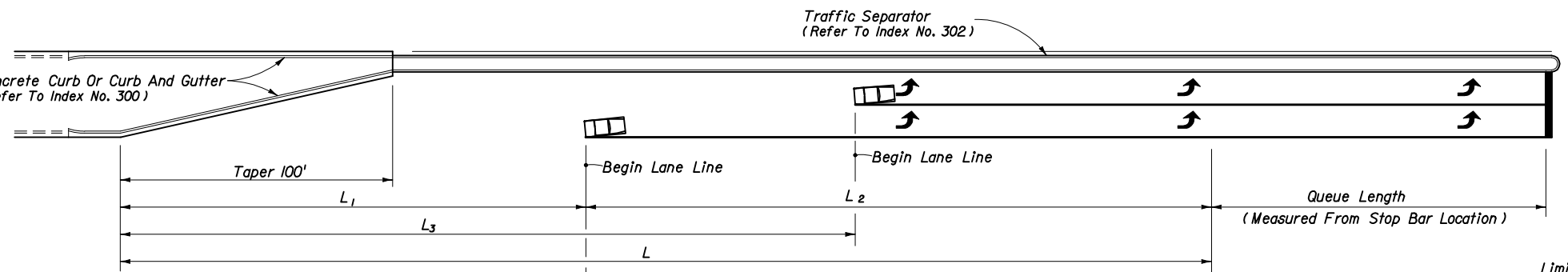
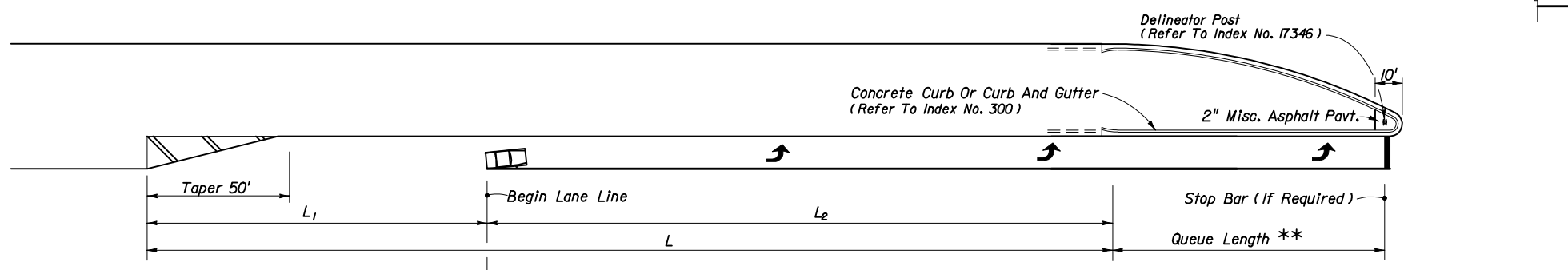


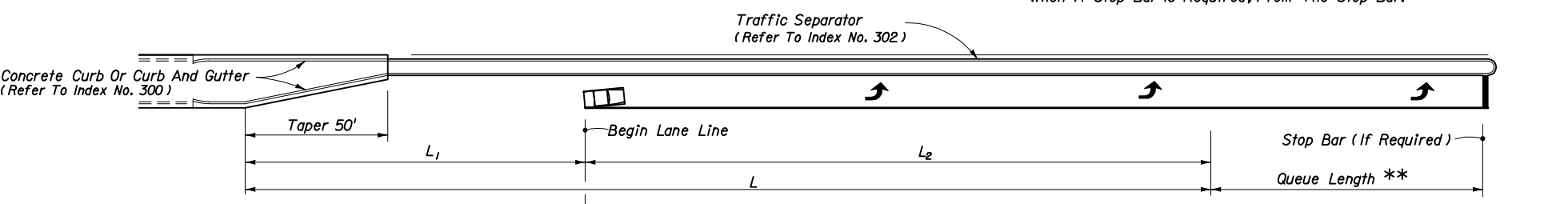
FLUSH AND/OR CURBED SEPARATION



**RAISED SEPARATION
DOUBLE LEFT TURNS**



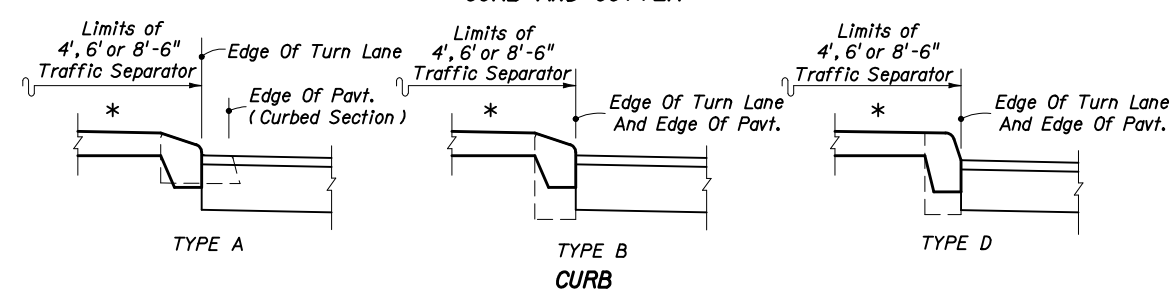
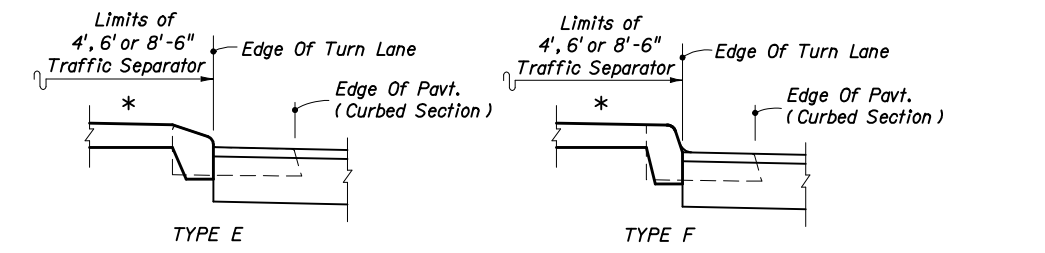
FLUSH AND/OR CURBED SEPARATION



**RAISED SEPARATION
SINGLE LEFT TURNS**

TURN LANES

TURN LANES • CURBED AND UNCURBED MEDIANS								
Design Speed (mph)	Entry Speed (mph)	Clearance Distance L ₁	URBAN CONDITITONS			RURAL CONDITITONS		
			Brake To Stop Distance L ₂	Total Decel. Distance L	Clearance Distance L ₃	Brake To Stop Distance L ₂	Total Decel. Distance L	Clearance Distance L ₃
35	25	70'	75'	145'	110'	---	---	---
40	30	80'	75'	155'	120'	---	---	---
45	35	85'	100'	185'	135'	---	---	---
50	40/44	105'	135'	240'	160'	215'	320'	160'
55	48	125'	---	---	---	260'	385'	195'
60	52	145'	---	---	---	310'	455'	230'
65	55	170'	---	---	---	350'	520'	270'



For Curb And Curb & Gutter Types, See Index No. 300
 * Option I Separators Shown (Refer To Index No. 302)
**MEDIAN CURB AND TRAFFIC SEPARATOR
 JUNCTURE DETAILS**

- GENERAL NOTES**
- The plan views shown are for turn lane taper shapes and dimensional purposes only, they do not prescribe the use of curb, curb and gutter, shoulders nor separators specifically to either rural or urban conditions.
 - Total deceleration distances must not be reduced except where lesser values are imposed by unrelocatable control points.
 - Right turn lane tapers and distances identical to left turn lanes under stop control conditions. Right turn lane tapers and/or distances are site specific under free flow or yield conditions.
 - These left turn configurations apply to continuous left turn lanes only where specifically called for in the plans.
 - For pavement markings see Index No. 17346.

- DESIGN NOTES**
- Basis for turn lane configurations:
 - Informed Driver.
 - Stop condition (With Or Without Stop Control).
 - Wet Pavement.
 - Reaction preceeding entry point.
 - Maximum safe deceleration rates for urban conditions.
 - Comfortable deceleration rates for rural conditions.

STATE OF FLORIDA DEPARTMENT OF TRANSPORTATION				
TURN LANES				
Designed By	Names	Dates	Approved By	
Drawn By	HSD/HKH	05/91	 State Roadway Design Engineer	
Checked By	JVG/RER	05/91		
	Revision	00	Sheet No.	Index No.
			1 of 1	301