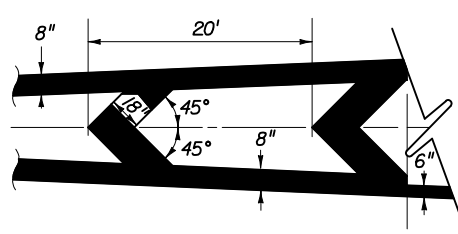
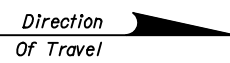


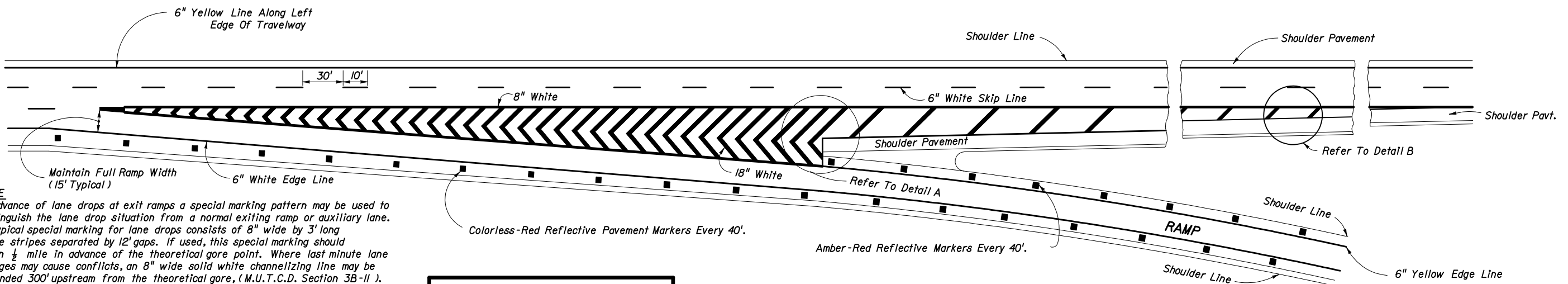
DETAIL A

For RPM Location Refer To Index 17352.



Note:
Reflective pavement markers are installed adjacent to the edge line.

**NORMAL TAPERED EXIT
(TWO THRU LANES)**

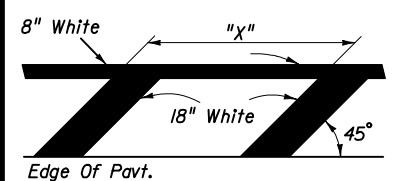


NOTE
In advance of lane drops at exit ramps a special marking pattern may be used to distinguish the lane drop situation from a normal exiting ramp or auxiliary lane. A typical special marking for lane drops consists of 8" wide by 3' long white stripes separated by 12' gaps. If used, this special marking should begin 1/2 mile in advance of the theoretical gore point. Where last minute lane changes may cause conflicts, an 8" wide solid white channelizing line may be extended 300' upstream from the theoretical gore. (M.U.T.C.D. Section 3B-11).

DETAIL B

"S" MPH	30	35	40	45	50	55
"X" Ft.	20	20	40	40	60	60

Passenger Car, Daytime, Posted Speeds
Or 85th Percentile (Use Higher Value)

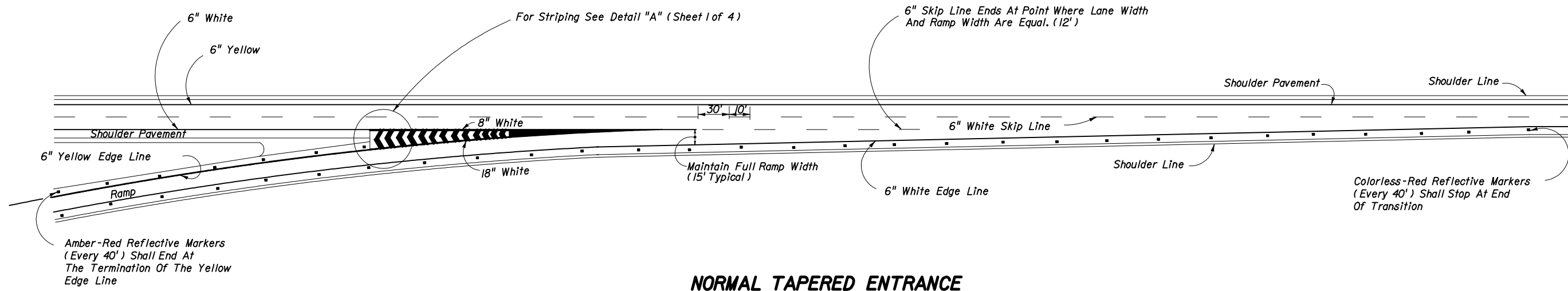


**NORMAL TAPERED EXIT ONLY
(TWO THRU LANES - THREE APPROACH LANES)**

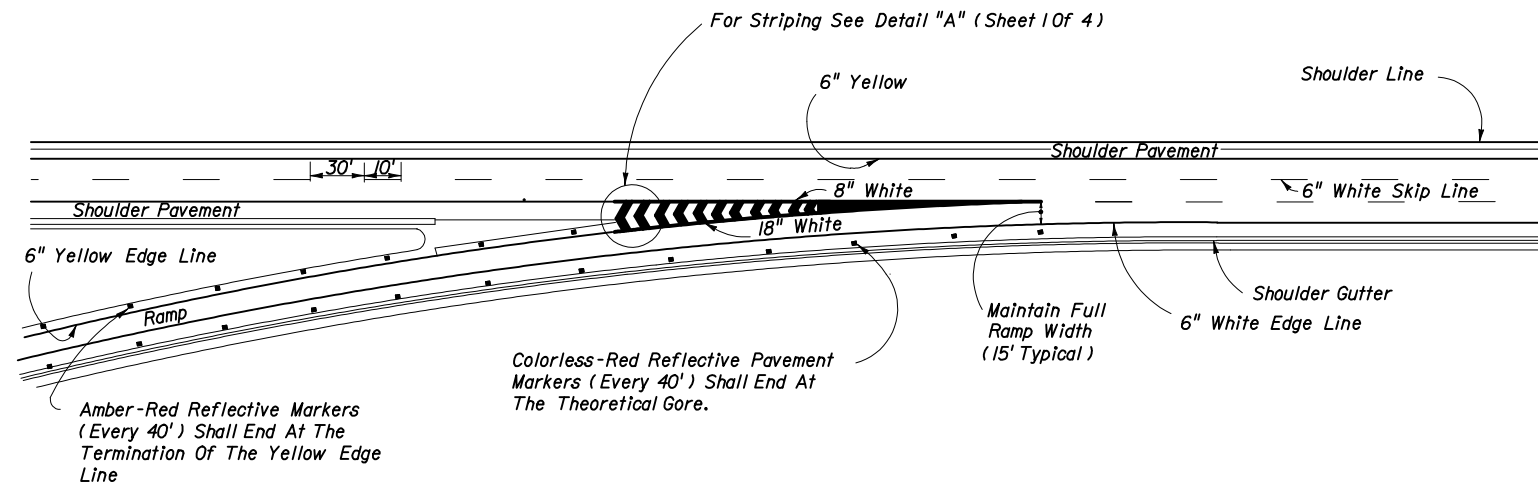
STATE OF FLORIDA DEPARTMENT OF TRANSPORTATION

INTERCHANGE MARKINGS

Names	Dates	Approved By		
Designed By	9-73	C. L. Scott		
Drawn By		State Traffic Standards Engineer		
Checked By	9-73	Revision	Sheet No.	Index No.
		00	1 of 4	17345




NORMAL TAPERED ENTRANCE

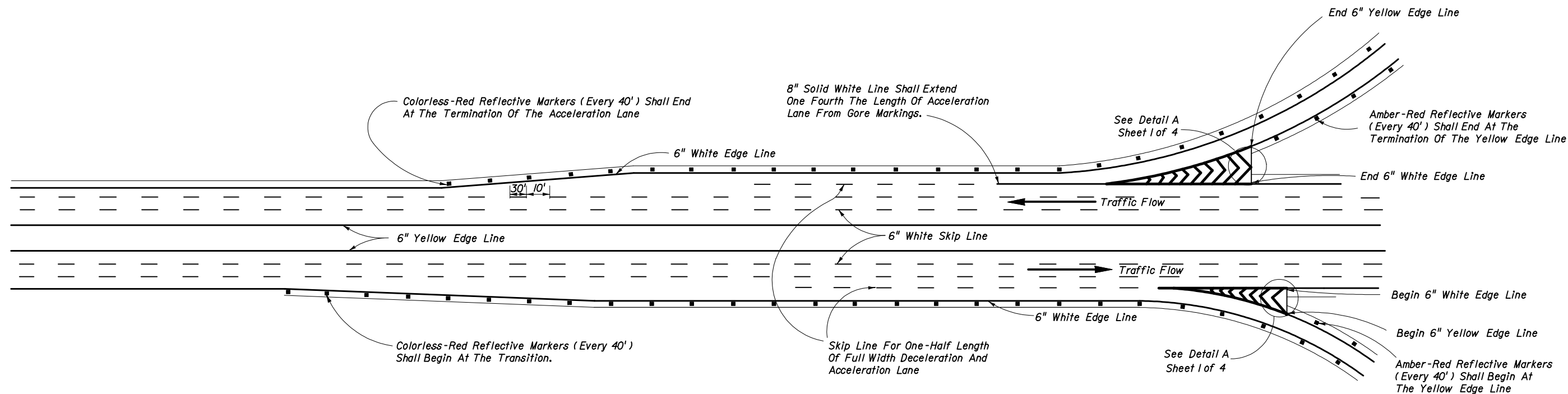


NORMAL TAPERED ENTRANCE WITH ADDED LANE

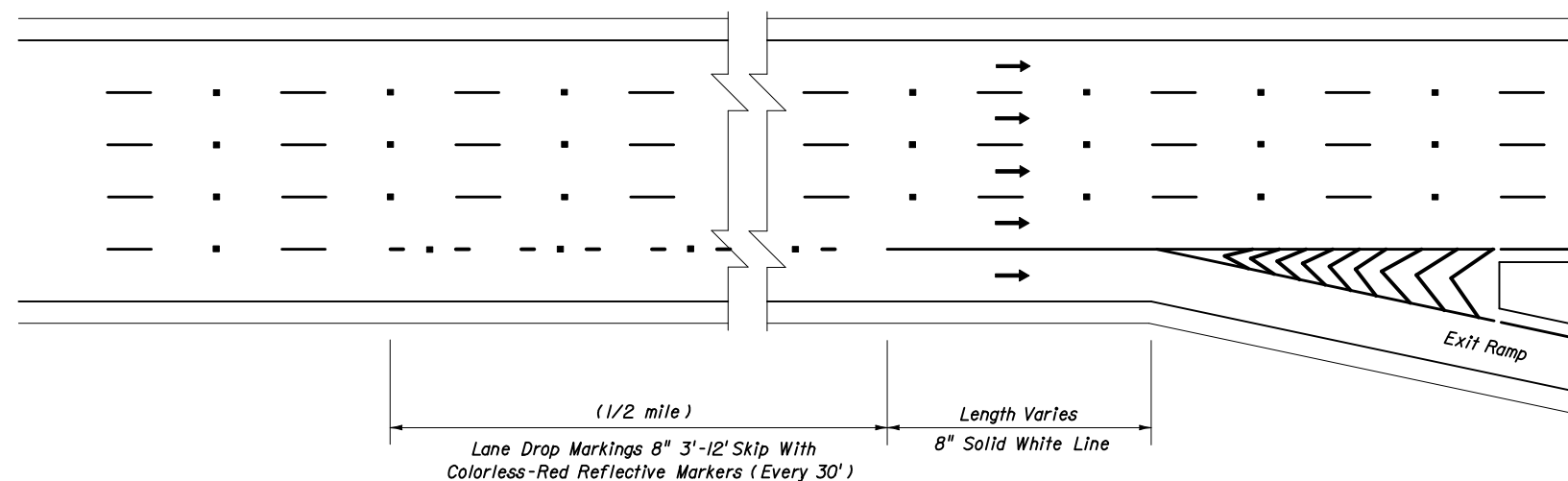
STATE OF FLORIDA DEPARTMENT OF TRANSPORTATION

INTERCHANGE MARKINGS

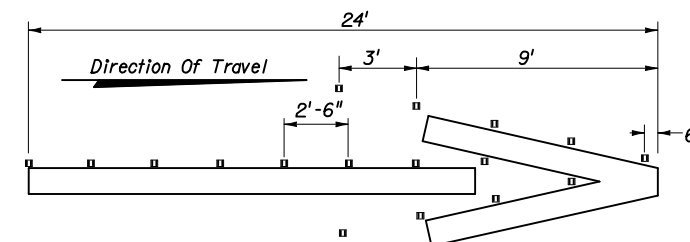
Names	Dates	Approved By		
Designed By	7-73	 State Traffic Standards Engineer		
Drawn By		Revision	Sheet No.	Index No.
Checked By	7-73	00	2 of 4	17345



PARALLEL ACCELERATION AND DECELERATION LANE



TYPICAL LANE DROP MARKINGS AT EXIT RAMP



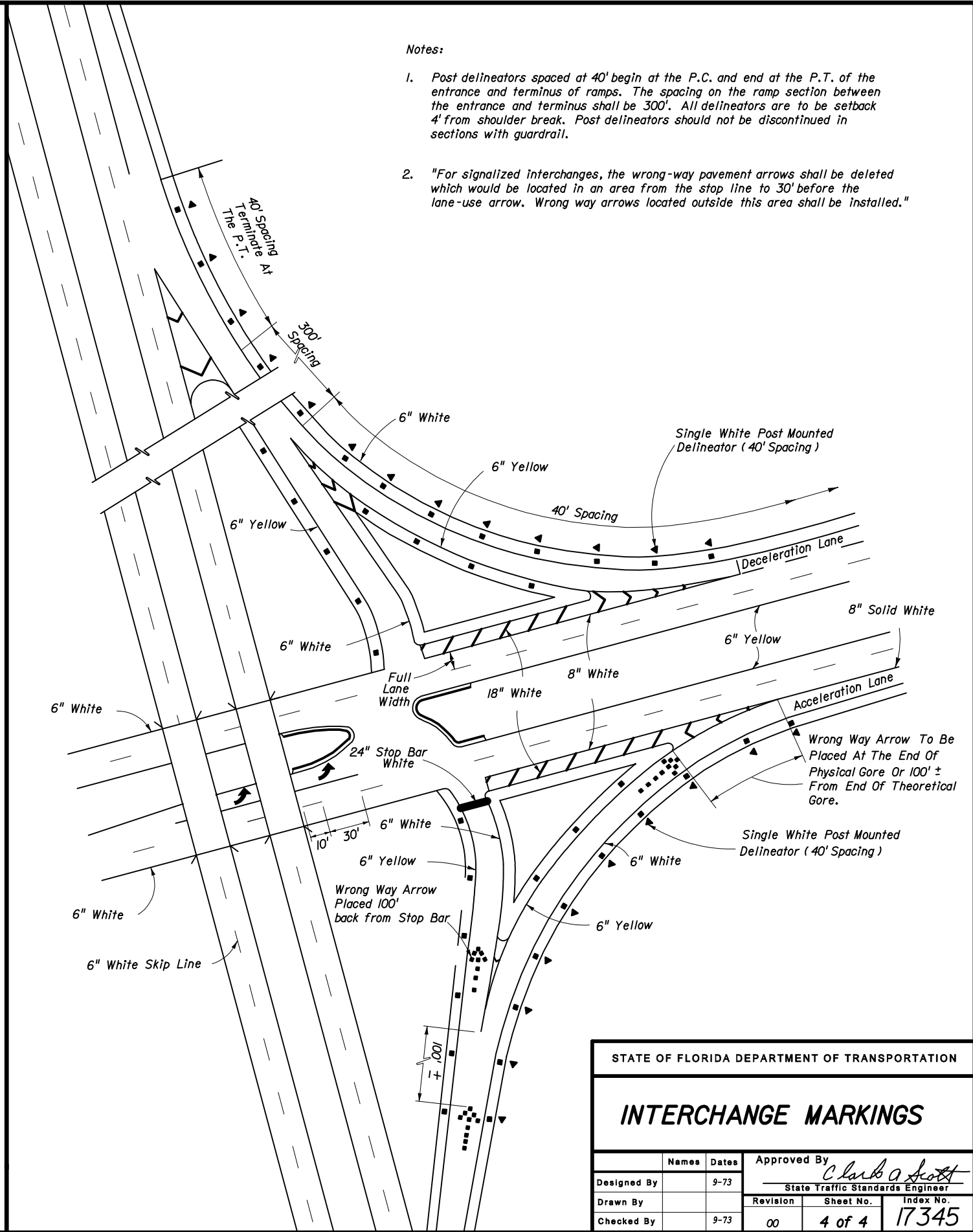
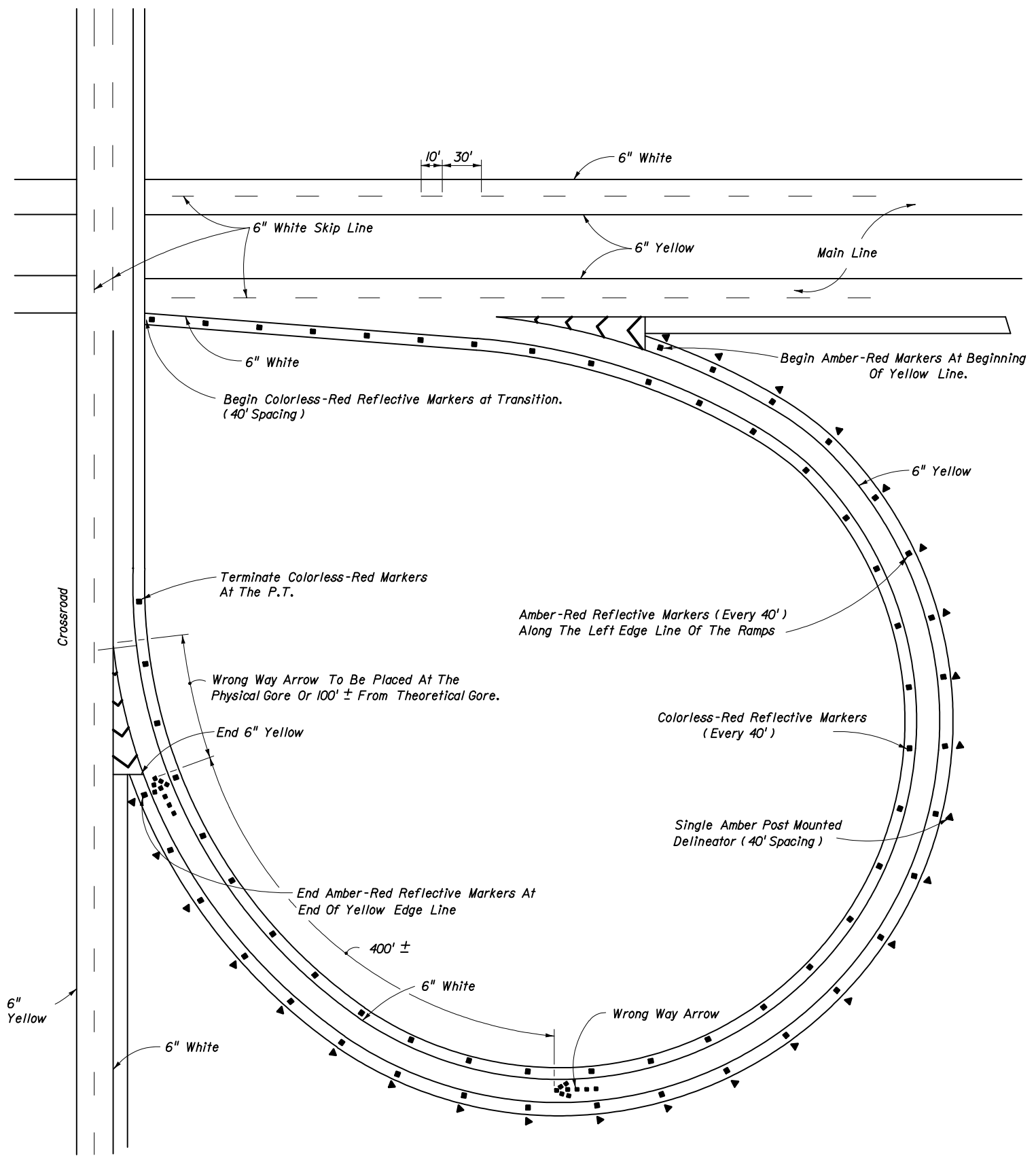
White Arrow With Colorless-Red Reflective Markers For Arrow details see Index No. 17346 sheet 1 of 13.

WRONG WAY ARROW

STATE OF FLORIDA DEPARTMENT OF TRANSPORTATION

INTERCHANGE MARKINGS

Designed By		Dates	9-73	Approved By	<i>Charles A. Scott</i>
Drawn By		Revision		State Traffic Standards Engineer	
Checked By		9-73	02	Sheet No.	3 of 4
				Index No.	17345




Notes:

1. Post delineators spaced at 40' begin at the P.C. and end at the P.T. of the entrance and terminus of ramps. The spacing on the ramp section between the entrance and terminus shall be 300'. All delineators are to be setback 4' from shoulder break. Post delineators should not be discontinued in sections with guardrail.
2. "For signalized interchanges, the wrong-way pavement arrows shall be deleted which would be located in an area from the stop line to 30' before the lane-use arrow. Wrong way arrows located outside this area shall be installed."

STATE OF FLORIDA DEPARTMENT OF TRANSPORTATION

INTERCHANGE MARKINGS

Names		Dates	Approved By		
Designed By		9-73	 State Traffic Standards Engineer		
Drawn By					
Checked By		9-73	Revision	Sheet No.	Index No.
			00	4 of 4	17345