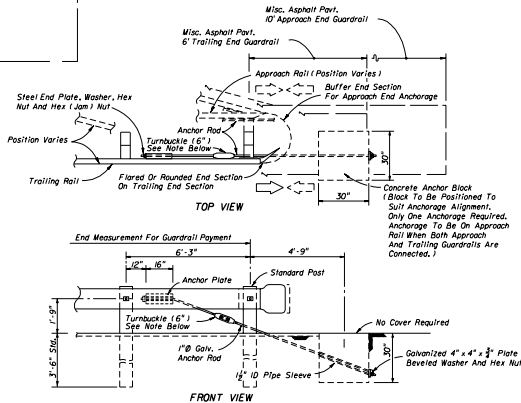


FRONT VIEW

The payment for the items of End Anchorage Assembly Type II (Cable Option) shall be full compensation for furnishing and installing either the Round or the Buffer End Section, the Beam Anchor Plate, Cable Assembly, Pipe Sleeve, Soil Plate, Steel Tube, Bearing Plate, Short Timber Breakaway Post, Offset Blocks and the necessary hardware.

CABLE ANCHOR OPTION



FRONT VIEW

Turnbuckle to be used only for guardrail that is reset vertically. The existing anchor rod (1" or 1 1/2" dia.) shall be field cut, threaded 4" on each end, and metalized in accordance with Sections 562 and 911 of the Standard Specifications. The cost for cutting, threading, metalizing and the turnbuckle shall be included in the contract unit price for Reset Guardrail, LF.

The payment for the items of End Anchorage Assembly Type II (Concrete Anchor Block Option) shall be full compensation for furnishing and installing the Beam Anchor Plate, Anchor Rod, Pipe Sleeve, Anchor Block, either Flared, Rounded or Buffer End Section, and the necessary hardware.

CONCRETE ANCHOR BLOCK OPTION

TYPE II NOTES

1. Unless specified in the plans, the contractor can supply either the cable anchor option or the concrete anchor block option.
2. Type II end anchorage assemblies are approved for all speeds and are intended for use as:
  - (a) trailing and anchorages for single face free standing guardrail systems;
  - (b) approach and anchorages for single face free standing guardrail systems when end anchorage is located outside of the clear zone; and,
  - (c) both approach and trailing ends of double face guardrail systems.
 Crash cushions shall be constructed at or in lieu of approach Type II end anchorages located inside the clear zone.
3. These end anchors are to be paid for under the contract unit price for Guardrail, End Anchorage Assembly (Type II) EA as called for in the plans or by permit.

END ANCHORAGE ASSEMBLY TYPE II

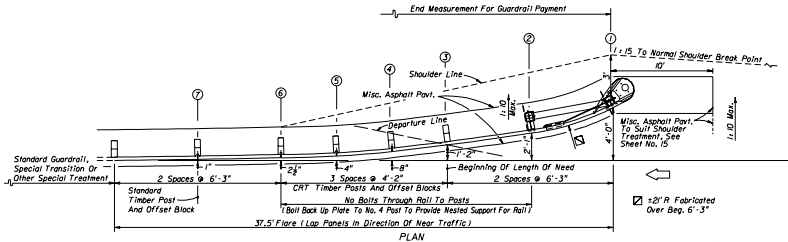
STATE OF FLORIDA DEPARTMENT OF TRANSPORTATION  
ROAD DESIGN

GUARDRAIL

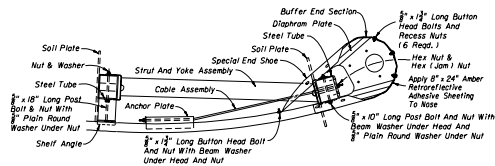
Revised	Date	Approved By
Designed By	08/08	08/08
Drawn By	08/08	08/08
Checked By	08/08	08/08
		22 of 31
		400

MODIFIED ECCENTRIC LOADER TERMINAL NOTES

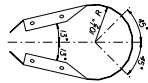
1. The MELT is applicable for design speeds up to 45 mph. The MELT is intended for use as an approach end guardrail anchorage for shoulder guardrail. Its alignment is a flare from the normal guardrail alignment with an effective length of 37.5' including three standard W-beam panel outside of any standard guardrail, guardrail transitions or other special treatments.
2. This standard drawing is produced by the Florida Department of Transportation solely for use by the Department and its assignees. This standard drawing provides the general graphics and information necessary to field identify component parts of the MELT and their incorporation into a whole system.
3. This standard drawing is sufficient for plan details for the MELT when installed in connection with shoulder guardrail and precludes the requirement for shop drawings unless the plans otherwise call for such submittals. The MELT shall be assembled in accordance with the distributor's detailed drawings, procedures and specifications.
4. The first two post must be short timber breakaway posts with steel foundation tubes and soil plates, post Nos. 3 thru 6 must be CRT timber posts and post No. 7 must be a standard timber post.
5. The MELT can not be used in medians where horizontal clearance requires the use of a backroll.
6. See the General Notes for galvanizing requirements of metallic components.
7. If the plans call for the MELT at a specific location, substitutions with other end anchorage assemblies will not be permitted unless approved by the Engineer. If the plans call for end anchorage assembly Flared at a specific location, the contractor has the option to construct any DOT approved flared assembly that meet the applications for that location. Where a flared end anchorage is called for in the plans, any approved substitution with a parallel end anchorage will not be eligible for VEP consideration.
8. The MELT shall be paid for under the contract unit price for Guardrail, End Anchorage Assembly (Flared), EA and shall be full compensation for furnishing and installing all components in accordance with the plans, the distributor's detailed drawings, procedures and specifications and this index.



MODIFIED ECCENTRIC LOADER TERMINAL (MELT)



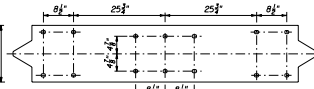
TOP VIEW



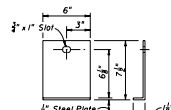
PLAN



ELEVATION

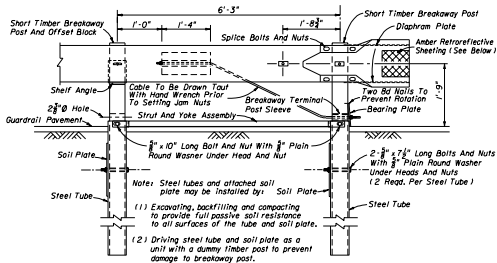


FLAT PLATE LAYOUT

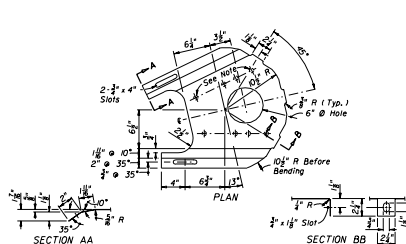


SHELF ANGLE

ALL SLOTS SHALL BE 1/2" x 1 1/2" BUFFERED END SECTION

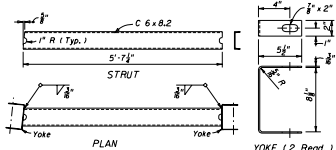


FRONT VIEW



Notes: Bolt holes are not required, but, diaphragms with either manufacturer produced two or three hole in line patterns are acceptable.

DIAPHRAGM PLATE (2 Req.)



Note: Assembly installed with channel turned down for right side guardrail and turned up for left side guardrail.

STEEL STRUT AND YOKE ASSEMBLY

STATE OF FLORIDA DEPARTMENT OF TRANSPORTATION  
ROAD DESIGN

GUARDRAIL

Drawn By	Checked By	Date	Approved By
MMW	JAG	3/95	GO
Drawn By	Checked By	Date	Approved By
MMW	JAG	3/95	GO

CONTROLLED RELEASE RETURN NOTES

1. Controlled release returns are intended for use (a) in openings in continuous guardrail for driveway and side road access when flares and transitions or standard radial returns can not be applied (Sheet II); and (b) for shielding the ends of bridge traffic rails and barrier walls where the driveway and side road access is in close proximity to the structure and space does not permit the proper use of Guardrail End Anchorage Assemblies Type II, MELT, SRT-350, ET-2000, BEST, LET, SKT-350, FLEAT-350 or REGEN<sup>®</sup>.
  2. Controlled release returns are not intended as a substitute or replacement for the appropriate use of approved vehicle impact attenuators.
  3. Controlled release returns with either 8', 16' or 24' radii are designed for highway speeds of 60 mph or less; the 32' radius return is to be used only for highway speeds of 45 mph or less.
  4. The controlled release returns shown are designed as full returns based on an intersection angle of 90°. The return can be terminated with the Guardrail End Anchorage Assembly Type CRT or connected to standard guardrail as shown or as otherwise detailed in the plans.
  5. The Guardrail End Anchorage Assembly Type CRT is to be used only for the controlled release returns with 8', 16', 24' and 32' radii as shown; the assembly is not to be used in any tangent rail or flared rail applications. Other types of end anchorage assemblies are not to be used in the controlled release returns.
  6. The area immediately behind the controlled release return shall have slopes not steeper than 1:2 and be maintained free of fixed objects in accordance with the area limits tabulated in the plan below.
  7. The surface approaching the controlled release return shall have a transverse slope not exceeding 1:10. The effective width of the transverse surface is to be based on standard vehicle departure, return radii and preceding shielding; the width (beyond shoulder) shall not be greater than the corresponding R<sup>2</sup> and 20' W values tabulated below.
  8. The curved guardrail portion of the controlled release return shall be full section shop bent panels (12.5' or 25' panels).
  9. Washers are not to be used between the guardrail beam and the head of the button head post bolts at any controlled release terminal (CRT) post or at any Guardrail End Anchorage Assembly Type CRT breakaway timber post.
  10. The guardrail beam of the 8' radius return is not bolted to the center control release post.
- ii. See the General Notes for galvanizing requirements of metallic components.
12. Controlled release return systems shall be paid for under the contract unit prices for Guardrail (Roadway), LF, Guardrail (Shop-Bent Panels), LF, and Guardrail, End Anchorage Assembly (Type CRT), EA as called for in the plans or by permit and shall be itemized for furnishing and installing all components in accordance with the plans and with this index. CRT posts are included in the cost for guardrail.

Note: To be constructed when flares and transitions or standard radial returns can not be applied. See Sheet II.

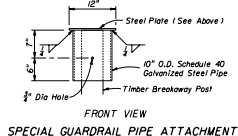
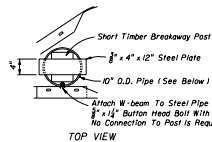
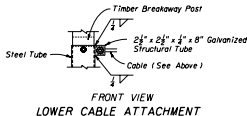
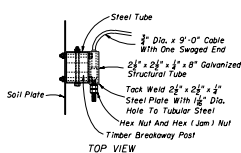
STATE OF FLORIDA DEPARTMENT OF TRANSPORTATION  
ROAD DESIGN

GUARDRAIL

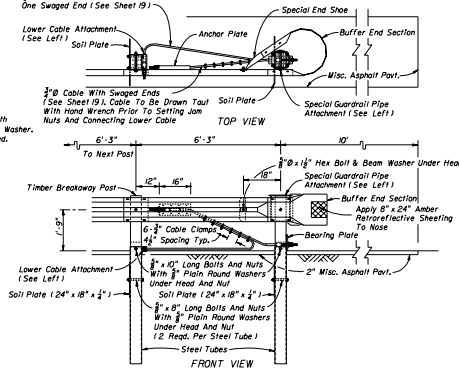
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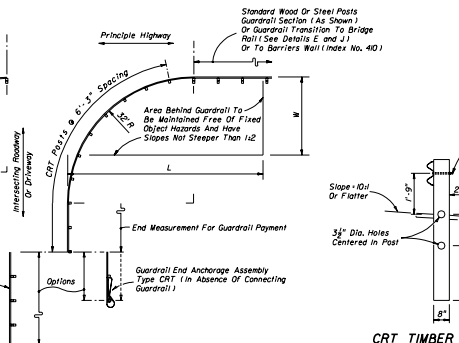
3/4" Dia. x 9'-0" Cable With One Swaged End (See Sheet I9)



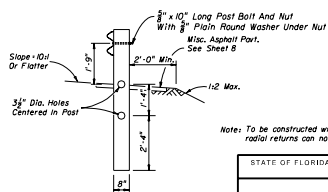
GUARDRAIL END ANCHORAGE ASSEMBLY TYPE CRT



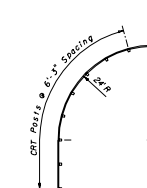
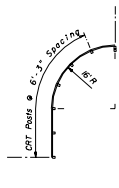
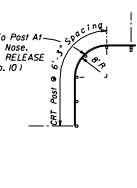
GUARDRAIL END ANCHORAGE ASSEMBLY TYPE CRT



CRT TIMBER POST

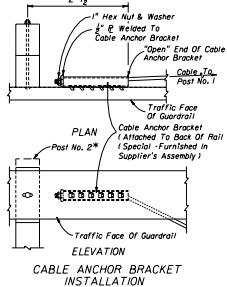
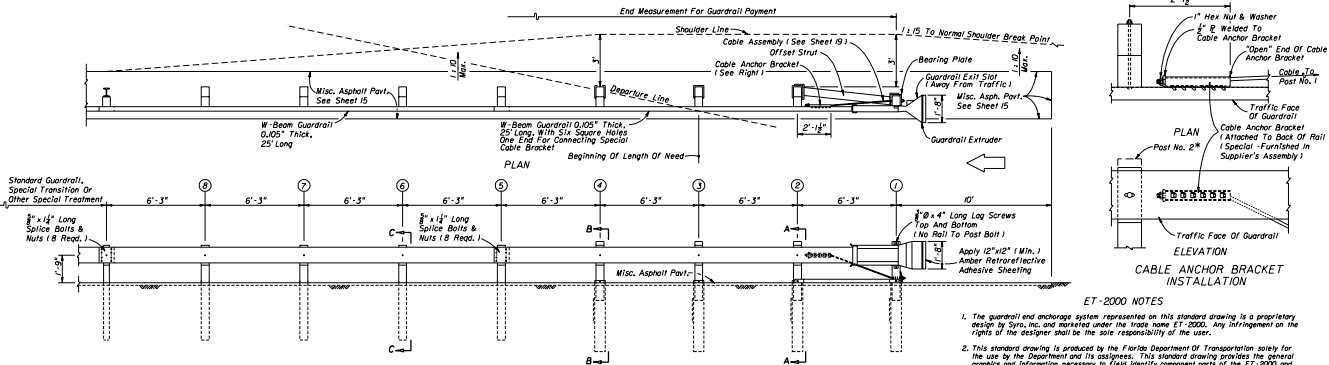


Do NOT Bolt Rail To Post At the Center Of The Nose. (See "CONTROLLED RELEASE RETURN NOTES" No. 10)



Return Nom. R	Length Of Shop Bent Panels	No. Of CRT Posts	Required Area Free Of Hazards L W
8'	12.5'	5	25' x 15'
16'	25'	6	30' x 15'
24'	37.5'	8	40' x 20'
32'	50'	11	50' x 20'

CONTROLLED RELEASE RETURN FOR SIDE ROAD AND DRIVEWAY ACCESS

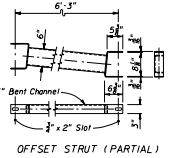
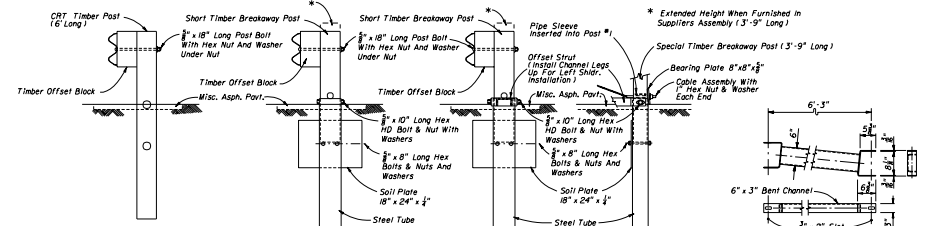


**ET-2000 NOTES**

- The guardrail end anchorage system represented on this standard drawing is a proprietary design by Syra, Inc. and marketed under the trade name ET-2000. Any infringement on the rights of the designer shall be the sole responsibility of the user.
- This standard drawing is produced by the Florida Department of Transportation solely for the use by the Department and its assignees. This standard drawing provides the general graphics and information necessary to field identify component parts of the ET-2000 and their incorporation into a whole system.
- This standard drawing is sufficient for plan details for the ET-2000 when installed in connection with shoulder guardrail and precludes the requirement for shop drawing submittals unless the plans otherwise call for such submittals. The ET-2000 shall be assembled in accordance with the manufacturer's detailed drawings, procedures and specifications.
- The ET-2000 is intended for use as an approach end guardrail anchorage for shoulder guardrail located parallel to travel or auxiliary lanes. The effective length of the ET-2000 is 50' including two 25' W-beam spans of guardrail. The effective length is outside of any standard guardrail, guardrail transitions or other special treatments. The ET-2000 alignment is an extension of the normal guardrail alignment, except when constructed with curb alignment of the ET-2000 will be forces over the first 25' of a rate of 1:25.
- The ET-2000 can not be used in medians where horizontal clearance requires the use of a beacon.
- Posts at location Nos. 1, 2, 3 and 4 must be timber breakaway posts with steel foundation tubes. The breakaway posts of location Nos. 5, 6, 7 and 8 may be constructed as shown in Section CC or may utilize timber breakaway posts with steel foundation tubes as shown in Section BB.
- See the General Notes for galvanizing requirements of metallic component.
- If the plans call for the ET-2000 of a specific location, substitutions with other end anchorage assemblies will not be permitted unless approved by the Engineer. If the plans call for an anchorage assembly parallel or a specific location the contractor has the option to construct any FDOT approved parallel assembly. Where a flare end anchorage is called for in the plans, any approved substitution with a parallel end anchorage will not be eligible for VEDC consideration.
- The ET-2000 shall be paid for under the contract unit price for Guardrail, End Anchorage Assembly (Parallel), EA and shall be full compensation for furnishing and installing all components in accordance with the plans, the manufacturer's detail drawings, procedures and specifications and this index.

Standard Guardrail, Special Transition Or Other Special Treatment

Do Not Attach Rail To Block At Post No. 5 And Roll To Post At Post No. 1



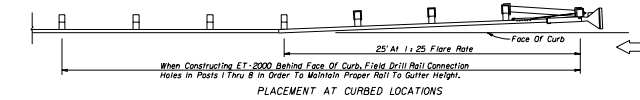
Note: Post No. 5 is similar to Section CC except rail is not attached to post and block.

SECTION CC TYPICAL POST NOS. 6, 7 AND 8

SECTION BB TYPICAL POST NOS. 3 AND 4

SECTION AA POST NO. 2

PARTIAL VIEW POST NO. 1



**DESIGN NOTES**

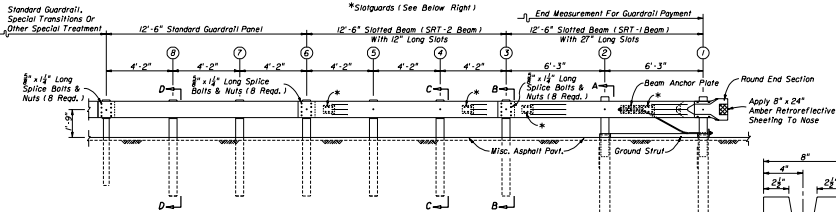
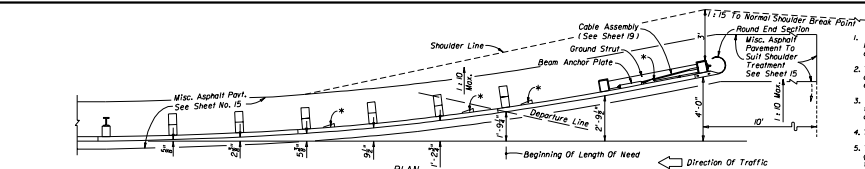
- A special site evaluation should be considered prior to using the ET-2000 where there is less than 25' clear area on the extrusion side (back side) of the ET-2000.
- The ET-2000 is suitable for all design speeds.

**END ANCHORAGE ASSEMBLY TYPE ET-2000**

STATE OF FLORIDA DEPARTMENT OF TRANSPORTATION  
ROAD DESIGN

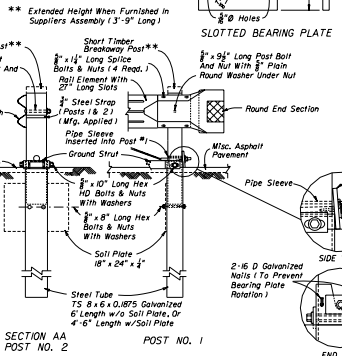
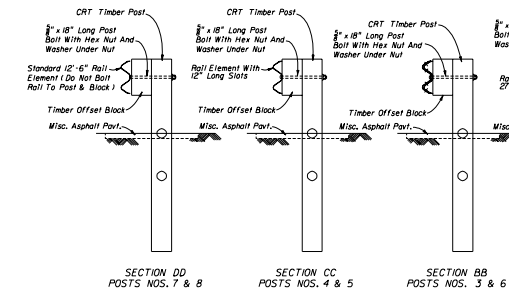
**GUARDRAIL**

Revised	Date	Approved By
DESIGNED BY	07/95	STATE HIGHWAY DESIGN DIVISION
DRAWN BY	08/95	STATE HIGHWAY DESIGN DIVISION
CHECKED BY	08/95	00



Note: Attach rail to post at posts Nos. 1 thru 6.

ELEVATION



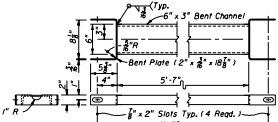
BEARING PLATE ORIENTATION

SRT-350 NOTES

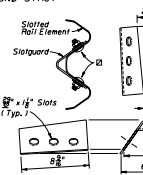
- The guardrail end anchorage system represented on this drawing is a proprietary right (B) post design by Trinity Industries, Inc. and marketed by Syro, Inc. under the trade name SRT-350, short for Slotted Rail Terminal. Any infringement on the rights of the designer shall be the sole responsibility of the user.
- This standard drawing is produced by the Florida Department Of Transportation solely for use by the Department and its agencies. This standard drawing provides the general graphics and information necessary to field identify component parts of the SRT-350 and their incorporation into a whole system.
- This drawing is sufficient for plan details for the SRT-350 when installed in connection with shoulder guardrail and precludes the requirement for shop drawing submittals unless called for elsewhere in the plans. The SRT-350 shall be assembled in accordance with the manufacturer's detailed drawings, procedures and specifications.
- The SRT-350 can not be used in medians where horizontal clearance requires the use of a backrail.
- The SRT-350 is suitable for all design speeds. The SRT-350 is intended for use as an approach end anchorage for shoulder guardrail, its alignment is a parabolic flare from the normal guardrail alignment with an effective length of 31.2' including two special slotted W-Beam panels and one standard W-Beam panel outside of any standard guardrail, guardrail transitions or other special treatments with the manufacturer's detailed drawings, procedures and specifications.
- Posts 1 and 2 must be timber breakaway posts each with a 3/8" steel strap located approximately 1" below the post bolt and a steel foundation base. CRT breakaway posts shall be used at all other locations within the system.
- See the General Notes for galvanizing requirements of metallic component.
- If the plans call for the SRT-350 at a specific location, submittals with other end anchorage assemblies will not be permitted unless approved by the Engineer. If the plans call for end anchorage assembly (Flared) at a specific location, the contractor has the option to construct any FDOT approved flared assembly. Where a flared end anchorage is called for in the plans, any other special treatments with the manufacturer's detailed drawings, procedures and specifications.
- The SRT-350 shall be paid for under the contract unit price for Guardrail, End Anchorage Assembly (Flared), EA and shall be full compensation for purchasing and installing all components in accordance with the plans; the manufacturer's detail drawings, procedures and specifications and this index.

SLOTTED BEAM NOTES

The 12'-6" SRT-1 and SRT-2 beams can be used for the repair or replacement of slotted rails in existing 9 or 10 post SRT-350 systems.



GROUND STRUT



Assemble With 3/8" x 1/2" Long Oval Shoulder Burdon Head Bolts And Nuts With Plain Round Washers Under Nuts (6 Reqd.).

SLOT GUARD

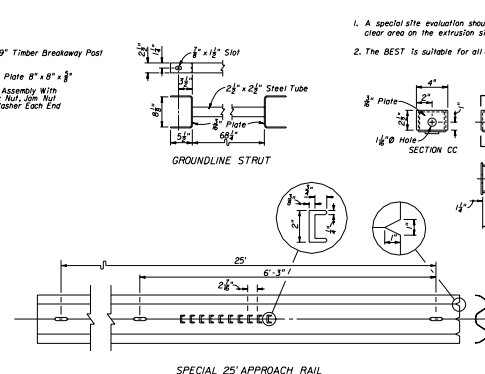
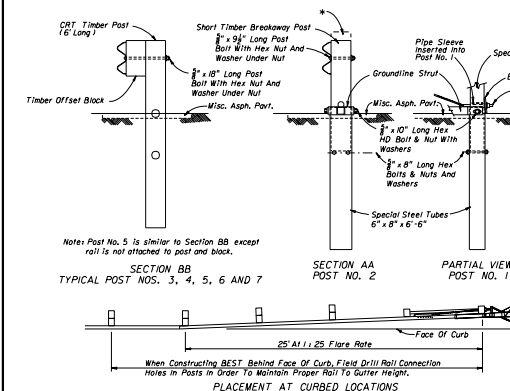
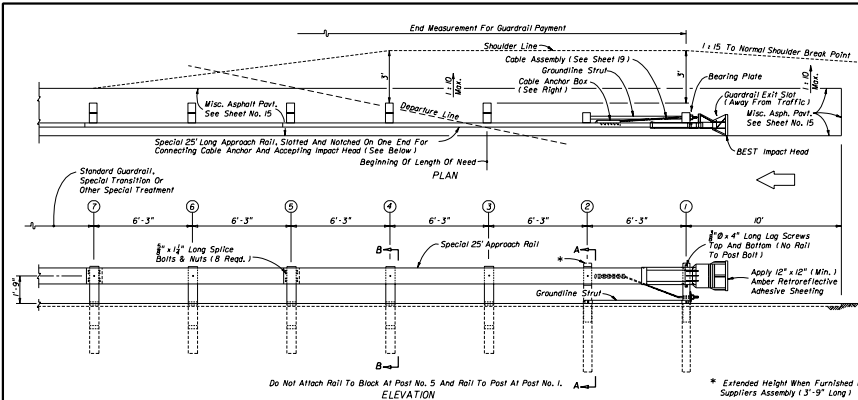
STATE OF FLORIDA DEPARTMENT OF TRANSPORTATION  
ROAD DESIGN

GUARDRAIL

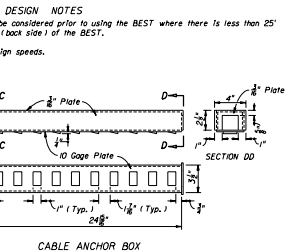
Revised	Date	Approved By
DESIGNED BY	2/26	STATE OF FLORIDA DEPARTMENT OF TRANSPORTATION
DRAWN BY	MMW	SCALE
CHECKED BY	JAG	DATE

26 OF 31

END ANCHORAGE ASSEMBLY TYPE SRT-350 (8 POST SYSTEM)

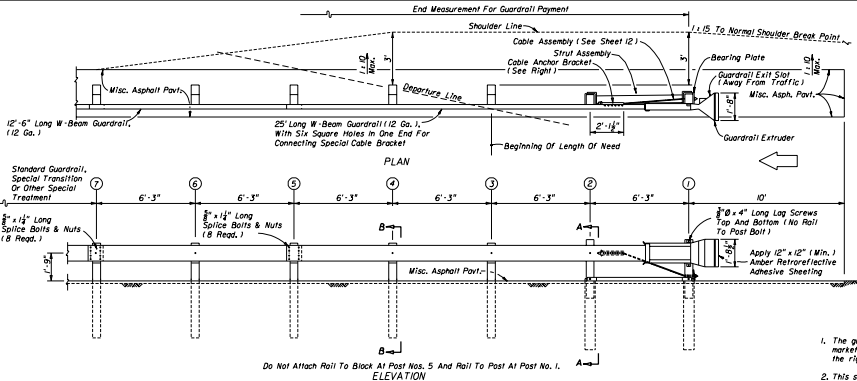


- 'BEST' NOTES**
- The guardrail end anchorage system represented on this standard drawing is a proprietary design by Interstate Steel Corporation and marketed under the trade name BEST. Any infringement on the rights of the designer shall be the sole responsibility of the user.
  - This standard drawing is produced by the Florida Department of Transportation solely for the use by the Department and its assignees. This standard drawing provides the general graphics and information necessary to field identify component parts of the BEST and their incorporation into a whole system.
  - This standard drawing is sufficient for plan details for the BEST when installed in connection with shoulder guardrail and precludes the requirement for shop drawing submittals unless the plans otherwise call for such submittals. The BEST shall be assembled in accordance with the manufacturer's detailed drawings, procedures and specifications.
  - The BEST is intended for use as an approach end guardrail anchorage for shoulder guardrail located parallel to travel or auxiliary lanes. The effective length of the BEST is 37'-5" including a 25' special W-Beam panel plus one 12.5' standard W-Beam panel outside of any other standard guardrail, guardrail transitions or other special treatments. The alignment of the BEST is an extension of the normal guardrail alignment, except when constructed with curb the alignment of the BEST will be flared over the first 25' at a rate of 1:25.
  - The BEST can not be used in medians where horizontal clearance requires the use of a backroll.
  - Posts at location Nos. 1 and 2 must be timber breakaway posts with special length steel foundation tubes without soil plates. The posts at location Nos. 3, 4, 5, 6 and 7 shall be CRT timber posts.
  - See the General Notes for galvanizing requirements of metallic components.
  - If the plans call for the 'BEST' of a specific location, substitutions with other end anchorage assemblies will not be permitted unless approved by the Engineer. If the plans call for end anchorage assembly 'parallel' at a specific location, the contractor has the option to construct any DOT approved parallel assembly. Where a flared end anchorage is called for in the plans, any approved substitution with a parallel end anchor will not be eligible for VECP consideration.
  - The BEST shall be paid for under the contract unit price for Guardrail, End Anchorage Assembly (Parallel), EA and shall be full compensation for furnishing and installing all components in accordance with the plans; the manufacturer's detail drawings, procedures and specifications and this Index.

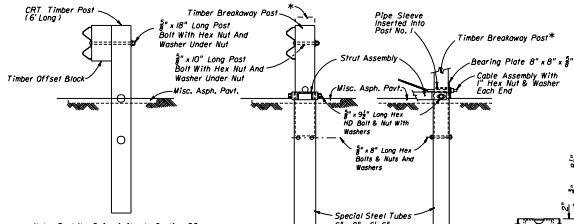


**END ANCHORAGE ASSEMBLY TYPE BEST**

STATE OF FLORIDA DEPARTMENT OF TRANSPORTATION ROAD DESIGN			
<b>GUARDRAIL</b>			
DESIGNED BY	DATE	APPROVED BY	
DRW	8/95	[Signature]	
DRAWN BY	REV	SCALE	REVISION
JAG	8/95	00	
CHECKED BY			
			27 of 31
			400



Do Not Attach Rail To Block At Post Nos. 5 And Rail To Post At Post No. 1.



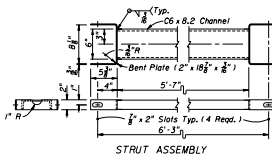
Note: Post No. 5 is similar to Section BB except rail is not attached to post and block.

SECTION BB  
TYPICAL POST NOS. 3, 4, 5, 6 AND 7

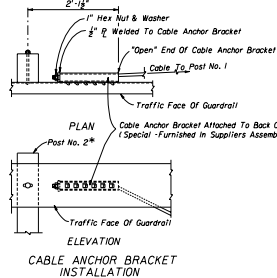
SECTION AA  
POST NO. 2

PARTIAL VIEW  
POST NO. 1

\* Extended Height When Furnished in Suppliers Assembly (3'-9" Long)

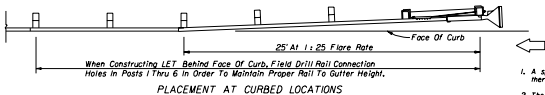


STRUT ASSEMBLY



LET NOTES

- The guardrail end anchorage system represented on this standard drawing is a proprietary design by Syro, Inc. and marketed under the trade name ET-2000 LET hereafter referred to and identified as LET. Any infringement on the rights of the designer shall be the sole responsibility of the user.
- This standard drawing is produced by the Florida Department of Transportation solely for use by the Department and its assignees. This standard drawing provides the general graphics and information necessary to field identify component parts of the LET and their incorporation into a whole system.
- This standard drawing is sufficient for plan details for the LET when installed in connection with shoulder guardrail and precludes the requirement for shop drawing submittals unless the plans otherwise call for such submittals. The LET shall be assembled in accordance with the manufacturer's detailed drawings, procedures and specifications.
- The LET is intended for use as an approach end guardrail anchorage for shoulder guardrail located parallel to travel or auxiliary lanes. The effective length of the LET is 37'-5" including one 25' special W-Beam panel and one (2-5' standard W-Beam panel). The effective length is outside of any other standard guardrail, guardrail transitions or other special treatments. The LET alignment is an extension of the normal guardrail alignment, except when constructed with curb the alignment of the LET will be flared over the first 25' at a rate of 1:25.
- The LET can not be used in medians where horizontal clearance requires the use of a backroll.
- Posts at location Nos. 1 and 2 must be timber breakout posts with special length steel foundation tubes without soil plates. Posts at location Nos. 3, 4, 5, 6 and 7 must be CRT timber posts.
- See the General Notes for galvanizing requirements of metallic component.
- If the plans call for the 'LET' at a specific location, substitutions with other end anchorage assemblies parallel to a specific location, the contractor has the option to construct any FDOT approved parallel assembly. Where a flared end anchorage is called for in the plans, any approved substitution with a parallel end anchor will not be eligible for VDOT consideration.
- The LET shall be paid for under the contract unit price for Guardrail, End Anchorage Assembly (Parallel), EA and shall be full compensation for furnishing and installing all components in accordance with the plans, the manufacturer's detailed drawings, procedures and specifications and this index.



PLACEMENT AT CURBED LOCATIONS

DESIGN NOTES

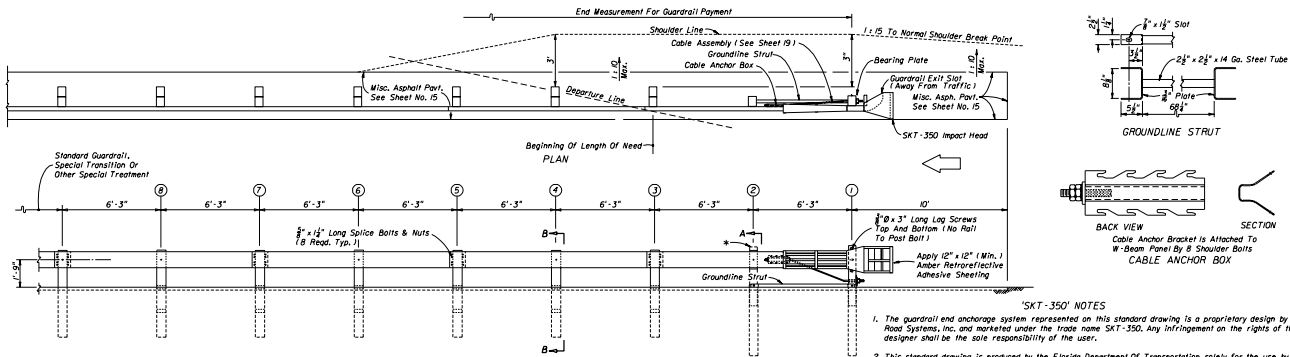
- A special site evaluation should be considered prior to using the LET where there is less than 25' clear area on the extrusion side (back side) of the LET.
- The LET is suitable for all design speeds.

END ANCHORAGE ASSEMBLY TYPE LET

STATE OF FLORIDA DEPARTMENT OF TRANSPORTATION  
ROAD DESIGN

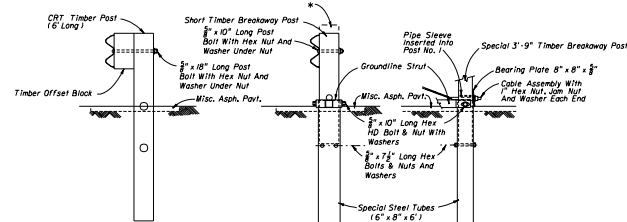
GUARDRAIL

DESIGNED BY	STAFF	DATE	APPROVED BY
Drawn By	JAN	09/97	GO
Checked By	JAN	09/97	GO
Sheet No. 28 of 31			400

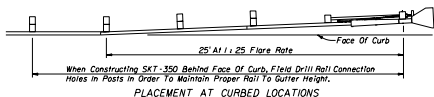


\* Extended Height When Furnished In Suppliers Assembly (3'-9" Long)

Do Not Attach Rail To Block At Post Locations 3, 5 And 7, And, Rail To Post At Post No. 1.



Note: Do not attach rail to block at post locations 3, 5 and 7.



#### DESIGN NOTES

- A special site evaluation should be considered prior to using the SKT-350 where there is less than 25' clear area on the extrusion side (back side) of the SKT-350.
- The SKT-350 is suitable for all design speeds.

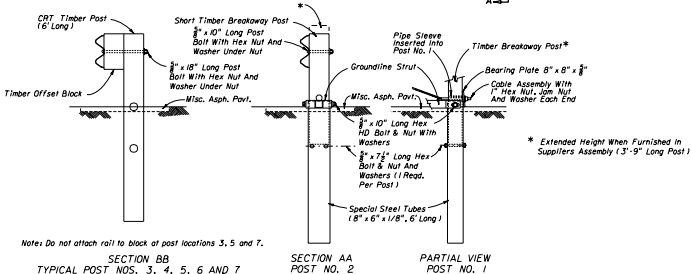
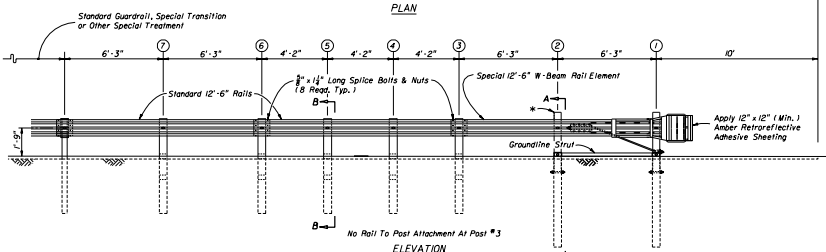
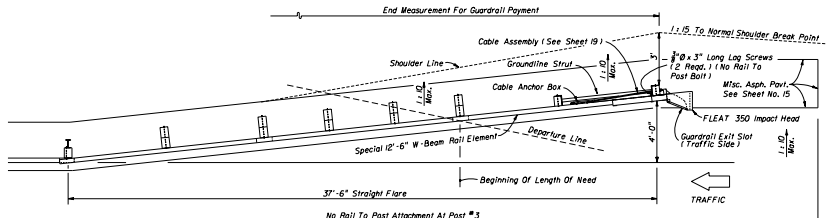
### END ANCHORAGE ASSEMBLY TYPE SKT-350

STATE OF FLORIDA DEPARTMENT OF TRANSPORTATION  
ROAD DESIGN

#### GUARDRAIL

DESIGNED BY	DATE	APPROVED BY	DATE
AW	09/95	[Signature]	09/95
DRAWN BY	AW	CHECKED BY	AW
CHECKED BY	AW	DATE	29 of 31



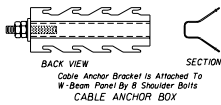
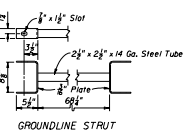


Notes Do not attach rail to block at post locations 3, 5 and 7.  
SECTION BB  
TYPICAL POST NOS. 3, 4, 5, 6 AND 7

SECTION AA  
POST NO. 2

PARTIAL VIEW  
POST NO. 1

END ANCHORAGE ASSEMBLY TYPE FLEAT-350



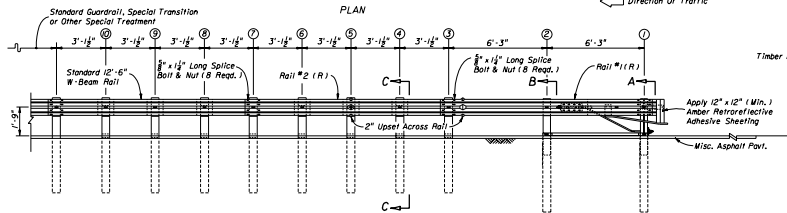
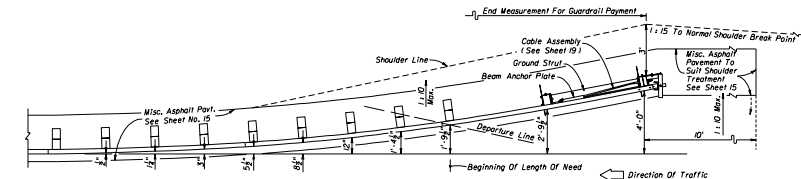
BACK VIEW SECTION  
Cable Anchor Bracket is Attached to W-Beam Panel by 8 Shoulder Bolts  
CABLE ANCHOR BOX

- The guardrail end anchorage system represented on this standard drawing is a proprietary design by Road Systems, Inc. and marketed under the trade name FLEAT-350. Any infringement on the rights of the designer shall be the sole responsibility of the user.
- This standard drawing is produced by the Florida Department Of Transportation solely for the use by the Department and its assignees. This standard drawing provides the general graphics and information necessary to field identify component parts of the FLEAT-350 and their incorporation into a whole system.
- This standard drawing is sufficient for plan details for the FLEAT-350 when installed in connection with shoulder guardrail and precludes the requirement for shop drawing submittals unless the plans otherwise call for such submittals. The FLEAT-350 shall be assembled in accordance with the manufacturer's detailed drawings, procedures and specifications.
- The FLEAT-350 is intended for use as an approach end guardrail anchorage for shoulder guardrail on one-way facilities and divided multi-lane facilities. The effective length of the FLEAT-350 is 37.5' including one 12.5' special W-Beam panel plus two 12.5' standard W-Beam panels outside of any other standard guardrail, guardrail transitions or other special treatments. The alignment of the FLEAT-350 is a straight flare with end offset of 4' from the normal guardrail alignment.
- The FLEAT-350 can not be used in medians where horizontal clearance requires the use of a backrail.
- Posts at location Nos. 1 and 2 must be timber breakaway posts with special length steel foundation tubes without soil plates. The posts at location Nos. 3, 4, 5, 6, and 7 shall be CRT Timber posts.
- See the General Notes for galvanizing requirements of metallic components.
- If the plans call for the 'FLEAT-350' at a specific location, substitution with other end anchorage assemblies will not be permitted unless approved by the Engineer. If the plans call for end anchorage assembly 'flared' at a specific location, the contractor has the option to construct any FDOT approved flared assembly. Where a flared end anchorage is called for in the plans, any approved substitution with a parallel end anchorage will not be eligible for VECP consideration.
- The FLEAT-350 shall be paid for under the contract unit price for Guardrail, End Anchorage Assembly (Flared), EA and shall be full compensation for furnishing and installing all components in accordance with the plans; the manufacturer's detail drawings, procedures and specifications and this index.

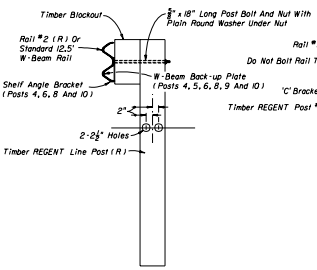
DESIGN NOTES

- The FLEAT-350 is suitable for all design speeds.

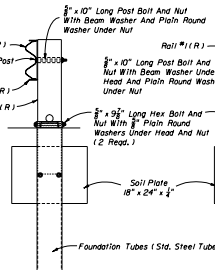
STATE OF FLORIDA DEPARTMENT OF TRANSPORTATION ROAD DESIGN			
GUARDRAIL			
Revised	Date	Approved By	
DES	07/98	[Signature]	
Drawn by	DES	Checked by	DES
Checked by	JMS	DATE	00
		30 of 31	400



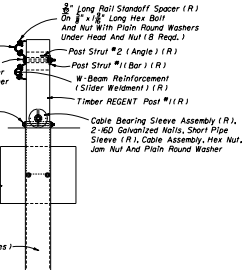
Note: Attach rail to post at posts Nos. 1 and 5 only. W-Beam back-up plates are to be installed at posts Nos. 4, 5, 6, 8, 9 and 10 only. Shelf angle brackets are to be installed at posts Nos. 4, 6, 8 and 10 only.



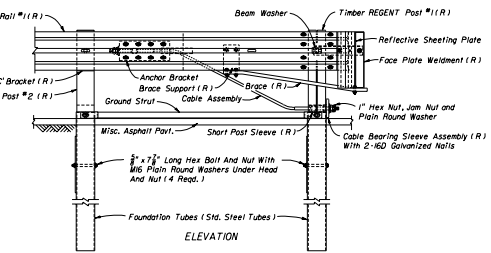
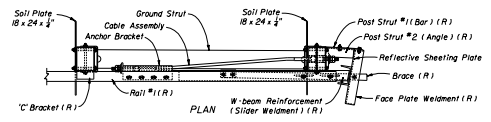
Note: Attach Rail To Post At Post No. 5 Only  
SECTION CC  
TYPICAL POST NOS. 3 THRU 10



SECTION BB  
POST NO. 2



SECTION AA  
POST NO. 1



'REGENT' NOTES

- The REGENT is suitable for all design speeds. The REGENT is intended for use as an approach end guardrail anchorage for shoulder guardrail. Its alignment is a parabolic flare from the normal guardrail alignment with an effective length of 37.5' including two special W-beam panels and one standard W-beam panel outside of any standard guardrail, guardrail transitions or other special treatments.
- This standard drawing is produced by the Florida Department Of Transportation solely for use by the Department and its assignees. This standard drawing provides the general graphics and information necessary to field identify component parts of the REGENT and their incorporation into a whole system.
- This standard drawing is sufficient for plan details for the REGENT when installed in connection with shoulder guardrail and precludes the requirement for shop drawing submittals unless the plans otherwise call for such submittals. The REGENT shall be assembled in accordance with the distributor's detailed drawings, procedures and specifications.
- The first post must be a timber REGENT Post #1 with a steel foundation tube and soil plates; the second post must be a timber REGENT Post #2 with a steel foundation tube and soil plates; and, posts Nos. 3 thru 10 must be timber REGENT line posts.
- The suffix (R) indicates components unique to the REGENT System, these components along with bolts, nuts and washers not labeled are to be furnished in the distributor's package.
- The REGENT can not be used in medians where horizontal clearance requires the use of a backrail.
- See the General Notes for galvanizing requirements of metallic components.
- If the plans call for the REGENT at a specific location, substitutions with other end anchorage assemblies will not be permitted unless approved by the Engineer. If the plans call for end anchorage assembly 'flared' at a specific location, the contractor has the option to construct any FDOT approved flared assembly. Where a flared end anchorage is called for in the plans, any approved substitution with a parallel end anchorage will not be eligible for VECF consideration.
- The REGENT shall be paid for under the contract unit price for Guardrail, End Anchorage Assembly (Flared), EA and shall be full compensation for furnishing and installing all components in accordance with the plans, the distributor's detailed drawings, procedures and specifications and this index.

STATE OF FLORIDA DEPARTMENT OF TRANSPORTATION  
ROAD DESIGN

GUARDRAIL

Revised	Date	Approved By
DESIGNED BY	07/98	<i>[Signature]</i>
DRAWN BY	07/98	
CHECKED BY	07/98	00

31 of 31 400

END ANCHORAGE ASSEMBLY TYPE REGENT