

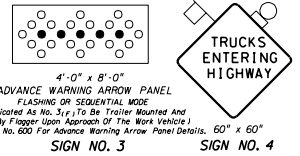
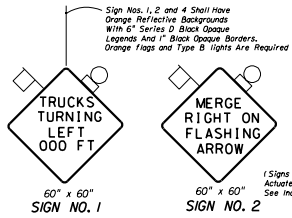
- SYMBOLS**
- Work Area
 - Type I Or Type II Barricade Or Vertical Panel Or Drum (With Steady Burning Light At Night Only). (Tubular Markers May Be Used During Daylight Only. Cones May Be Used - See Index 600).
 - Type I Or Type II Barricade Or Vertical Panel Or Cone Or Tubular Marker Or Drum
 - Work Zone Sign
 - Advance Warning Arrow Panel
 - Work Vehicle
 - Lane Number

TRAFFIC TRANSITION AREA UPSTREAM FROM CROSSOVER

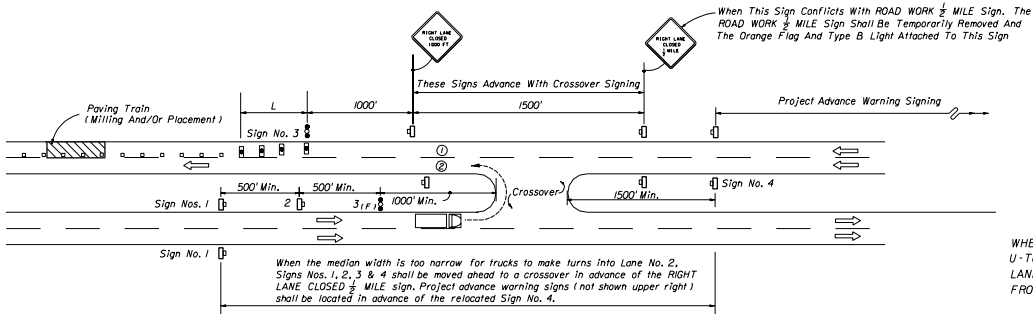
CASE I

GENERAL NOTES

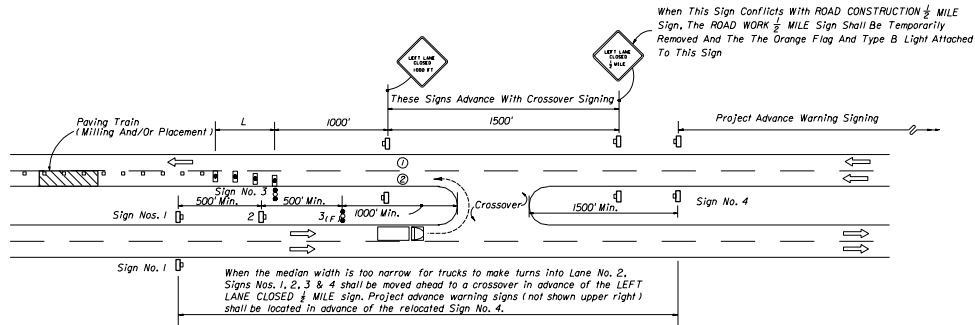
1. When crossovers do not exist, the contractor will construct temporary crossovers in accordance with Index No. 631.
2. L: Length of taper in feet:
 - = WS for speeds ≥ 45 mph
 - = $\frac{WS^2}{60}$ for speeds ≤ 40 mph
 Where:
 - W - Width of lateral transition in feet.
 - S - Posted speed limit (mph).
3. Within the lateral transitions, the maximum spacing between cones and tubular markers shall be 25'. Maximum spacing between Type I or Type II barricades or vertical panels or drums shall be based on the speed limit as follows: 15' up to 25 MPH; 30' for 30-40 MPH; 50' for 45 MPH or greater. Spacing for devices parallel to the travel lanes shall be 25' centers for cones or tubular markers and 50' for Type I or Type II barricades or vertical panels or drums.
4. Arrows denote direction of traffic only and do not reflect pavement markings.



STATE OF FLORIDA DEPARTMENT OF TRANSPORTATION			
ROAD DESIGN			
TRAFFIC CONTROL THROUGH WORK ZONES			
CROSSOVER FOR PAVING			
TRAIN OPERATIONS • RURAL			
Designed By	Checked	Date	Approved By
Drawn By	Checked	Date	1 of 2
Checked By	00		630



CONDITION A



CONDITION B

TRAFFIC TRANSITION AREA DOWNSTREAM FROM CROSSOVER

CASE II

Note: See Sheet I of 2 For General Notes, Sign No. Details, And Conditions.

CONDITION A
 WHEN THE PAVING TRAIN IS IN LANE ① THE U-TURNING VEHICLE SHALL CAUTIOUSLY TURN INTO LANE ② AND PROCEED IN LANE ② TO THE FRONT OF THE TRAIN

CONDITION B
 WHEN THE PAVING TRAIN IS IN LANE ② THE U-TURNING VEHICLE SHALL TURN INTO LANE ②, CAUTIOUSLY MERGE INTO LANE ① AND PROCEED TO THE FRONT OF THE PAVING TRAIN

CONDITION A & B
 THE ADVANCE WARNING ARROW PANEL IS REQUIRED, UNDER NO CIRCUMSTANCES WILL THE TRAFFIC TRANSITION BE LOCATED WITHIN THE LIMITS OF THE CROSSOVER

STATE OF FLORIDA DEPARTMENT OF TRANSPORTATION
 ROAD DESIGN

TRAFFIC CONTROL THROUGH WORK ZONES
**CROSSOVER FOR PAVING
 TRAIN OPERATIONS • RURAL**

DESIGNED BY	DATE	APPROVED BY
SWP		<i>James M. Mill</i>
DRAWN BY	DATE	WORK NO.
SWP	00	2 of 2
CHECKED BY	DATE	WORK NO.
SWP	00	2 of 2

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