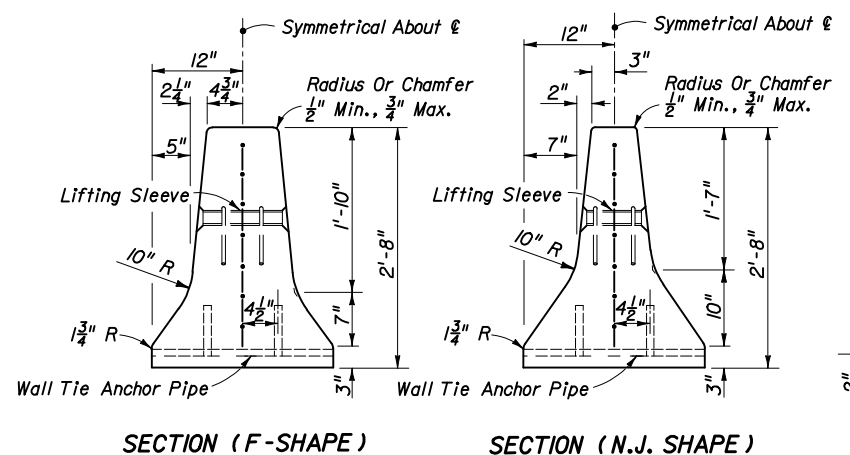


**PLAN
TEMPORARY CONCRETE BARRIER WALL ALIGNMENT**

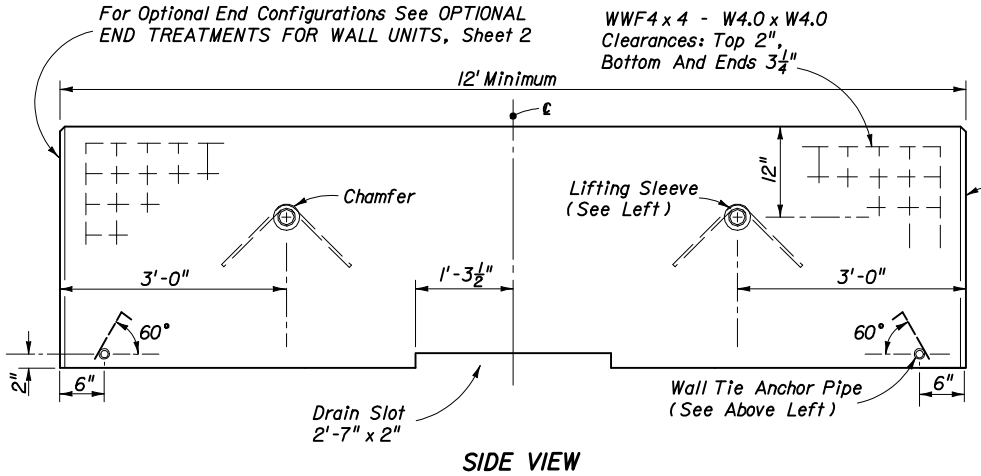


Anchor bolts shall have a pullout and shear capacity of 14,000 lbs. Expansion or chemical anchor bolts will be used to secure walls to approach slabs. Expansion or chemical anchor bolts or through bolts with washers and nuts will be used to secure walls to bridge decks. Core drills shall be used to construct through bolt holes, and, drills specified by the manufacturer shall be used to construct expansion and chemical anchor bolt holes. Chemical anchorage shall be an Adhesive Material System in accordance with Specification Sections 416 and 937. After removal of walls, anchors shall be removed to 1" min. below deck surface and holes filled with epoxy grout.

BRIDGE DECK AND APPROACH SLAB INSTALLATIONS

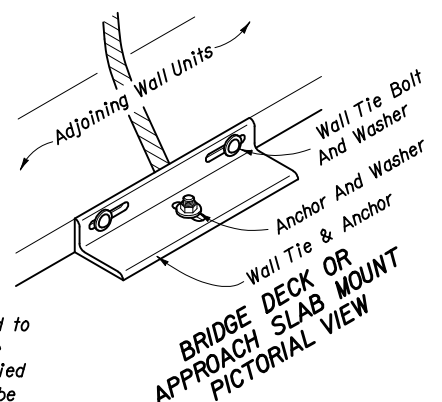


SECTION (F-SHAPE) SECTION (N.J. SHAPE)



SIDE VIEW

WALL UNIT



BRIDGE DECK OR APPROACH SLAB MOUNT PICTORIAL VIEW

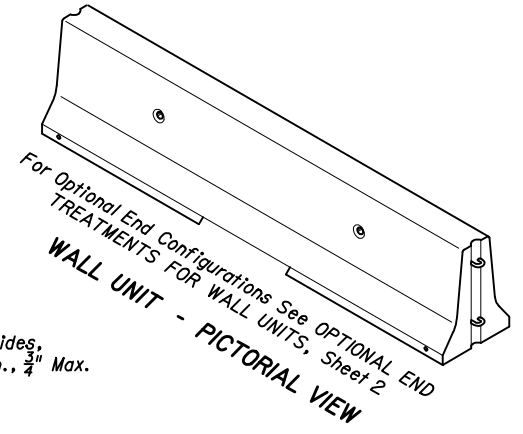
GENERAL NOTES

1. Temporary concrete barrier wall units may be either the New Jersey shape or the F-Shape configuration, unless the plans specify other types of temporary concrete barrier wall; however, intermixing of units with different shapes in a continuous run of barrier will not be permitted.
2. Material and workmanship for the wall shall meet the requirements of Sections 400 and 521 of the Standard Specifications, except the bottom of the unit can be finished to a dense uniform surface by floating in lieu of the Class 3 finish. Concrete shall be Class II.
3. Type C Steady-Burn Lights are to be mounted on top of temporary concrete barrier walls that are used as barriers along travel ways in work zones. The lights are to be spaced at 50' centers in transitions, 100' centers on curves and 200' centers on tangent roadways. For additional information refer to 'Warning Lights' on Index No. 600.
4. Wall units shall not be used for permanent barrier wall construction regardless of unit length, unless specifically permitted by the plans.
5. The temporary concrete barrier wall units with the optional end connections shown on this index are the standard optional units for Florida Department Of Transportation projects. Standard optional end units can be intermixed in a run of wall, and interconnected with other barrier systems as specified on other standard drawings or with appropriate transitions as detailed in the plans.

Temporary concrete barrier wall units with end configurations that are on the 'Qualified Products' listing may be substituted for the standard optional end units when approved by the Engineer; however, substitution units cannot be intermixed with dissimilar units in a run of wall. Substitution units shall have positive interconnections between each adjoining unit; wall units with plain ends will not be permitted regardless of ties or anchorages.

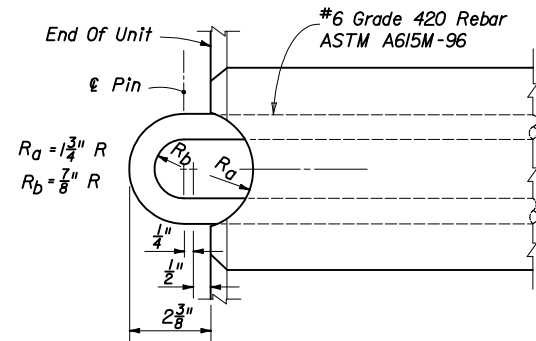
The temporary concrete barrier wall unit design shown on this index is to be discontinued from use on State highway projects by October 1, 2012; see the NOTICE on sheet 3 of 4. Any wall unit that contains double drain slots, that were cast in conformance with prior Index No. 415 designs and current end connections, are to be used only on the high side of the cross section.

6. Wall units may be reused provided they have the structural integrity and surface qualities of new units.
7. Wall units shall be furnished by the Contractor except when the plans stipulate the availability of Department owned units. Regardless of unit source the Contractor shall furnish all hardware and shall be responsible for all handling including loading, transport, unloading, stockpiling, installation, removal and return.
- When the plans stipulate that temporary concrete barrier wall(s) are to become property of the Department at completion of the project, only the standard optional end treatment units are to be used, except as otherwise specified in the plans.
8. If the plans specify Barrier (Temporary) Optional, the Contractor has the option to furnish either concrete or water filled barriers. If the plans specify Barrier (Temporary) Concrete, substitution with water filled barriers will not be permitted.
9. Wall units used for work zone traffic control and other temporary applications shall be paid for under the contract unit price for Barrier (Temporary) Concrete, LF, or Barrier (Temporary) Optional, LF. Type C Steady-Burn Lights shall be paid for under the contract unit price for Lights, Temp. Barrier Wall Mount (Type C, Steady-Burn), ED.

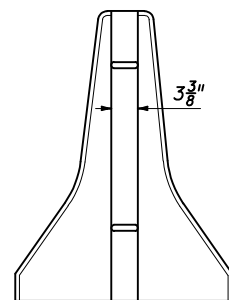


**For Optional End Configurations See OPTIONAL END TREATMENTS FOR WALL UNITS, Sheet 2
WALL UNIT - PICTORIAL VIEW**

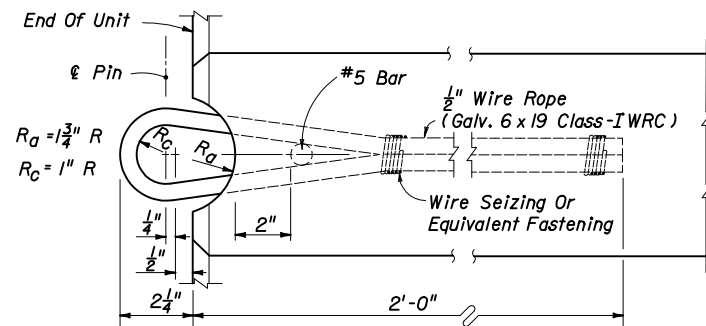
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|--|-----------|-------------------------|-----------|-----------|
| STATE OF FLORIDA DEPARTMENT OF TRANSPORTATION | | | | |
| PRECAST CONCRETE TEMPORARY BARRIER WALL | | | | |
| Names | Dates | Approved By | | |
| Designed By | | Roadway Design Engineer | | |
| Drawn By | HSD 04/82 | Revision | Sheet No. | Index No. |
| Checked By | JVG 04/82 | 02 | 1 of 4 | 415 |



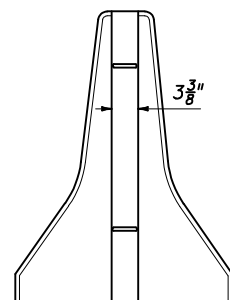
TOP VIEW



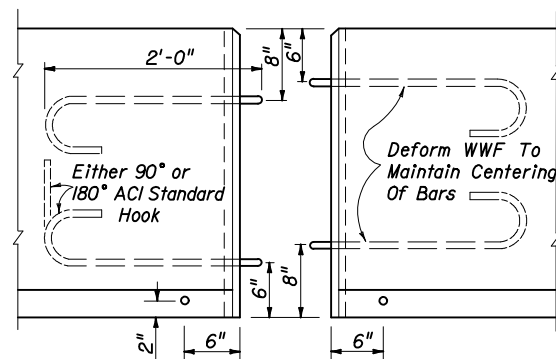
END VIEW



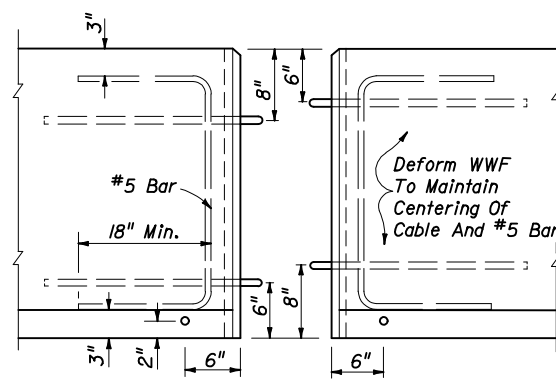
TOP VIEW



END VIEW

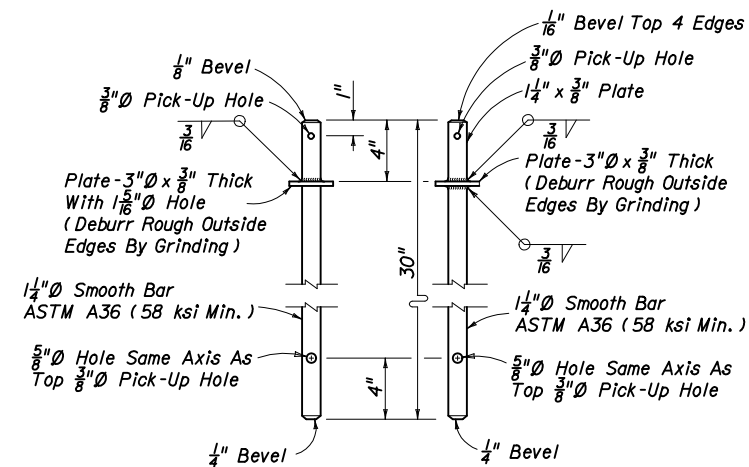


SIDE VIEW
ROUND BAR CONNECTOR



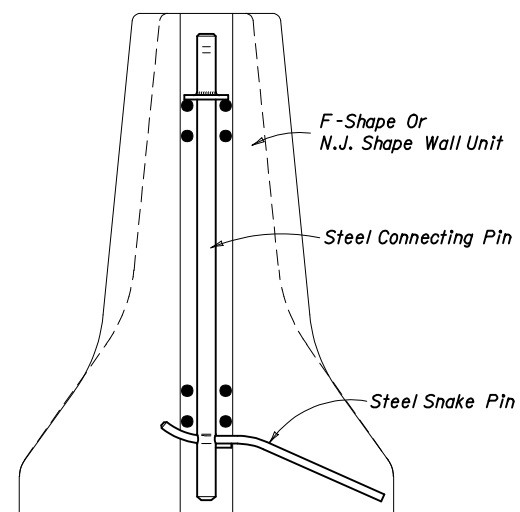
SIDE VIEW
WIROPE CONNECTOR

OPTIONAL END TREATMENTS FOR WALL UNITS



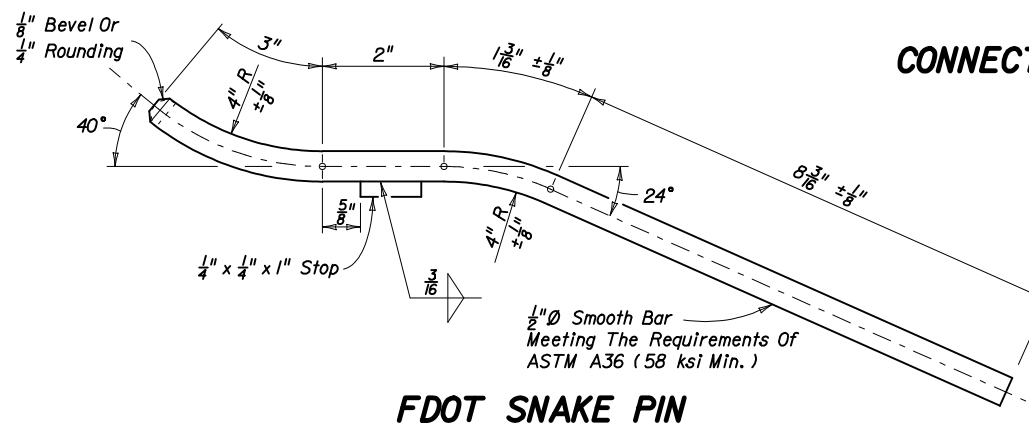
OPTIONAL PINS

STEEL CONNECTING PIN

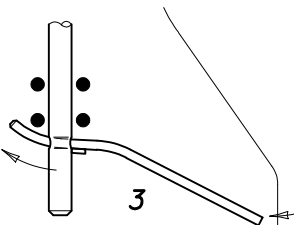
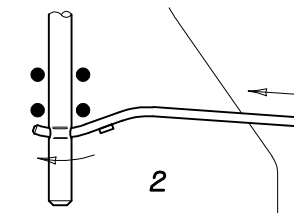
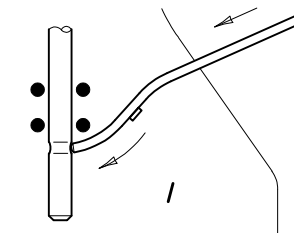


ASSEMBLED UNIT

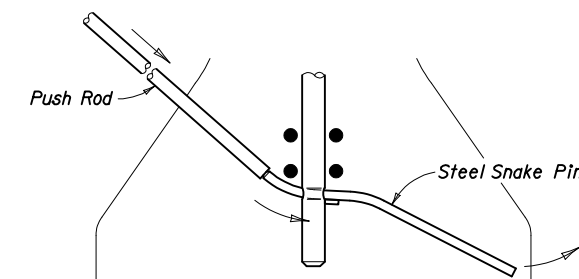
CONNECTING PIN ASSEMBLY



FDOT SNAKE PIN



INSERTING FDOT SNAKE PIN

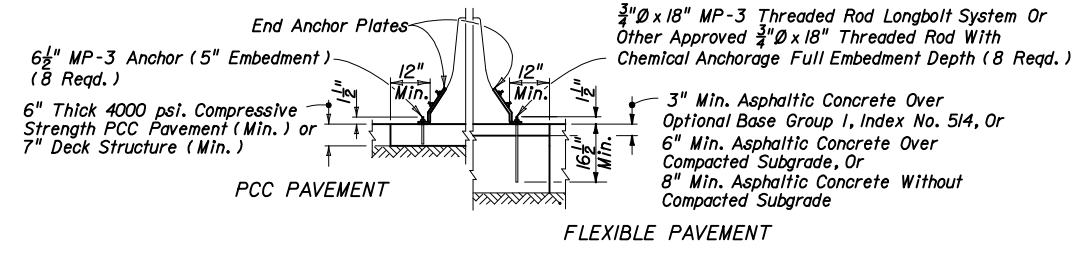
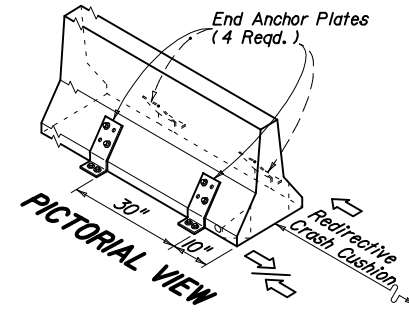


REMOVING FDOT SNAKE PIN

STATE OF FLORIDA DEPARTMENT OF TRANSPORTATION

PRECAST CONCRETE
TEMPORARY BARRIER WALL

| Names | Dates | Approved By | | |
|-------------|-----------|-----------------------------------|-----------|-----------|
| Designed By | | State Roadway Design Engineer | | |
| Drawn By | HSD 04/82 | | | |
| Checked By | JVG 04/82 | Revision | Sheet No. | Index No. |
| | | 00 | 2 of 4 | 415 |



SURFACE ANCHORAGE REQUIREMENTS

END ANCHORAGE NOTES

- For temporary barrier wall end anchorage applications, see 'TEMPORARY CONCRETE BARRIER WALL ALIGNMENT' and 'NOTES FOR TEMPORARY CONCRETE BARRIER WALL END SHIELDING'.
- The temporary concrete barrier wall anchor plate depicted above is a proprietary design by Energy Absorption Systems, Inc. Other temporary anchorage methods can be substituted when wall rigidity is assured by any of the following:
 - proven by associated crash test of redirective crash cushions, or
 - meet anchorage prescribed in 'A Guide To Standardized Highway Barrier Hardware', or
 - crash cushion manufacturer's engineered design, or
 - approved shop drawings on a case by case basis.
- The cost for anchoring the wall segment will be included in the cost for the adjoining redirective crash cushion.

BARRIER WALL END ANCHORAGE

NOTES FOR TEMPORARY CONCRETE BARRIER WALL END SHIELDING

- Redirective crash cushions are the principal (standard) device to be used for shielding approach ends of temporary concrete barrier walls. Except where the plans designate a particular type of redirective crash cushion for a specific location, the contractor has the option to construct either the REACT 350, QuadGuard, ADIEM 350 or TRACC crash cushions subject to the uses and limitations described on Index Nos. 434, 435, 436 and 440 respectively. The barrier wall end segment must be anchored to a paved surface in accordance with 'BARRIER WALL END ANCHORAGE'.
- Temporary redirective crash cushions shall be installed in accordance with the manufacturer's specifications and recommendations. Temporary crash cushions can be either new or functionally sound used devices. Performance of intended function is the only condition for acceptance, whether the crash cushion is new, used, refurbished, purchased, leased, rented, on loan, shared between projects, or made up of mixed new and used components.
- Inertial crash cushions are not optional systems for locations designated for redirective crash cushions by the plans; can not be substituted for redirective crash cushions, and are not eligible for VECP consideration.
- A yellow post mounted Type 1 Object Marker shall be centered 3' in front of the nose of all temporary crash cushions. Mounting hardware shall be in accordance with Index Nos. 11860 and 11865. The cost of the Object Marker shall be included in the cost of the crash cushion.
- Optional temporary redirective crash cushions are to be paid for per location under the contract unit price for Vehicular Impact Attenuator (Temporary) (Redirective Option), LO.

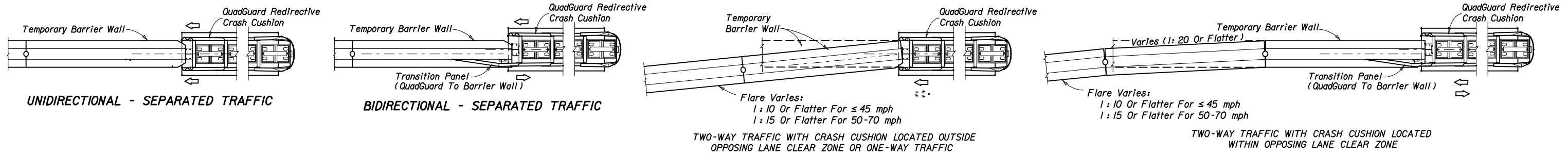
NOTICE

THE TEMPORARY CONCRETE BARRIER WALL UNIT SHOWN ON THIS INDEX THAT IS PRODUCED PRIOR TO OCTOBER 1, 2002, AND THAT IS IN GOOD CONDITION, CAN BE USED ON STATE HIGHWAY PROJECTS THROUGH SEPTEMBER 30, 2012. TEMPORARY CONCRETE BARRIER UNITS PRODUCED ON AND AFTER OCTOBER 1, 2002 FOR USE ON STATE HIGHWAY PROJECTS MUST MEET NCHRP 350 CRITERIA, AND MUST BE INCLUDED ON THE QUALIFIED PRODUCTS LIST. IF AND WHEN A GENERIC TEMPORARY CONCRETE BARRIER WALL UNIT IS APPROVED FOR USE ON STATE HIGHWAY PROJECTS, THE UNIT DESIGN WILL BE POSTED ON THE ROADWAY DESIGN WEB SITE.

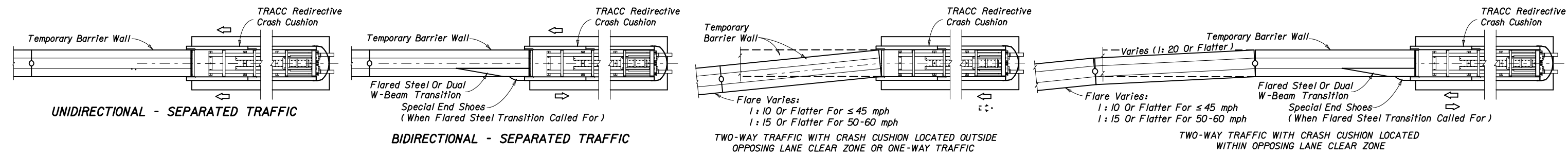
STATE OF FLORIDA DEPARTMENT OF TRANSPORTATION

**PRECAST CONCRETE
TEMPORARY BARRIER WALL**

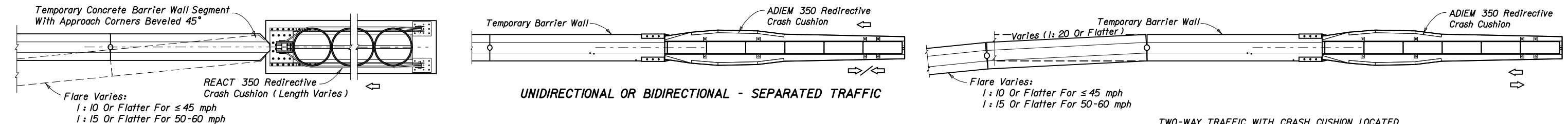
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| Designed By | Names | Dates | Approved By | | |
| Drawn By | HKH | 03/99 | Revision | Sheet No. | Index No. |
| Checked By | JVG | 03/99 | 02 | 3 of 4 | 415 |



SHOULDER - RIGHT OR LEFT (RIGHT SIDE SHOWN)
TEMPORARY CONCRETE BARRIER WALL END TREATMENT WHEN SHIELDED BY A QuadGuard CRASH CUSHION



SHOULDER - RIGHT OR LEFT (RIGHT SIDE SHOWN)
TEMPORARY CONCRETE BARRIER WALL END TREATMENT WHEN SHIELDED BY A TRACC CRASH CUSHION



FOR ANY APPROACH CONDITION IN ACCORDANCE WITH INDEX NO. 434
TEMPORARY CONCRETE BARRIER WALL END TREATMENT WHEN SHIELDED BY A REACT 350 CRASH CUSHION

SHOULDER - RIGHT OR LEFT (RIGHT SIDE SHOWN)
TEMPORARY CONCRETE BARRIER WALL END TREATMENT WHEN SHIELDED BY AN ADIEM 350 CRASH CUSHION

See 'TEMPORARY CONCRETE BARRIER WALL ALIGNMENT', 'BARRIER WALL END ANCHORAGE' and 'NOTES FOR TEMPORARY CONCRETE BARRIER WALL END SHIELDING' for additional information.

SHIELDING TEMPORARY CONCRETE BARRIER WALL ENDS WITH REDIRECTIVE CRASH CUSHIONS (REDIRECTIVE OPTION)

| | | | | |
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| STATE OF FLORIDA DEPARTMENT OF TRANSPORTATION | | | | |
| PRECAST CONCRETE TEMPORARY BARRIER WALL | | | | |
| Designed By | Names | Dates | Approved By | |
| Drawn By | HKH | 3/99 | <i>Ben Blankenship</i> State Roadway Design Engineer | |
| Checked By | JVG | 3/99 | Revision | 00 |
| | | | Sheet No. | 4 of 4 |
| | | | Index No. | 415 |